



## PLANNING COMMISSION STAFF REPORT

<b>File No.</b>	PDC15-058 & PD15-053
<b>Applicant:</b>	Darlene Barnes, SteelWave
<b>Location</b>	Terminus of America Center Drive (APNs: 015-45-013, 024, 032, 048, 049, 050, 051, 051, 052, & 053 )
<b>Existing Zoning</b>	A(PD) Planned Development
<b>Council District</b>	4
<b>Historic Resource</b>	No
<b>Annexation Date:</b>	November 30, 1912
<b>CEQA:</b>	Subsequent Environmental Impact Report (EIR) to the Legacy Terrace Development Planned Development Rezoning and Prezoning Final EIR (Resolution No. 69392).

### APPLICATION SUMMARY:

1. **File No. PDC15-058:** Planned Development Rezoning to rezone from the A(PD) Planned Development Zoning District to the A(PD) Planned Development Zoning District to allow an additional 190,000 square feet of commercial and office/research and development; and
2. **File No. PD15-053:** Planned Development Permit to allow the construction of an approximately 192,350 square foot six-story office building, associated amenity space, expansion of the parking garage approved for the eastern portion of the site, and removal of 87 nonordinance trees.

### RECOMMENDATION:

Planning staff recommends that the Planning Commission recommend that the City Council:

1. Adopt a resolution certifying the Subsequent Environmental Impact Report to the Legacy Terrace Development Planned Development Rezoning and Prezoning Final EIR (Resolution No. 69392) and making certain findings concerning significant impacts, mitigation measures, alternatives, and adopting a Statement of Overriding Consideration and Mitigation, Monitoring and Reporting Program, all in accordance to the California Environmental Quality Act, as amended (CEQA).
2. Adopt an ordinance of the City of San José rezoning an approximately 70.5 gross acres located at 0 America Center Drive from the A(PD) Planned Development Zoning District to the A(PD) Planned Development Zoning District to allow an additional 190,000 square feet

of commercial and Office/Research & Development (R&D) and to remove 6.7 acres from the zoning district current boundary (File No. PDC15-058).

3. Adopt a resolution approving a Planned Development Permit, subject to conditions, to allow the construction of an approximately 192,350 square foot six-story office building (Building 5), associated amenity space, expansion of the parking garage approved for the eastern portion of the site, and removal of 87 nonordinance sized trees.

## PROJECT DATA

GENERAL PLAN CONSISTENCY			
General Plan Designation		Combined Industrial/Commercial <input checked="" type="checkbox"/> Consistent <input type="checkbox"/> Inconsistent	
Consistent Policies		LU-8.1	
SURROUNDING USES			
	General Plan Land Use	Zoning	Existing Use
North	Open Space, Parkland, and Habitat	Open Space	Open Space, Guadalupe River
South	Combined Industrial/Commercial	A(PD) Planned Development	Office
East	Open Space, Parklands, and Habitat	A(PD) Planned Development; R-MH Mobilehome Park	Office; Mobile Home Park, Railroad Tracks
West	Open Space, Parklands, and Habitat	A(PD) Planned Development Industrial; Open Space	Open Space, San Thomas Aquino Creek

RELATED APPROVALS	
Date	Action
02/29/00	Planned Development Rezoning to allow 1,015,000 square feet of office/research and development space, a 176-room hotel, and riverfront commercial uses on a 70.5-gross acre site (File No. PDC99-044).
05/24/00	Planned Development Permit to allow the construction of 900,100 square feet of commercial, office, hotel, and parking uses on a 70.5 gross acre site (File No. PD00-052). <i>Expired</i>
06/16/06	Planned Development Rezoning from the A(PD) Planned Development Zoning District to the A(PD) Planned Development Zoning District to increase in the amount of "River Commercial" uses from 25,000 square feet to 88,233 square feet on a 70.5-gross acre site (File No. PDC03-024).
11/08/07	Planned Development Permit to allow the construction of 981,362 square feet of office/R&D space spread over four six-story buildings and a 176 room hotel, and parking uses on a 70.5 gross acre site (File No. PD07-060).

<b>08/12/15</b>	Planned Development Permit Amendment to allow architectural modifications, addition of 20 parking stalls and a 19,000 square foot amenities space, and a late night incidental drinking establishment. (File No. PDA07-060-01).
<b>02/13/09</b>	Planned Development Permit to allow the construction of a 60,000 square foot indoor recreation and banquet facility and associated outdoor recreation uses on 6.7 gross acre site (File No. PD07-085).
<b>04/12/12</b>	Planned Development Permit to allow the construction of a 82,000 square foot hotel with 175 rooms, as permitted under PD07-060 (File No. PD12-048).
<b>03/22/16</b>	Planned Development Zoning and Planned Development Permit to allow the development of a four-story hotel with 261 rooms and installation of a segment of the Bay Trail adjacent to the Guadalupe River (File No. PDC15-016 & PD16-015).

## PROJECT DESCRIPTION

On October 29, 2015, the following applications were filed:

### Planned Development Rezoning (File No. PDC15-058)

The proposed Planned Development Rezoning would maintain the existing designation of A(PD) Agriculture Planned Development Zoning District and would include:

- a) The removal of the approximately 6.7 acre River Commercial area adjacent to the Guadalupe River/Alviso Slough which was covered under a separate Planned Development Zoning (PDC15-016), approved in March 2016;
- b) Adjustment in the boundaries for the remaining land use areas to reflect minor lot line adjustments; and
- c) A 190,000 square foot increase in the allowed commercial office/R&D and the existing allowed height of 90 feet will remain in place.

**Planned Development (File No. PD15-053)** The six-story office building (Building 5) would be up to 83 feet tall at the top of roof, and would contain approximately 192,350 square feet of floor space. Additional parking would be accommodated through a five-story expansion of the previously approved parking structure resulting in a total of 1,870 structured parking spaces, 1,740 surface parking spaces, and the removal of 87 nonordinance sized trees. Onsite circulation will remain the same, with the main access point located at the southern end of the site.

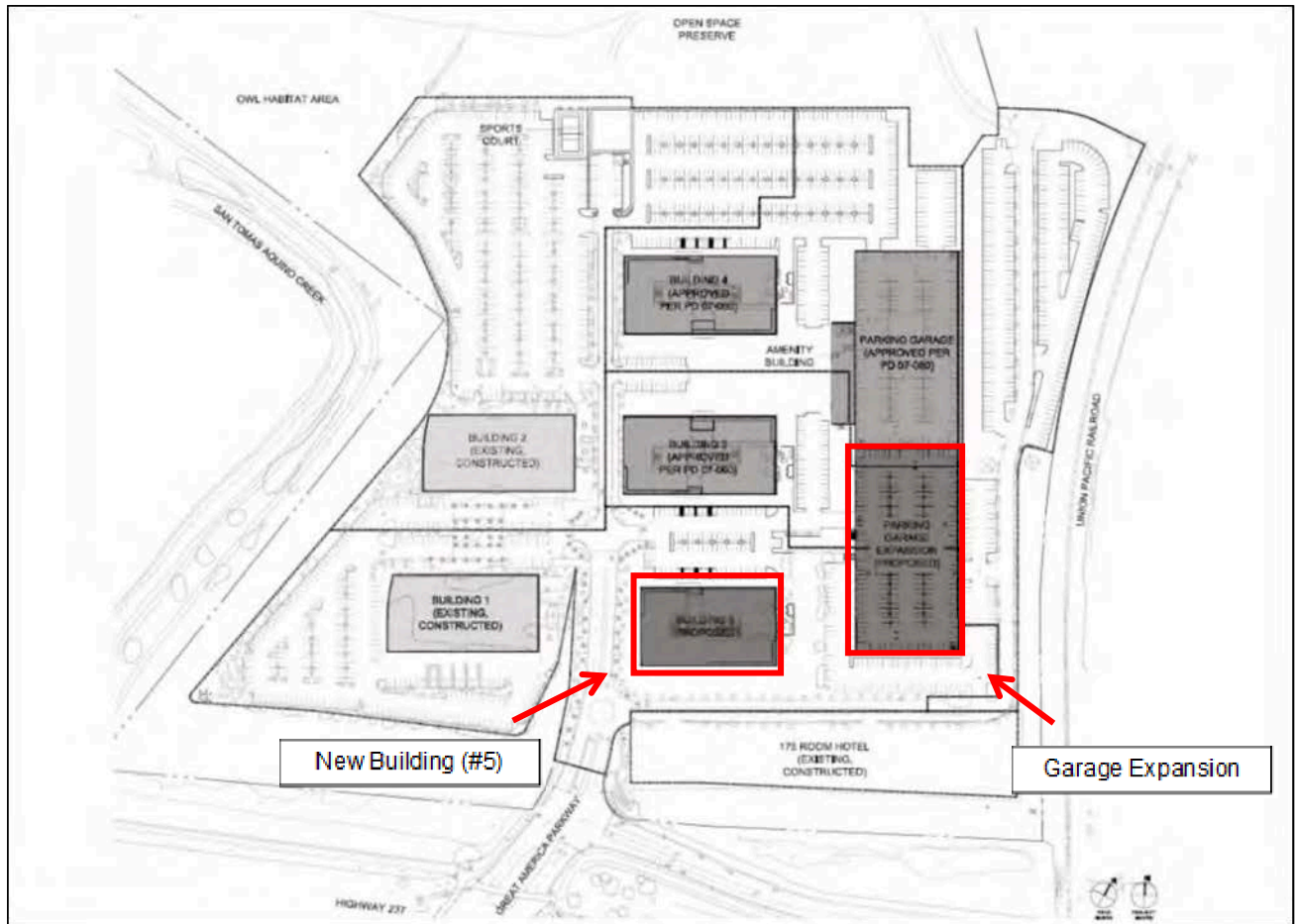


Figure 1: Site Plan

### Site and Surrounding Uses

The project site is a partially developed 63.7 gross-acre site, known as the America Center site. It is currently developed with two occupied office buildings, a hotel, [two additional office buildings under construction], a parking structure with 800 parking spaces, the connector roads and surface parking lots with 2,124 parking spaces. The proposed Building 5 and parking garage expansion will replace the existing surface parking lots. The area north of the site is designated as Open Space Preserve pursuant to the original zoning Planned Development Zoning (File No. PDC99-044).

The site is bordered by State Route 237 to the south, the San Tomas Aquino Creek to the west, salt evaporation ponds along the edge of the San Francisco Bay to the northwest, and the Southern Pacific railroad tracks to the east. Across San Tomas Aquino Creek is a wetlands mitigation site. There is an extended-stay hotel and two office buildings to the east of the Union Pacific Railroad (UPRR) train tracks.





Figure 2: Aerial of Project Site

## ANALYSIS

The proposed project was analyzed for conformance with the following: 1) Envision San José 2040 General Plan; 2) the Alviso Master Plan; 3) Zoning and PD Zoning; and 4) the California Environmental Quality Act (CEQA).

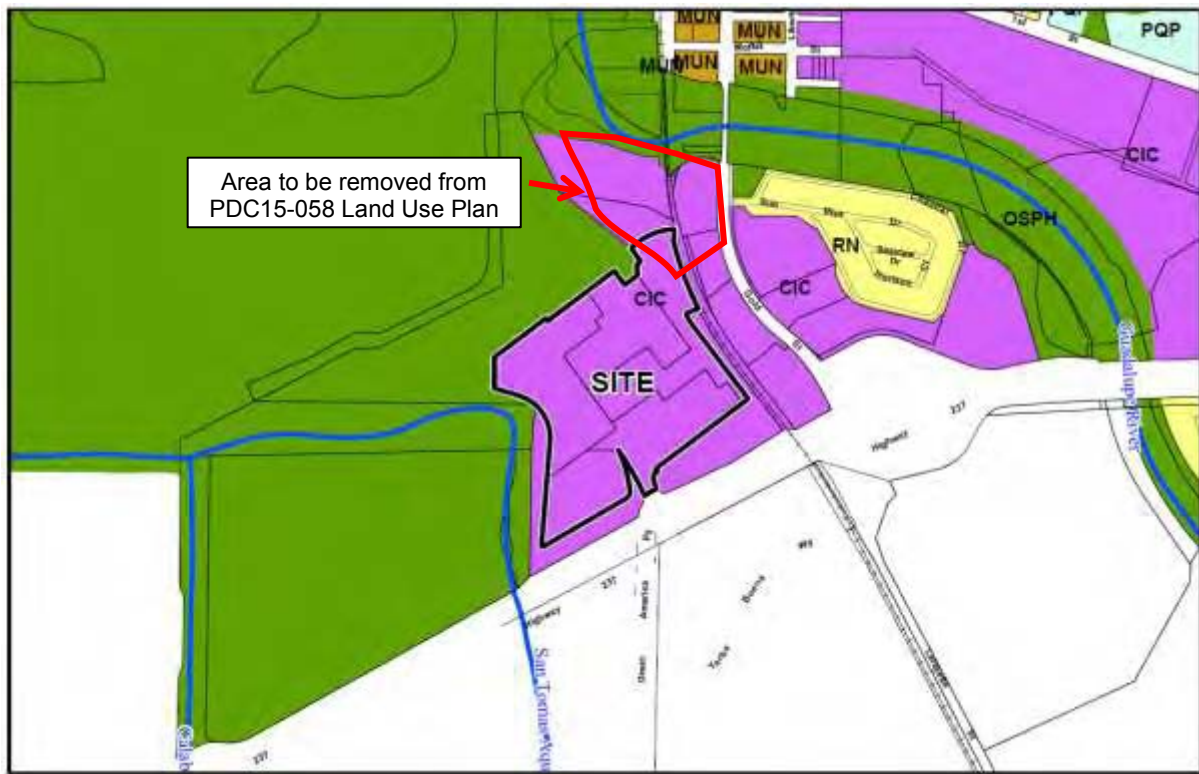
### Envision San José 2040 General Plan Conformance

The project has an Envision San Jose 2040 General Plan Land Use/Transportation Diagram designation of Combined Industrial/Commercial (CIC) (see Figure 3). This land use designation is intended for a wide variety of commercial, office, industrial developments, or a compatible mix of these uses. The America Center site contains a mix of uses including office space and hotel that serves local industry by providing business space and hotel stay for visiting staff.

The project is also consistent with the following General Plan policies:

1. Land Use Policy LU-8.1: In areas that are designated for mixed industrial and commercial uses, allow only commercial uses that are compatible with industrial uses. Non-employment uses are prohibited in these areas.

*Analysis:* The proposed Rezoning and PD Permit would accommodate additional 190,000 square feet for Building 5 and the expansion of the parking structure would accommodate the additional required parking. The requested changes would make for a larger single or multi user campus. The additional square footage will provide office space and additional employment opportunities. The proposed building and parking are compatible with the existing buildings and other commercial uses on the site in that the architecture, massing and uses are consistent with the existing development on the site.



**Figure 3: General Plan Land Use/Transportation Diagram**

2. Environmental Resources Policy ER-2.1: Ensure that new public and private development adjacent to riparian corridors in San José are consistent with the provisions of the City's Riparian Corridor Policy Study and any adopted Santa Clara Valley Habitat Plan/ Natural Communities Conservation Plan.
3. Environmental Resources Policy ER-2.2: Ensure that a 100-foot setback from riparian habitat is the standard to be achieved in all but a limited number of instances, only where no significant environmental impacts would occur.
4. Environmental Resources Policy ER-2.3: Design new development to protect adjacent riparian corridors from encroachment of lighting, exotic landscaping, noise and toxic substances into the riparian zone.
5. Environmental Resources Policy ER-4.4: Require that development projects incorporate mitigation measures to avoid and minimize impacts to individuals of special-status species.
6. Environmental Resources Policy ER-5.1: Avoid implementing activities that result in the loss of active native birds' nests, including both direct loss and indirect loss through abandonment, of native birds. Avoidance of activities that could result in impacts to nests during the breeding season or maintenance of buffers between such activities and active nests would avoid such impacts.
7. Environmental Resources Policy ER-5.2: Require that development projects incorporate measures to avoid impacts to nesting migratory birds.

8. Environmental Resources Policy ER-6.5: Prohibit use of invasive species, citywide, in required landscaping as part of the discretionary review of proposed development.
9. Environmental Resources Policy ER-7.1: In the area north of Highway 237 design and construct buildings and structures using bird friendly design and practices to reduce the potential for bird strikes for species associated with the Baylands or the riparian habitats of lower Coyote Creek.

*Analysis:* The project would respect the 100-foot riparian setback specified within of General Plan Policy ER-2.1, ER-2.2, ER-2.3 and Council Policy 6-34. Landscape plans would be in compliance with Policy ER-6.5. Mitigation measures MM BIO-1.1 and MM BIO-1.2 would be implemented consistent with General Plan Policy ER-4.4, ER-5.1, and ER-5.2. Bird-safe design features are included in the building and are reviewed by city staff as part of the overall design review process for consistency with City Council Policy 6-34 and Policy ER-7.1.

## **Alviso Master Plan**

### *Land Use*

The site is designated Combined Industrial/Commercial by the Alviso Master Plan, which allows for a wide range of industrial uses, including warehousing, office, research and development, and light manufacturing. This land uses designation also allows for retail, restaurant, hotel, or other commercial establishments. The proposed project fits into the parameters of this land use designation. The project is also consistent with several objectives, goals and policies of the Alviso Master Plan.

1. Industrial/Non-Industrial Relationships Objective: Setbacks and buffers should be established to protect environmental resources (e.g., Coyote Creek) and "sensitive uses" (e.g., residential, day care, and school uses) from potential negative impacts of industrial use.

*Analysis:* The proposed Rezoning and PD Permit maintains the adopted setbacks and established open space areas included in the original Planned Development Zoning. The proposed building 5 location is located at the interior of the site, between existing buildings. The proposed parking garage expansion is located along the existing rail line and away from designated open space areas.

2. River Orientation Policy 3: New Buildings adjacent to the Guadalupe River/Alviso Slough should be of an appropriate scale and character to enhance this waterway as a public-oriented recreation resource and as a natural riparian corridor.

*Analysis:* The Master Plan has a height limit in this area of 90 feet, which the rezoning is consistent with. As noted above, the building and garage locations are located at the interior of the site or adjacent to the existing rail line, away from existing waterways.

3. Industrial/Non-Industrial Relationship Objective: Setbacks and buffers should be established to protect environmental resources and "sensitive uses" from potential negative impacts of industrial use.

*Analysis:* The proposed use is not a typical industrial use that has operational effects on "sensitive uses". The proposed office building and parking garage addition will not be storing, handling, disposing of, and/or using acutely hazardous materials. Moreover, the

*proposed project site is located over 1,000' outside of the Riparian Corridor, which is located to the west of existing office buildings on the site.*

4. Gateway Entrances Objective: Development located near Highway 237 along both sides of Gold Street, First Street, and Zanker Road should foster a “gateway” feel through building orientation, signs, trees, landscaping, and other features.

*Analysis: The proposed building and garage addition will further enhance the overall America Center site. The constructed and entitled office buildings on the site have a high degree of visibility from State Route 237 and are designed to have a distinctive high-quality, “modern” architectural style that will support the image of a high-tech, high-quality office at Gold Street.*

5. Community Character Policy 2: New development should reflect Alviso's bayside character through the design of buildings and landscaping.
6. Lands Outside of the Village Area Design Objective: Given the high visibility of most of this area, development should be attractive and should fit in the context of the larger community; and should reflect some of the elements and materials of seaside styles to contribute to Alviso's sense of place.

*Analysis: America Center is located on the former landfill and is outside the Village area. The Alviso Master Plan includes several design guidelines for projects located outside the Village area. The proposed buildings are consistent with the development standards in that the Building 5 and garage are under 90 feet in height and they comply with the established setbacks. The building style is a simple form with softer colors compatible with the Bayfront area. The parking garage is located at the interior of the site adjacent to the railroad tracks. Landscaping around the site is comprised of a simple plant palette and located primarily with the site area.*

## **Zoning and PD Zoning Conformance**

The proposed Planned Development Rezoning would maintain the existing Zoning of A(PD). The Land Use Plan and Development Standards includes allowable land uses, setbacks, height and mitigation. The Development Standards will change slightly to allow the additional square footage, parking spaces and applicable mitigation measures. The allowable uses in the designated Commercial, Office, Research and Development area include professional and/or research and development offices and parking structure uses. All uses permitted under the IP Industrial, CO Commercial Office, and CP Commercial Pedestrian districts are also permitted. Day care, conferencing, dining rooms, personal business service retail, restaurants and satellite and broadcast reception facilities are also permitted. The new office building must maintain a minimum of a 40-foot building separation and the garage addition would maintain a minimum 20-foot setback from the east property line. The maximum building height is 90 feet, consistent with the Alviso Master Plan. Parking is required to provide a minimum of 1 space per 250 square feet (net square feet).

The proposed Building 5 is an office/R&D building and the building provides over 40-foot separation from all adjacent buildings and would be 83 feet in height at the top of roof. The parking garage addition maintains the 20-foot east property line setback and is at a maximum height of 54 feet. Parking required for building 5 is 770 parking spaces, which is accommodated



in the addition to the parking garage. Therefore, the proposed development complies with the Development Standards of the proposed and existing Planned Development Zoning.

In addition, the proposed Rezoning includes the removal of the area encompassed in PDC15-016, which was for the Hotel (Residence Inn) at the terminus of America Center Court. This area has its own Zoning, Land Use Plan and Development Standards that were adopted as part of PDC15-016. The area proposed to be removed from the America Center PD Zoning is noted in the Zoning Map (Figure 4).

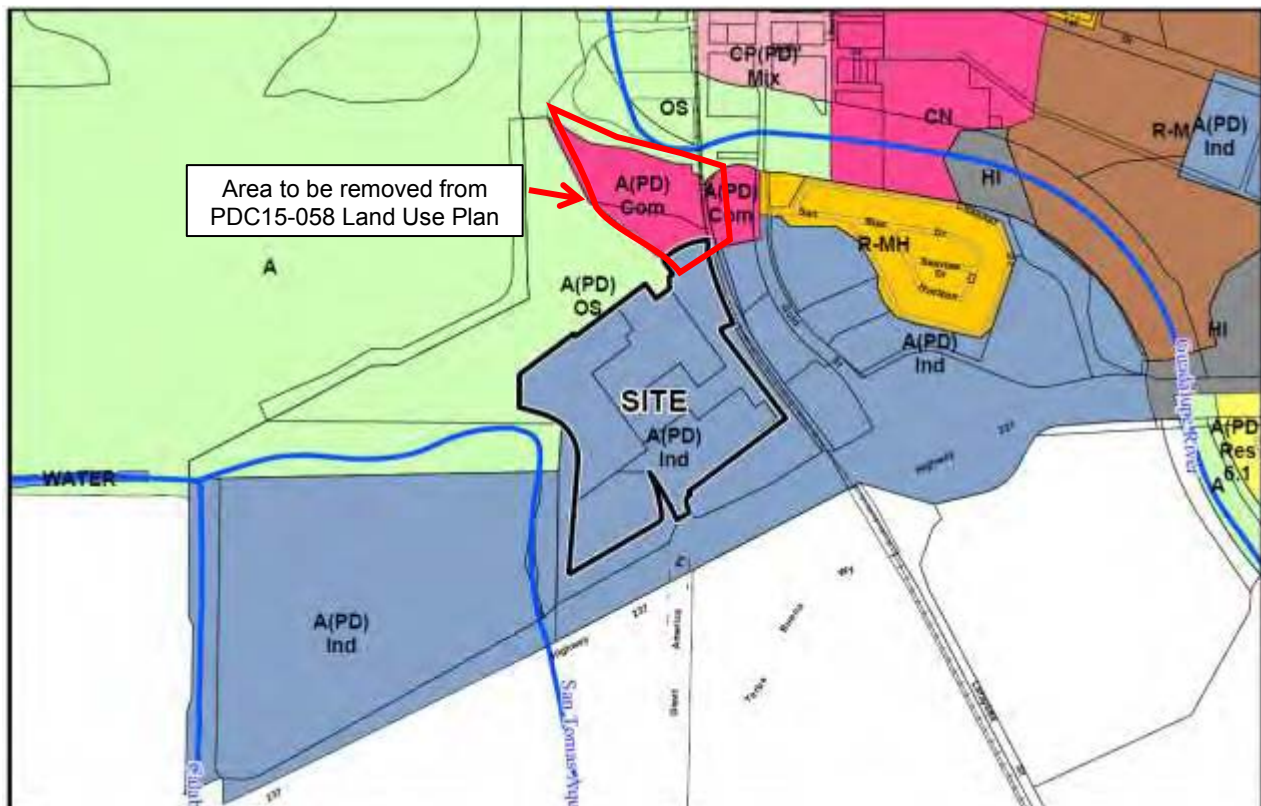


Figure 4: Zoning Map

### **Council Policy 6-34 Riparian Corridor Protection and Bird Safe Design.**

Council Policy 6-34 (adopted in August of 2016) provides guidance, consistent with the General Plan, for protecting and restoring riparian habitat; limiting the creation of new impervious surface within Riparian Corridor setbacks; and encouraging bird-safe design in Bayland and riparian habitats of lower Coyote Creek north of SR 237. This policy supplements the regulations for riparian corridor protection already contained within the Habitat Plan, Municipal Code, and other existing City policies that may provide for riparian protection and bird-safe design.

Specific guidance pertaining to setbacks, allowed activities, and materials and lighting in riparian areas are included within Council Policy 6-34. Further, bird-safe design guidelines for structures north of SR 237 advise that buildings:

- Avoid use of mirrors and large areas of reflective glass;
- Avoid use of transparent glass skyways, walkways, or entryways, free-standing glass walls, and transparent building corners;
- Avoid funneling open space to a building façade;
- Strategically place landscaping to reduce reflection and views of foliage inside or through glass;
- Avoid or minimize up-lighting and spotlights; and
- Turn non-emergency lighting off, or shield it, at night to minimize light from buildings that is visible to birds, especially during bird migration season (February through May and August through November).

Analysis: The project would respect the 100-foot riparian setback specified within of General Plan Policy ER-2.1, ER-2.2, ER-2.3 and Council Policy 6-34. Landscape plans would be in compliance with Policy ER-6.5. Mitigation measures MM BIO-1.1 and MM BIO-1.2 would be implemented consistent with General Plan Policy ER-4.4, ER-5.1, and ER-5.2. Bird-safe design features are included in the building and are reviewed by city staff as part of the overall design review process for consistency with City Council Policy 6-34 and Policy ER-7.1.

### **Planned Development Permit Findings**

Chapter 20.100 of the San Jose Municipal Code establishes evaluation criteria for issuance of a Planned Development Permit. These criteria are applied to the project based on the above-stated findings related to General Plan, Zoning and CEQA conformance and for the reasons stated below, and subject to the conditions set forth in the proposed permit. In order to make the Planned Development Permit findings pursuant to Section 20.100.720 of the San Jose Municipal Code and recommend approval to the City Council, Planning Commission must determine that:

1. The Planned Development Permit, as issued, is consistent with and furthers the policies of the General Plan;

Analysis: *As described above, the project is consistent with and furthers the policies of the General Plan and Alviso Master Plan.*

2. The Planned Development Permit, as issued, conforms in all respects to the Planned Development Zoning of the property;

Analysis: *The Planned Development Permit conforms in all respects to the proposed A(PD) Planned Development Zoning of the property, including uses, setbacks and height.*

3. The Planned Development Permit, as approved, is consistent with applicable City Council policies, or counterbalancing considerations justify the inconsistency;

*As discussed below, the project is in conformance with City Council Policy 6-30 Public Outreach. Staff received confirmation that the on-site sign was posted at the subject property, in accordance with the City Council Policy 6-30 on November 5, 2015 and a community meeting was held on October 16, 2016. In addition, the project is also consistent with City Council Policy 6-34 in that the proposed project includes View Dynamic Glass which provides varied tinting, building 5 is located with the center of site and no glass has been proposed on the parking garage addition. Lastly, the project complies with City*

*Council Policy 8-14 Post-Construction Hyrdromodification Management through the proposed stormwater treatment management system for the project site.*

4. The interrelationship between the orientation, location, mass and scale of building volumes, and elevations of proposed buildings, structures and other uses on-site are appropriate, compatible and aesthetically harmonious;

*Analysis: The orientation, location, similar height, mass and scale of the proposed Building 5 is harmonious with the surrounding built buildings and will be constructed of high quality materials. Additionally, the garage extension will be harmonious with the existing parking garage in that it will use the same materials and proportions.*

5. The environmental impacts of the project, including, but not limited to aesthetics, air quality, noise, and transportation/traffic, even if insignificant for purposes of the California Environmental Quality Act (CEQA), will not have an unacceptable negative effect on adjacent property or properties.

*Analysis: A Subsequent Environmental Impact Report was prepared for the project in compliance with the California Environmental Quality Act (CEQA) and the CEQA Guidelines. The proposed project impacts are discussed below. Based on the review of the proposed project by all of the various department, there are no non-CEQA related impacts anticipated for this project. The proposed project would result in a new impact related to transportation as discussed below. These impacts are significant and unavoidable. A Statement of Overriding Considerations has been prepared which discusses the projects benefits. There are also other significant and unavoidable impacts as identified in the original EIR, while, other identified impacts can be mitigated to a less than significant level.*

### **CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA)**

A Draft Subsequent Environmental Impact Report (DSEIR) for the America Center Phase III Project was prepared by the Director of Planning, Building and Code Enforcement in accordance to CEQA. The DSEIR analyzed project level environmental impacts and discussed alternatives to the proposed project.

The America Center Phase III Project DSEIR is a Subsequent EIR to the previously certified Final Environmental Impact Report for the Legacy Terrace Development Planned Development Rezoning and Prezoning (Legacy Terrace FEIR), SCH# 99082004. The Legacy Terrace FEIR was adopted by City Council Resolution No. 69392 on February 15, 2000. The proposed project is within the boundaries of the Legacy Terrace site (renamed America Center prior to completion of the first phase of the project). The DSEIR provides an updated evaluation of the existing environmental setting and identifies new significant environmental effects that were not previously identified in the Legacy Terrace FEIR.

#### *Identified Significant and Unavoidable Impacts*

The Legacy Terrace FEIR identified the following significant, unavoidable impacts that would be the same under the proposed project:

1. Aesthetics: views of the site from SR 237 and recreational trails in the vicinity
2. Air Quality: vehicle-related operational emissions of NOx

3. Noise: short-term, construction noise at sensitive residential receptors
4. Transportation/Traffic: mixed-flow lane freeway segment traffic at SR 237 between Great America Parkway and North First Street (PM peak hour)

Implementation of the proposed project would also result in the following new or substantially more severe significant impacts, which are significant and unavoidable.

5. Transportation: impacts at the following locations:

Mixed-flow lane freeway segment impacts:

- Eastbound SR 237 between Great America Parkway and North First Street (PM Peak Hour)
- Eastbound SR 237 between North First Street and Zanker Road (PM peak hour)
- Westbound SR 237 between I-880 and McCarthy Boulevard (AM peak hour)
- Westbound SR 237 between McCarthy Boulevard and Zanker Road (AM and PM peak hours)

High Occupancy Vehicle (HOV) lane freeway segment impacts:

- Westbound SR 237 between I-880 and McCarthy Boulevard (AM peak hour)

#### *Less than Significant Impact with Mitigation*

The proposed project impacts listed below would be reduced to a less than significant level with mitigation:

- Air Quality: construction-related dust
- Biological Resources: impacts to nesting birds and burrowing owls
- Geology and Soils: differential settlement and seismicity
- Hazards and Hazardous Materials: soil contamination and landfill gas
- Hydrology and Water Quality: contaminated stormwater runoff
- Transportation/Traffic: intersection impacts at Lafayette Street and the Gold Street Connector

The proposed CEQA resolution, Mitigation Monitoring and Reporting Program (MMRP), and EIR further explains in detail how these mitigations reduce each identified above to a less than significant level.

#### *Alternatives*

As required under CEQA, the DSEIR evaluated two no-project alternatives and one design alternative. These alternatives are as follows:

1. No Project – No Development Alternative: Under the No Project – No Development Alternative, the buildings and parking lots existing and under construction at the site would remain. Building 5 would not be constructed. The site would remain as zoned and approved for Buildings 1 through 4 and the parking garage.



2. No Project – Develop Under Current PD Zoning Alternative: The project site is currently designated Combined/Industrial Commercial in the City’s General Plan and is located within a Planned Development (PD) zoning district. Under the current PD zoning (PDC99-044), 900,000 square feet of development is allowed for the Commercial Office/R&D portion of the project site. Of that total, 867,762 square feet have been constructed or is currently under construction and 32,238 square feet of entitlement remains.

Under the current PD zoning, a 32,238-square-foot office building could be constructed. A potential project under the No Project – Develop Under Current PD Zoning Alternative would likely be one story and would occupy the footprint of the proposed Building 5. The building would likely not be visible from SR 237 and nearby recreational trails as it would be shielded on all sides by existing, much taller structures. Pile driving would still be required for a smaller Building 5 and parking garage extension.

3. Reduced Intensity Alternative: A Reduced Intensity Alternative would potentially allow for 55,000 additional feet of development resulting in an approximately 87,000-square foot, three-story Building 5 (assuming the same footprint as the proposed project and use of the remaining 32,238 square feet of yet unbuilt but entitled square footage at the site). The building would likely not be visible as it would be shielded on all sides by existing, much taller structures. It is unknown the extent of pile driving that might be necessary for a three-story structure.

*Analysis: These alternatives were thoroughly discussed and evaluated in the DSEIR. Staff does not recommend adopting any of the alternatives because Building 5 would be located within the existing development minimizing visibility and impacts to the adjacent area. The parking garage addition will accommodate the additional parking needed for the development, reducing the amount of surface parking.*

#### *Circulation and Public Comments*

The DSEIR was circulated for public review and comment for 45-days from June 12, 2017 to July 27, 2017. The City received a total of five written comment letters during the public circulation period. Comments received were from the California Department of Transportation (Caltrans); Santa Clara Valley Transportation Authority (VTA); Santa Clara Valley Audubon Society; MR Wolfe & Associates, P.C. on behalf of the Organizacion Comunidad de Alviso; and Steve Dunn, SteelWave. Issues raised in these comment letters include the following:

- Transportation analysis and impacts for project-generated traffic
- Transportation improvements (Great America Parkway/State Route 237 and Lafayette Street/Gold Street Connector intersections)
- Traffic mitigation measures (MM TRA-1.1 and MM TRA(C)-1.2)
- Transportation Demand Management (TDM) program
- Queuing analysis and queuing reductions
- Traffic congestion impacts
- Surrounding traffic improvements
- Improvements to pedestrian and bicycle access
- Bicycle parking and connectivity

- Transit access and ridership
- Impacts to potential sensitive archaeological sites and Native American consultation
- Nitrogen deposition impacts
- Air pollutant monitoring and toxic air contaminants
- Greenhouse gas emissions assessment

A First Amendment to the DSEIR was prepared that provided responses to the comments submitted during the public circulation period and revisions to the text of the DSEIR. The First Amendment together with the DEIR constitute the Final Environmental Impact Report (FEIR) for the proposed project. The DEIR and First Amendment to the DEIR are available for review on the City's Active EIRs website at: <http://www.sanjoseca.gov/index.aspx?NID=5230>

#### *DSEIR Recirculation Unnecessary*

The comments received do not identify substantive inadequacies in the Draft SEIR or new previously unidentified significant impacts that require recirculation. The recirculation of an EIR is required when significant new information is added to the EIR after public notice is given of the availability of the draft EIR for public review but before certification. "Information" can include changes in the project or environmental setting as well as additional data or other information. New information added to an EIR is not "significant" unless the EIR is changed in a way that deprives the public of meaningful opportunity to comment on a substantial adverse environmental effect of the project or a feasible way to mitigate or avoid such an effect (CEQA Guidelines Section 15088.5).

In accordance with CEQA Guidelines Section 15088, the First Amendment to the Draft SEIR for the project includes written responses to all comments received during the public review period for the Draft SEIR. As required by Section 15132 of the CEQA Guidelines, the responses in the First Amendment to the Draft SEIR address significant environmental points and comments on the content and adequacy of the DSEIR. The responses and comments provide clarification and refinement of information presented in the Draft SEIR and, in some cases, correct or update information in the Draft SEIR. No significant new information has been added to the SEIR since publication of the Draft SEIR; therefore, the SEIR does not need to be recirculated.

#### *Recommended Actions*


Staff recommends that the Planning Commission recommend that the City Council adopt a resolution certifying the Final SEIR, including a Statement of Overriding Consideration for the identified significant and unavoidable impacts. The overriding benefits of the project include: i) increased employment within San José; ii) implementation of a Transportation Demand Management (TDM) program, iii) increased economic development, and iv) development that will support the Envision San José 2040 General Plan strategies, goals, and policies. The DSEIR, associated analysis and technical reports are available online at: <http://www.sanjoseca.gov/index.aspx?nid=2434>. The Legacy Terrace EIR and associated documents are available through <http://www.sjpermits.org/> under File Number PDC99-044.

**PUBLIC OUTREACH**

In order to inform the public of the proposed project, Staff followed Council Policy 6-30: Public Outreach Policy. A Community Meeting was held on October 16, 2016 and approximately twelve members of the community attended. The Community Meeting included scoping for the EIR and project overview. Attendees inquired about the proposed project, what it included and timeline.

A notice of the public hearing was distributed to the owners and tenants of all properties located within the 1,000 feet of the project site and all Alviso neighborhoods, and posted on the City website. The staff report is also posted on the City's website. Staff has been available to respond to questions from the public.

**Project Manager:** Shaunn Mendrin, AICP

**Approved by:**  , Division Manager for Rosalynn Hughey,  
Interim Planning Director

**Date:** 12/21/2017

**Attachments:**

Draft CEQA Resolution and Signed MMRP  
Draft Ordinance  
Draft Resolution  
Development Standards  
Reduced Plan Sets (PDC15-058 and PD15-053)

**Owner/Applicant:**

Darlene Barnes  
SteelWave  
4000 East Third Avenue, Suite 500  
Foster City, CA 94404

RESOLUTION NO. \_\_\_\_\_

**A RESOLUTION OF THE COUNCIL OF THE CITY OF SAN JOSE CERTIFYING THE AMERICA CENTER PHASE III PROJECT SUBSEQUENT ENVIRONMENTAL IMPACT REPORT TO THE LEGACY TERRACE DEVELOPMENT PLANNED DEVELOPMENT REZONING AND PREZONING ENVIRONMENTAL IMPACT REPORT (SCH #1999082004) AND MAKING CERTAIN FINDINGS CONCERNING SIGNIFICANT IMPACTS, MITIGATION MEASURES AND ALTERNATIVES, AND ADOPTING A STATEMENT OF OVERRIDING CONSIDERATIONS AND A MITIGATION MONITORING AND REPORTING PROGRAM, ALL IN ACCORDANCE WITH THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, AS AMENDED**

**WHEREAS**, the America Center Phase III Project includes an increase to the allowed amount of Commercial Office/R&D area for the project site by 190,000 square feet, for the total allowed Commercial Office/R&D space to be 1,090,000 square feet; modification of the General Development Plan boundaries to reflect removal of the northeastern portion of the current project area (6.7 acres) and to reflect minor lot line adjustments; construct an approximately 192,350 square foot, six-story office building, associated amenity space, and expand the parking garage approved for the eastern portion of the site; all located on an approximately 63 gross acre site north of California State Route (SR) 237 at the terminus of Great America Parkway, in the Alviso community of the City in San José, California (collectively referred to herein as the "Project"); and

**WHEREAS**, approval of the America Center Phase III Project would constitute a project under the provisions of the California Environmental Quality Act of 1970, together with related state and local implementation guidelines and policies promulgated thereunder, all as amended to date (collectively, "CEQA"); and

**WHEREAS**, the City of San José (“City”) prepared and completed, in accordance with CEQA the Final Environmental Impact Report for the Legacy Terrace Development Planned Development Rezoning and Prezoning (“Legacy Terrace FEIR”) (Planning File No. PDC99-05-044), which analyzed the environmental impacts of the development of office, research and development, and associated commercial uses totaling 900,000 square feet, a 175-room hotel, and approximately 25,000 square feet of Riverfront Commercial uses on approximately 45.2 acres of a 70.5 acre site; and

**WHEREAS**, on February 8, 2000, the Planning Commission of the City of San José certified said Legacy Terrace FEIR, which certification was not appealed; and

**WHEREAS**, in connection with the adoption of a resolution approving said Legacy Terrace Development Planned Development Rezoning and Prezoning (Planning File No. PDC99-05-044), the City Council adopted Resolution No. 69392 on February 15, 2000, setting forth certain findings pertaining to the Legacy Terrace FEIR and adopting a mitigation monitoring and reporting program, all pursuant to the provisions of CEQA; and

**WHEREAS**, the proposed Project was evaluated and analyzed under the Legacy Terrace FEIR and it was determined a subsequent environmental report to the Legacy Terrace FEIR was required as further explained in the subsequent environmental report, as defined below, for the Project; and

**WHEREAS**, the City is the lead agency for the Project, and has prepared a Final Subsequent Environmental Impact Report to the Legacy Terrace FEIR for the Project pursuant to and in accordance with CEQA, which the Final Subsequent Environmental Impact Report is comprised of the Draft Subsequent Environmental Impact Report for the Project (the “Draft SEIR”), together with the First Amendment to the Draft SEIR (collectively, all of said documents are referred to herein as the “FSEIR”); and

**WHEREAS**, on January 10, 2018, the Planning Commission of the City of San José reviewed the FSEIR prepared for the America Center Phase III Project, and recommended to the City Council that it find the environmental clearance for the proposed Project was completed in accordance with the requirements of CEQA and further recommended the City Council adopt this Resolution; and

**WHEREAS**, CEQA requires that, in connection with the approval of a project for which an environmental impact report has been prepared which identifies one or more significant environmental effects of the project, the decision-making body of a public agency make certain findings regarding those effects and adopt a mitigation or monitoring program and overriding statement of consideration for any impact that may not be reduced to a less than significant level.

**NOW, THEREFORE**, BE IT RESOLVED BY THE COUNCIL OF THE CITY OF SAN JOSE:

1. That the above recitals are true and correct; and
2. That the City Council does hereby find and certify that the FSEIR has been prepared and completed in compliance with CEQA; and
3. The City Council was presented with, and has independently reviewed and analyzed, the FSEIR and other information in the record and has considered the information contained therein, including the written and oral comments received at the public hearings on the FSEIR and the Project, prior to acting upon or approving the Project, and has found that the FSEIR represents the independent judgment of the City of San José ("City") as lead agency for the Project, and designated the Director of Planning, Building and Code Enforcement at his office at 200 East Santa Clara Street, 3rd Floor Tower, San José, California, 95113, as the custodian of documents and record of proceedings on which the decision of the City is based; and
4. That the City Council does hereby find and recognize that the FSEIR contains additions, clarifications, modifications, and other information in its response to comments on the Draft SEIR or obtained by the City after the Draft SEIR was issued and circulated for public review and does hereby find that such changes and additional information are not significant new information as that phrase is

described under CEQA because such changes and additional information do not indicate that any of the following would result from approval and implementation of the Project: (i) any new significant environmental impact or substantially more severe environmental impact not already disclosed and evaluated in the Draft SEIR, (ii) any feasible mitigation measure considerably different from those analyzed in the Draft SEIR that would lessen a significant environmental impact of the Project has been proposed and would not be implemented, or (iii) any feasible alternative considerably different from those analyzed in the Draft SEIR that would lessen a significant environmental impact of the Project has been proposed and would not be implemented; and

5. That the City Council does hereby find and determine that recirculation of the FSEIR for further public review and comment is not warranted or required under the provisions of CEQA; and
6. The City Council does hereby make the following findings with respect to the significant effects of the environment of the Project, as identified in the FSEIR, with the understanding that all of the information in this Resolution is intended as a summary of the full administrative record supporting the FSEIR, which full administrative record should be consulted for the full details supporting these findings.

## **AMERICA CENTER PHASE III PROJECT SIGNIFICANT ENVIRONMENTAL IMPACTS**

### **Aesthetics**

**Impact:**       **Impact AES-1:** The proposed Project would contribute to impacts to views from SR 237 and from trails in the vicinity, which would represent an overall significant aesthetics impact.

**Mitigation:**   None.

**Finding:**       No feasible mitigation was identified to reduce this aesthetic impact to a less than significant level. **(Significant and Unavoidable Impact)**

**Facts in Support of Finding:** The America Center structures would significantly obscure scenic vistas viewed from SR 237 and from public trails in the vicinity. The Legacy Terrace FEIR concluded that development of five office/R&D buildings, a 175-room hotel and then proposed River Commercial uses would significantly obscure scenic views from SR 237, the Alviso community, Alviso Marina County Park and the San Francisco Bay Wildlife

Refuge. With construction of the proposed Building 5, views would be generally limited because Building 5 is surrounded on all sides by other previously constructed or approved buildings; however, there is still the potential for the structure and parking structure extension to be viewed from the Guadalupe River Trail, Bay Trail alignments, and portions of SR 237. The proposed Project would be required to implement policies from the City's Commercial Design Guidelines and Design Guidelines for Commercial Development for Lands Outside of the Alviso Village Area to reduce the Project's effects on the visual character of the area related to architectural design, use of quality materials, and landscaping. Although the Project's compliance with these policies will reduce the aesthetic/visual impacts, the Project's visual impacts from scenic vistas are in an area that is contributing to a previously considered significant and unavoidable impact, as described within the Legacy Terrace FEIR.

### **Air Quality**

**Impact:** **Impact AIR-1:** The proposed Project would contribute to an impact as a result of exceedance of BAAQMD standards for operational nitrogen oxide (NOx) emissions, as previously identified for the buildout of the America Center site in the Legacy Terrace FEIR.

**Mitigation:** **MM AIR-1.1:** The Project applicant shall include the following updated measures from the Legacy Terrace FEIR:

- Provide physical improvements, such as sidewalk improvements, landscaping and bicycle parking which would encourage pedestrian and bicycle modes of travel;
- Connect the site to the regional bicycle/pedestrian trail system;
- Provide shuttle bus service to the Tasman/Lafayette light rail and Altamont Corridor Express (ACE) rail system; and
- Implement other feasible transportation demand management (TDM) program measures; including a ride-matching program, guaranteed ride home programs, coordination with regional ride-sharing organizations, and a transit incentives program. The Project applicant shall submit a Transportation Demand Management (TDM) Plan to the satisfaction of the Transportation Manager of the Department of Public Works and the Department of Planning, Building and Code Enforcement (PBCE) Supervising Environmental Planner prior to approval of a Planned Development Permit. The TDM Plan shall contain components or equivalent measures to result



in a ten percent (10%) reduction in projected weekday mobile emissions. The Project will be required to submit an annual monitoring report to the Transportation Manager of the Department of Public Works and the PBCE Supervising Environmental Planner to measure the effectiveness of the TDM plan. Additional TDM measures may be required if the TDM measures are not effective in achieving a ten percent (10%) reduction.

**Finding:** With the implementation of MM AIR-1.1, regional air quality impacts would be lessened and would not result in substantially greater impacts than the significant and unavoidable nitrogen oxide (NOx) impact previously identified in the Legacy Terrace FEIR. However, these measures would not reduce NOx levels resulting from implementation of the proposed Project to a less than significant level. **(Significant and Unavoidable Impact)**

**Facts in Support of Finding:** The operational criteria pollutant emissions modeled in the Legacy Terrace FEIR exceeded all applicable BAAQMD thresholds. Since the certification of the Legacy Terrace FEIR, there have been significant changes in air quality standards and methodologies for impact calculations. Additionally, pollutant levels from auto emissions (the primary source of operational criteria pollutants) have dropped substantially due to state regulatory standards and requirements. It is anticipated that vehicle-related pollutant emissions levels will continue to drop in future years.

The entire America Center site, including the proposed Project, would not result in an exceedance of BAAQMD thresholds except with regard to NOx. The NOx exceedance would not result in a substantial increase in the severity of the previously identified significant regional air quality impact; rather, the severity of the exceedance has substantially lessened. With the implementation of Mitigation Measure AIR-1.1, NOx emissions from the operation of the proposed Project would be reduced but would still result in a significant and unavoidable impact, as described in the Legacy Terrace FEIR.

**Impact:** **Impact AIR-2:** Odors could occur as a result of drilling holes for support piles that penetrate the landfill cap and impact sensitive receptors in the area.

**Mitigation:** **MM AIR-2.1:** The Project applicant shall prepare and implement an odor-control plan prior to the onset of construction which includes the following odor control elements:

- Scheduling of construction phasing such that the amount of uncovered/disturbed waste at one time is minimized;
- Controlling odors by covering any exposed landfill material with soil, foam, or other suitable material (including application of deodorant or other odor control materials);
- Considering seasonal weather conditions that can concentrate odors or direct odors towards sensitive receptors; and
- Providing the residents of the nearby Summerset Mobile Estates and the Supervising Planner of PBCE with the name and phone number of a Project Contact who shall respond to any complaints about dust, odors, or other nuisances associated with waste excavation and relocation operations.

**Finding:** Implementation of MM AIR-2.1 would limit the generation of odors and resulting odor impacts to sensitive receptors in the area. **(Less Than Significant with Mitigation Incorporated)**

**Facts in Support of Finding:** As the work involving placement of the cap and cover is complete, significant odors are not anticipated as a result of disturbance to buried landfill materials. However, construction of the proposed Project, specifically Building 5 and the expanded parking garage, would involve drilling holes for the support piles that would penetrate the landfill cap. As required by Mitigation Measure MM HAZ-1.1, material excavated by the driller would be contained and disposed of at an appropriate facility. These activities could result in odor impacts at area sensitive receptors. Implementation of the odor control plan would reduce the odor impacts to neighboring sensitive receptors to a less than significant level.

### **Biological Resources**

**Impact:** **Impact BIO-1:** If present, construction activities could cause disturbance to birds nesting and foraging in the project area.

**Mitigation:** **MM BIO-1.1:** The Project applicant shall implement the following measures to avoid impacts to nesting birds on and adjacent to the site during construction.

- Construction activities shall be scheduled to avoid the nesting season. If construction activities are scheduled to occur outside the nesting season, all impacts on nesting birds protected under the Migratory Bird Treaty Act (MBTA) and California Fish and Game

Code will be avoided. The nesting season for most birds in Santa Clara County extends from February 1st to August 31st (inclusive).

- If it is not possible to schedule construction activities between September 1st and January 31<sup>st</sup> (inclusive), then pre-construction surveys for nesting birds shall be conducted by a qualified ornithologist to ensure that no nests will be disturbed during project implementation. This survey shall be completed no more than 14 days prior to the initiation of construction activities during the early part of the breeding season (February 1st through April 30th, inclusive) and no more than 30 days prior to the initiation of these activities during the late part of the breeding season (May 1st through August 31st, inclusive). During this survey, the ornithologist shall inspect all trees and other potential nesting habitats (e.g., trees, shrubs, ruderal grasslands, buildings) in and immediately adjacent to the construction areas for nests. If an active nest is found sufficiently close to work areas to be disturbed by these activities, the ornithologist in consultation with California Department of Fish and Wildlife (CDFW) will determine the extent of a construction-free buffer zone to be established around the nest (typically 300 feet for raptors and 100 feet for other species) to ensure that no nests of species protected by the MBTA and California Fish and Game Code will be disturbed during project implementation.
- A report summarizing results of the pre-construction surveys and subsequent efforts to protect nesting raptors or birds (if found to be present) shall be submitted to the City of San José Supervising Environmental Planner of PBCE.

**Finding:** Implementation of Mitigation Measure MM BIO-1.1 would reduce impacts to nesting raptors and other migratory birds to less than significant levels.  
**(Less Than Significant with Mitigation Incorporated)**

**Facts in Support of Finding:** Conducting pre-construction surveys and implementing a construction-free buffer zone around any migratory bird nests will ensure that raptor or migratory bird nests are not disturbed during Project construction, under the MBTA and California Fish and Game Code. The size of the buffer zones will be determined by consultation between the qualified ornithologist and the California Department of Fish and Wildlife, and based on scientific evidence and best management practices. Compliance with Mitigation Measure MM BIO-1.1 will avoid impacts to nesting birds.

**Impact:** **Impact BIO-2:** If present, construction activities could cause disturbance to burrowing owls nesting and foraging in the Project area.

**Mitigation:** **MM BIO-2.1:** The Project applicant shall implement the following measures to avoid impacts to nesting or non-nesting burrowing owls on or immediately adjacent to the site, consistent with Condition 15 of Chapter 6 of the Santa Clara Valley Habitat Plan.

- Prior to any site disturbance, staging, or construction-related activities, a qualified biologist shall conduct burrowing owl preconstruction surveys in all suitable habitat areas on the Project site and within 250 feet of all construction activity. The purpose of the preconstruction surveys is to document the presence or absence of burrowing owls on the Project site and within 250 feet of construction activity in order to avoid direct impacts to burrowing owls. To maximize the likelihood of detecting owls, the preconstruction survey shall last a minimum of three hours. The survey shall begin one hour before sunrise and continue until two hours after sunrise (three hours total) or begin two hours before sunset and continue until one hour after sunset. Additional time may be required for large Project sites. A minimum of two surveys shall be conducted (if owls are detected on the first survey, a second survey is not needed). All owls observed shall be counted and their locations mapped.
- Surveys shall conclude no more than two calendar days prior to site disturbance, staging, or construction-related activities. Therefore, the Project applicant must begin surveys no more than four days prior to construction (two days of surveying plus up to two days between surveys and construction). To avoid last-minute changes in schedule or contracting that may occur if burrowing owls are found, the Project applicant may also conduct a preliminary survey up to 14 days before construction. This preliminary survey may count as the first of the two required surveys as long as the second survey concludes no more than two calendar days in advance of construction.
- If burrowing owls are present during the nonbreeding season (September 1st to January 31st, inclusive), a 250-foot buffer zone shall be maintained around the occupied burrow(s) as determined by a qualified biologist, if feasible. If maintaining such a buffer is not feasible, then the buffer must be great enough to avoid injury or mortality of individual owls based on monitoring results. During the breeding season (generally February 1st to August 31st, inclusive), a 250-foot buffer, within which no newly initiated project-related activities shall be permissible, shall be maintained between project activities and occupied burrows. Owls present between February 1st

and August 31st (inclusive) will be assumed to be nesting, and the 250-foot protected area shall remain in effect until August 31st. If monitoring evidence indicates that the owls are no longer nesting or the young owls are foraging independently, the buffer may be reduced based on monitoring results, in consultation with the City and CDFW.

- If nesting owls are determined to be present on the site, and Project activities cannot feasibly avoid disturbance of the area within 250 feet of the occupied burrow during the nesting season (i.e., February 1st through August 31st, inclusive) due to other seasonal constraints, a qualified biologist shall be present during all activities within 250 feet of the nest to monitor the owls' behavior. If, in the opinion of the qualified biologist, the owls are unduly disturbed (i.e., disturbed to the point of harm or reduced reproductive success), all work within 250 feet of the occupied burrow will cease until the nest is determined to no longer be active by a qualified biologist.

**Finding:** Implementation of Mitigation Measure MM BIO-2.1, which is an updated and expanded versions of the mitigation measure that was included in the Legacy Terrace FEIR and consistent with General Plan Policy ER-5.1 and 5.2, would reduce impacts to nesting or non-nesting burrowing owls to less than significant levels. **(Less Than Significant with Mitigation Incorporated)**

**Facts in Support of Finding:** Several pairs of burrowing owls, a California species of special concern, are historically known to have nested in the vicinity of the site and may forage on the Project site on occasion.

The larger America Center Project site was required to implement a burrowing owl mitigation and management program. There are no records of burrowing owls on the Project site for Building 5 and the parking garage expansion. A focused survey for suitable habitat of the burrowing owl within the proposed development footprint failed to find evidence of any burrowing owl presence and no suitable roosting, nesting, or foraging habitat was present. However, potentially suitable roosting and foraging habitat is present within 250 feet of the Project footprint. Heavy ground disturbance, noise, and vibrations caused by proposed construction could potentially disturb foraging or roosting burrowing owls and cause them to move away from work areas. As grading, demolition, and construction activities could impact burrowing owl habitat, conducting pre-construction surveys and the implementation of a construction-free buffer zone around nests and owl sighting locations, consistent with Condition 15 of the Santa Clara Valley

Habitat Plan, will ensure that the species are not disturbed during Project earthmoving activities.

### **Geology and Soils**

**Impact:** **Impact GEO-1:** Differential settlement could result in structural damage to the proposed development.

**Mitigation:** **MM GEO-1.1:** The Project applicant shall complete a design-level geotechnical investigation for the Project site prior to issuance of any grading permits for individual site improvements to address the potential geologic hazards. Design-level engineering studies shall be submitted to the City's Public Works Department for review and approval.

Building foundations shall be designed based on this geotechnical investigation. Building loads shall be supported on driven pile foundations as appropriate to support the building loads without significant damage due to settlement. Foundation piles shall be designed to accommodate downdrag loads caused by the subsidence of landfill materials due to the natural degradation of landfill components, and under the weight of the final cover and/or soil placed on the site for development purposes. In addition, a reinforced concrete "skirt wall" around the perimeter of each building shall be installed to resist lateral loads placed on the building during a seismic event. Settlement adjacent to the skirt wall shall be monitored and soil replaced to avoid the loss of lateral support as fill settles.

Conceptual foundation installation procedures in the area include: predrilling each pile location to the full depth of the landfill (50 to 60 feet); and/or installing a cased shaft at each pile location from the ground surface to the bottom of the landfill, removing landfill materials from the cased hole, and driving the foundation piles through the casing into the supporting soil below the landfill. Alternatively, non-displacement type piles, such as steel H-piles, could be driven directly through landfill materials. Corrosion control measures to protect steel and/or concrete piles shall be included in the design-level geotechnical investigation.

**MM GEO-1.2:** An updated settlement map shall be prepared based upon site monitoring and additional surveys prior to the completion of the design-level geotechnical investigation. The updated settlement map shall confirm appropriate post settlement grades on the site. The map shall be provided to the City of San José Public Works Department for review and approval.

**MM GEO-1.3:** To allow for settlement between structures and the surrounding ground at building entrances, "hinged slabs" or interlocking pavers shall be used. For hinged slabs, one end of the hinged slab will be fixed to the pile-supported structure and the other end will rest in the earth fill that will settle with time. The design of the hinged slab shall be based on the maximum operation slope of the slab. For pedestrian slabs, the estimated finished grade after settlement shall be based on a maximum slope required by the Americans with Disabilities Act. For vehicular slabs, the estimated finished grade after settlement shall be based on a maximum gradient differential of 11 percent between the slab and the stationary foundation, which allow use of the parking structure entrances without scraping the bottom of vehicles. Alternatively, interlocking pavers installed at building entrances can be easily adjusted to grade after settlement has occurred. Pavers shall be monitored at more frequent intervals than hinged slabs and regraded at regular intervals to avoid tripping hazards. The design of hinged slabs or pavers shall be completed using the current settlement map for the site.

**MM GEO-1.4:** Roadways and other paving systems shall utilize flexible materials such as asphaltic concrete, interlocking paving units, and avoid or limit the use of Portland cement concrete and other nonflexible materials. Where concrete is utilized, adequate expansion and spacing joints shall be used to accommodate differential settlement. Geotextile fabric or other materials shall be placed below the subgrade base section to provide bridging over localized "soft" areas determined by the geotechnical engineer during compaction of the fill material. Joints shall be adequately sealed between differing materials (i.e., asphalt and concrete curbs) to prevent water infiltration.

**MM GEO-1.5:** Pavements and other surface improvements shall be designed with adequate slope so that after settlement, reversals of stormwater flow direction or adverse flattening of the roadway pavement surface does not occur.

**MM GEO-1.6:** On-site utilities which operate via gravity shall be designed based upon the anticipated settlement on the site. These utilities shall be designed with adequate slope so that after settlement, reversal or flattening of the slope of utility lines does not occur.

**MM GEO-1.7:** Pipe materials which can accommodate differential settlement without separation of pipe joints or leakage shall be used on the site. Piping could utilize high density polyethylene or, in some cases, dual contained polyvinyl chloride pipe. For either type of pipe system, metallic

fittings, valves, and flexible connections could be housed inside vaults for corrosion protection and to aid leak detection.

**MM GEO-1.8:** Under slab utilities, shall be connected to the structural slab using hangers constructed of a non-corrosive material, such as stainless steel. To counter the effect of soil in the utility trench settling and dislocating the utility line from the hanger, a non-cohesive backfill, such as pea gravel, shall be used in the trench. As the ground settles, the non-cohesive backfill shall be able to move around the pipe. Alternatively, no backfill shall be placed in the trench, with plywood or other materials being used to prevent concrete from the structural slab pour from entering the utility trench. As the surrounding ground settles, the utility pipe would be supported by hangers.

**MM GEO-1.9:** To accommodate the difference in settlement between the building and surrounding ground, flexible utility connections contained within a settlement vault shall be employed.

**MM GEO-1.10:** The Project applicant shall prepare and implement an Operations and Maintenance Program for the building, utilities, and pavement, and shall include a site grade monitoring schedule. Site grades shall be monitored every three months for the first two years. After two years, the monitoring duration shall be reevaluated based on the settlement rates and site characteristics. The Operations and Maintenance Program shall specify the types of repairs to be made in the event that indications of localized depressions, slope changes or cracking of pavements are found.

**Finding:** With the implementation of MM GEO-1.1 through MM GEO-1.10, geology and soils impacts as a result of differential settlement and expansive soils would be reduced to a less than significant level. **(Less Than Significant with Mitigation Incorporated)**

**Facts in Support of Finding:** As described in the Legacy Terrace FEIR, settlement of unengineered fill material as a result of the consolidation and compression of the landfill and compression of native soil could affect building foundations and the operation of utility lines. Settlement could result in adverse flattening of gravity utility slopes and lead to a reversal of flow direction or inadequate velocities to prevent accumulation within pipes. Differential settlement could also cause separation of utility lines at pipe joints, resulting in leakage of interruption lines.

Differential settlement can result in structural damage to the proposed buildings, roadways and pavement of the proposed Project due to the variable nature of the project site's non-engineered fill materials.



Under Title 27 of the California Code of Regulations (CCR), Section 21190, waste disposal site post-closure land uses are overseen by various regulatory agencies, including the Regional Water Quality Control Board (RWQCB), CalRecycle, and others. The City of San José is the designated Solid Waste Local Enforcement Agency (LEA) with oversight authority over the project site's Post-Closure Land Use Plan. The plan must be prepared and reviewed pursuant to Section 21190 of CCR Title 27. Review and approval of the Post-Closure Land Use Plan at past solid waste disposal sites represents a major part of the LEA's responsibility to protect public health and safety and the environment. CalRecycle established regulations addressing post-closure land use activities in 1989 based on documented problems associated with poorly regulated development on disposal sites (Final Statement of Reasons, Disposal Site Standards for Closure and Post-closure, pages III-7.8 129-139). The Project site's Post-Closure Land Use Plan for the development of Phase II of the project (Buildings 3 and 4) was approved by the City of San José as the LEA in September of 2015. The City of San José will also review the Post-Closure Land Use Plan for the proposed project (Phase III) prior to the start of and grading or construction.

Modifications to the Post-Closure Land Use Plan would address deferential settlement issues related to construction of Building 5 and the parking garage expansion. Mitigation Measures MM GEO-1.1 to MM GEO-1.10, were included in the Legacy Terrace FEIR for which an updated and expanded version would be included in this Project, will reduce the impacts of differential settlement on the Project site.

### **Hazards and Hazardous Materials**

**Impact:** **Impact HAZ-1:** Methane gas within landfill materials could result in flammable or explosive conditions. Toxic compounds, including volatile organic compounds, and asbestos, could be present and pose health risks to construction workers and/or the public.

**Mitigation:** **MM HAZ-1.1:** The Project applicant shall follow the United States Environmental Protection Agency (EPA)-approved Soil Management Plan (SMP) developed for closure, capping, maintenance, and post-capping construction activities at the Highway 237 Landfill to reduce or eliminate exposure risk to human health and the environment, specifically, potential risks associated with the presence of methane, hydrogen sulfide, and volatile organic compounds (VOCs) in soil gas and potential unknown conditions. The SMP shall be followed for any development related activities

that penetrate the low-permeability layer of the landfill cap, such as pile installation or excavations. As required by the SMP, prior notification of planned activities that trigger implementation of SMP protocols shall be provided to the PBCE, Environmental Services Department, the City's LEA, and other regulatory agencies providing oversight (such as the RWQCB and CalRecycle) prior to issuance of any grading permit.

**MM HAZ-1.2:** Contractors and subcontractors at the Project site shall develop a health and safety plan specific to their scope of work and based upon the known environmental conditions for the site. Each health and safety plan shall be implemented under the direction of a Site Safety and Health Officer and provided to all regulatory agencies providing oversight (such as the LEA, CalRecycle, or RWQCB).

**MM HAZ-1.3:** The Project site is on a former landfill and shall follow environmental monitoring procedures required by CalRecycle and the San José LEA.

**MM HAZ-1.4:** The Project applicant shall ensure that where an irrigation system is installed for landscaping, it shall be designed to optimize watering using the most current automatic irrigation equipment and monitoring methods. To help minimize infiltration, subdrains shall be constructed for all tree planting areas. The trees shall have subdrains that discharge to the storm drain system.

**MM HAZ-1.5:** Prior to issuance of any grading permit for site improvements, the Project applicant shall provide the LEA, CalRecycle, and an appropriate oversight agency (such as the Department of Toxic Substances Control [DTSC] or RWQCB and the City's Department of Public Works) with a Project-level, engineering analysis that addresses, in sufficient detail, the following elements of the final project design:

- Soil gas mitigation and monitoring systems, including structure monitoring and perimeter monitoring systems;
- Differential settlement;
- Site surface drainage and final grading; and
- Any other elements of the design as required by the LEA or Department of Public Works, including specialized analysis that may be warranted by the City. The Project applicant shall bear the responsibility for providing any such specialized analysis.

**MM HAZ-1.6:** The Project applicant shall incorporate a landfill gas control system into all buildings constructed as a part of the Project. Proposed structures shall be constructed with a sub-slab soil gas mitigation system to vent landfill gases and other soil vapor. The soil gas mitigation system may consist of perforated pipes placed in a permeable granular layer under building and garage concrete slabs. The perforated pipes shall be connected to a system that discharges vapor to the building's exterior. The system shall include a methane sensor/venting system that is capable of venting soil vapor out from beneath the building, and a low-permeable barrier layer, such as Liquid Boot, shall be installed in the buildings and certain areas in the parking garage that have the potential to accumulate landfill gas in order to prevent soil vapors from intruding into the structures. The low-permeable vapor barrier membrane shall be located above the permeable granular layer. The membrane shall be sealed around foundation piles, grade beams, and slab penetrations (such as utility lines). The Project applicant shall also install a landfill gas venting system beneath hardscape areas near the proposed structures.

The landfill gas control system for the office buildings shall include an integrated methane sensor/blower system that is capable of actively drawing soil vapor from beneath the building. Methane sensors provided at the discharge point of each soil gas mitigation system shall control the active operation of the venting system.

The Project applicant shall prepare and implement an Operations and Maintenance Program for the soil gas mitigation systems. The program shall include instructions for how to ensure that the system functions properly.

**MM HAZ-1.7:** The Project applicant shall construct site utility trenches with landfill gas cut-offs to prevent landfill gas from migrating along utility trenches. Below-grade electrical facilities shall be designed for explosive conditions, in accordance with the California Building Standards Code.

**Finding:** Implementation of MM HAZ-1.1 to MM HAZ-1.7 would ensure that hazardous substances on-site would not result in a significant hazard to construction workers, future users of the site, or the environment. **(Less Than Significant with Mitigation Incorporated)**

**Facts in Support of Finding:** As described in the FSEIR, the majority of the America Center site is elevated due to its location on top of the closed Highway 237 Landfill. The landfill was a disposal site that was in operation from 1962 to 1982. Materials reported in the landfill include municipal trash, soil,

concrete, asphalt, wood, and other construction rubble. Four modern commercial office buildings (two of which are currently under construction), a hotel building, parking garage (also under construction), open space preserve, and parking lots are located on the former landfill. The closed landfill generates landfill gas, including methane, which is a combustible gas. The landfill may also contain petroleum hydrocarbons and volatile organic compounds (VOCs) generated from organics from refuse and/or underlying estuary soils, plasticizers, wood preservatives in demolition debris, glue thinners and asphalt or roofing material contained within fill materials

As evaluated in the Legacy Terrace FEIR, development of the Project site would expose construction workers to VOCs and methane in soil vapor/gas from the closed Highway 237 Landfill. Methane exposure is considered to be an explosion hazard at locations at certain concentrations. Volatile organic compounds have the potential to migrate from soil vapor to indoor air via a vapor intrusion pathway. Additionally, asbestos-containing or other contaminated materials could be encountered where waste is removed as part of installation of drilled foundation piles.

The Project will comply with local and state monitoring procedures and prepare a project-level, engineering analysis. The preparation and implementation of a site management plan and a health safety plan will limit human health exposure risk to these compounds. Additionally, the irrigation system, landfill gas control system, and site utility trenches will be designed as described in the above mitigation measures to reduce the impacts of hazardous compounds infiltrating the project work areas and buildings. Mitigation Measures MM HAZ-1.1 to MM HAZ-1.7 were included in the Legacy Terrace FEIR, and the FSEIR for the Project includes updated versions of these mitigation measures, all of which together will reduce the impacts of hazards and hazardous materials to construction workers, future users of the site, and the environment.

### **Noise**

**Impact:** **Impact NOI-1:** Pile driving noise generated in the eastern portion of the site could impact residents at Summerset Mobile Estates and commercial uses along Gold Street in the short-term.

**Mitigation:** **MM NOI-1.1:** Prior to the start of construction, the Project applicant shall prepare and implement a noise logistics plan to reduce construction noise levels as low as practical. The noise logistics plan shall be submitted to the

Supervising Environmental Planner of PBCE for review and approval. The noise logistics plan would include, but not be limited to, the following measures:

- Construction hours within 500 feet of residential uses will be limited to the hours of 7:00 a.m. and 7:00 p.m. weekdays, with no construction on weekends or holidays. Pile driving shall be limited to the hours of 8:00 a.m. to 5:00 p.m. Monday through Friday.
- Utilize 'quiet' models of air compressors and other stationary noise sources where technology exists.
- Equip all internal combustion engine-driven equipment with mufflers, which are in good condition and appropriate for the equipment.
- Locate all stationary noise-generating equipment, such as air compressors and portable power generators, as far away as possible from adjacent land uses.
- Locate staging areas and construction material areas as far away as possible from adjacent land uses.
- Prohibit all unnecessary idling of internal combustion engines.
- If impact pile driving is proposed, multiple-pile drivers shall be considered to expedite construction. Although noise levels generated by multiple pile drivers would be higher than the noise generated by a single pile driver, the total duration of pile driving activities would be reduced.
- If impact pile driving is proposed, temporary noise control blanket barriers shall shroud pile drivers or be erected in a manner to shield the adjacent land uses. Such noise control blanket barriers can be rented and quickly erected.
- The contractor shall prepare a detailed construction plan identifying a schedule of major noise generating construction activities. This plan shall identify a noise control disturbance coordinator and procedure for coordination with the adjacent noise sensitive facilities so that construction activities can be scheduled to minimize noise disturbance. This plan shall be made publicly available for interested community members.
- The disturbance coordinator shall be responsible for responding to any local complaints about construction noise. The disturbance coordinator will determine the case of the noise complaint (e.g., starting too early, bad muffler, etc.) and will require that reasonable measures warranted to correct the problem be implemented. The

telephone number for the disturbance coordinator at the construction site shall be posted and included in the notice sent to residences and commercial businesses within 100 feet regarding the construction schedule.

The Project, as currently proposed, would implement measures to reduce short-term noise impacts resulting from pile driving activities in the eastern portion of the site, but not to a less than significant level.

**Finding:** Despite the implementation MM NOI-1.1, and as identified within the Legacy Terrace FEIR, construction noise-related impacts on sensitive residential receptors as a result of pile-driving activities would still be considered significant and unavoidable. **(Significant and Unavoidable Impact)**

**Facts in Support of Finding:** Commercial/office uses along Gold Street in Alviso and residential uses in the Summerset Mobile Estates Mobile Home Park are located approximately 200 feet and 950 feet east of the Project site, respectively. Additionally, a 175-room hotel (Aloft) is located on the southern portion of the site, east of the Project entrance from Great America Parkway. Construction noise along the eastern boundary of the America Center could result in a significant impact to residents of the mobile home park because of projected noise levels and the repetitive impulsive nature of pile driving noise, which could interfere with indoor and outdoor activity. It could also affect the commercial office and hotel uses in the vicinity.

Construction noise impacts primarily happen when construction activities occur during noise-sensitive times of the day (early morning, evening, or nighttime hours), the construction occurs in areas immediately adjoining noise sensitive land uses, or when construction durations last over extended periods of time. The Project, specifically Building 5 and the parking garage expansion, would be completed in approximately 20 months with heavy construction to be completed in less than 12 months. Although implementation of Mitigation Measure MM NOI-1.1 would reduce construction noise impacts to surrounding land uses, it would not effectively reduce the construction noise levels that substantially exceed ambient and background noise levels to less than significant at adjacent sensitive residential receptors. This impact is significant and unavoidable.

### **Transportation and Traffic**

**Impact:** **Impact TRA-1:** The added trips as a result of the proposed Project would cause the critical movement delay to increase by four or more seconds and

the demand-to-capacity ratio to increase by 0.01 or more at the Lafayette Street and Gold Street Connector intersection during the AM peak hour.

**Mitigation:** **MM TRA-1.1:** Prior to Public Works Clearance, the Project applicant shall pay a fair share amount towards improvements (including full cost for design, construction, etc.) to the Lafayette Street and Gold Street Connector intersection. Improvements would include the addition of a second northbound left-turn lane in a vacant area between the Gold Street Connector and SR 237. The improvement shall require widening of the Gold Street Connector and shifting of travel lanes to the south by approximately 12 feet to accommodate a second receiving lane for the second northbound left-turn lane. The roadway widening would also require the relocation of the Highway 237 Bikeway, south of the Gold Street Connector. The Director of Public Works shall determine the fair share based on the cost of the improvement at the time the payment is due and the Project's contribution to the impact (an estimated 5 percent). The fair share amount shall be paid to the City of San José Public Works Depositors Fund.

During construction of the intersection improvement, a trail detour shall be provided and/or the Highway 237 Bikeway relocated prior to construction of the road widening.

**Finding:** Impacts to the Lafayette Street and Gold Street Connector intersection would be reduced to the less than significant level with the incorporation of Mitigation Measure MM TRA-1.1. **(Less Than Significant with Mitigation Incorporated)**

**Facts in Support of Finding:** The Lafayette Street/Gold Street Connector would be significantly impacted by the Project under background plus Project conditions based on findings from applicable municipal and Santa Clara County's Congestion Management Program impact criteria.

The addition of a second northbound left-turn lane at this intersection in a vacant area between the Gold Street Connector and SR 237 was also identified as a mitigation measure for the approved City Place development in the City of Santa Clara. Traffic associated with the City Place development is included within background conditions of this study; however, since the City of San José has no authority of development within other jurisdictions or their development schedules, the Project applicant will be required to contribute a fair-share amount to the City of San José Public Works Depositors Fund for the construction of this improvement.

**Impact:** **Impact TRA-2:** Implementation of the proposed Project would have a significant impact on mixed-flow lanes and/or a high occupancy vehicle (HOV) lane during one or both peak hours on the following four freeway segments: eastbound SR 237 between Great America Parkway and North First Street, westbound SR 237 between I-880 and McCarthy Boulevard, westbound SR 237 between I-880 and McCarthy Boulevard, westbound SR 237 between McCarthy Boulevard and Zanker Road, and the HOV lane on westbound SR 237 between I-880 and McCarthy Boulevard.

**Mitigation:** None

**Finding:** No feasible mitigation was identified to reduce these traffic impacts to a less than significant level. **(Significant and Unavoidable Impact)**

**Facts in Support of Finding:** The Legacy Terrace FEIR identified impacts to eastbound SR 237 between Great America Parkway and North First Street, but did not identify impacts on the other freeway segments. Full mitigation of significant Project impacts on freeway segments would require roadway widening to construct additional through mixed-flow or high-occupancy vehicle (HOV) lanes. There are no feasible mitigation measures available to reduce impacts on local freeway study segments to a less than significant level as it is beyond the capacity of any one Project to acquire right-of-way and add lanes to a state freeway. Furthermore, no comprehensive Project to increase freeway capacity on SR 237 has been developed by Caltrans or VTA. Although VTA has Voluntary Mitigation Programs for impacts along SR 237, there are no specifically identified improvement projects to which to pay fair share fees. The proposed TDM measures, as outlined in Mitigation Measure MM AIR-1.1, would reduce the impacts identified under Impact TRA-2, but not to a less than significant level.

Implementation of the proposed Project would have a significant impact on freeway mixed-flow lanes and/or a high-occupancy vehicle (HOV) lane during one or both peak hours. Thus, these impacts would be significant and unavoidable.

### **Cumulative Impacts**

**Impact:** **Impact TRA(C)-1:** The proposed Project would result in a cumulatively considerable contribution to traffic impacts at two San José intersections (Lafayette Street and Gold Street Connector, and Great America Parkway and Eastbound SR 237) based on cumulative impact criteria.



**Mitigation: MM TRA(C)–1.1: Lafayette Street and Gold Street Connector - See MM TRA-1.1**

**MM TRA(C)-1.2: Great America Parkway and Eastbound SR 237 -** Improvements to the Great America Parkway/State Route 237 intersection include the addition of a third left-turn lane and second right-turn lane to the westbound approach to the intersection (SR 237 off-ramp), and would reduce the project and cumulative impact at this intersection to a less than significant level. These improvements are fully funded and will be constructed by the City of Santa Clara's City Place development, as a condition of approval.

**Finding:** Impacts to the Lafayette Street and Gold Street Connector intersection would be reduced to the less than significant level with the incorporation of Mitigation Measure MM TRA-1.1. **(Less than Significant Cumulative Impact with Mitigation)**

Impacts to the Great America Parkway and Eastbound SR 237 intersection would be reduced to the less than significant level with the incorporation of Mitigation Measure MM TRA(C)-1.2. **(Less than Significant Cumulative Impact with Mitigation)**

**Facts in Support of Finding:** Based on the intersection level of service analysis, two City of San José intersections would be cumulatively impacted during the AM peak hour. Mitigation Measure MM TRA-1.1, for the fair-share contribution to the improvements at the intersection of Lafayette Street and the Gold Street Connector (e.g., addition of a second northbound left-turn lane), would reduce the Project and cumulative impact at this intersection to a less than significant level.

The addition of a third left-turn lane and second right-turn lane to the westbound approach to the intersection (SR 237 off-ramp) was identified as a mitigation measure for the approved City Place development in the City of Santa Clara. Traffic associated with the City Place development is included within background conditions of this study; however, since the City of San José has no authority of development within other jurisdictions or their development schedules, the Project applicant will be required to contribute a fair-share amount to the City of San José Public Works Depositors Fund for the construction of this improvement.

## **FINDINGS CONCERNING ALTERNATIVES**

In order to comply with the purposes of CEQA, it is important to identify alternatives that reduce the significant impacts that are anticipated to occur if the project is implemented and to try to meet as many of the project's objectives as possible. The CEQA Guidelines emphasize a common sense approach -- the alternatives should be reasonable, should "foster informed decision making and public participation," and should focus on alternatives that avoid or substantially lessen the significant impacts.

The alternatives analyzed in the FSEIR were developed with the goal of being at least potentially feasible, given Project objectives and site constraints, while avoiding or reducing the Project's identified environmental effects. The following are evaluated as alternatives to the proposed project:

1. No Project – No Development Alternative
2. No Project – Develop Under Current PD Zoning Alternative
3. Reduced Intensity Alternative

#### **1. No Project – No Development Alternative**

**A. Description of Alternative:** Under the No Project – No Development Alternative, the buildings and parking lots existing and under construction at the site would remain. Building 5 would not be constructed. The site would remain as zoned and approved for Buildings 1 through 4 and the parking garage.

**B. Comparison of Environmental Impacts:** Under the No Project - No Development Alternative, the Project site would remain as it is, and all of the environmental impacts anticipated to occur under the proposed Project would be avoided.

**C. Finding:** The No Project - No Development Alternative would avoid the Project's significant unavoidable transportation and cumulative transportation impacts. This alternative would also avoid the other significant impacts resulting from the Project that would be reduced to a less than significant level with the incorporation of mitigation measures.

The No Project - No Development Alternative would meet some, but not all, of the proposed Project's specific objectives to provide jobs and provide a more efficient and economically productive use at the site, in that the existing America Center buildings accomplish these goals. However, the site would provide approximately 600 fewer jobs without the additional square footage proposed as part of the Project.

## **2. No Project – Develop Under Current PD Zoning Alternative**

- A. Description of Alternative:** The Project site is currently designated *Combined/Industrial Commercial* in the City's General Plan and is located within a Planned Development (PD) zoning district. Under the current PD zoning (PDC99-044), 900,000 square feet of development is allowed for the Commercial Office/R&D portion of the project site. Of that total, 867,762 square feet have been constructed or is currently under construction and 32,238 square feet of Commercial Office/R&D entitlement remains.

Under the current PD zoning, a 32,238-square-foot office building could be constructed. A potential project under the No Project – Develop Under Current PD Zoning Alternative would likely be one story and would occupy the footprint of the proposed Building 5. The building would likely not be visible as it would be shielded on all sides by existing, much taller structures. Pile driving would still be required for a smaller Building 5 and parking garage expansion.

- B. Comparison of Environmental Impacts:** The No Project – Develop Under Current PD Zoning Alternative would avoid the Project's identified transportation impacts. This alternative would still result in the same impacts to biological, cultural resources, geology, hazards materials, hydrology, and construction noise as the Project. Aesthetic impacts would be less than the proposed project as the building would be mostly shielded from views from SR 237 and recreation trails by taller surrounding structures.

- C. Finding:** This alternative would avoid the Project's transportation impacts; however, impacts to biological resources, cultural resources, geology, hazardous materials, hydrology, and noise will remain the same as the proposed Project.

The No Project–Develop Under Current PD Zoning Alternative would meet some of the Project objectives with development consistent with the General Plan and Alviso Master Plan, and would intensify the development of the site and utilize the closed landfill area. However, building a one-story 32,238-square-foot structure would not meet the efficiency goals of the large-scale development objective given the complexities of building on the closed Highway 237 Landfill site. Additionally, this alternative would result in approximately 500 fewer jobs on site than the proposed Project and would not meet project objectives related to creating buildings sizeable enough to attract large employers to the Alviso Community/San José.

### **3. Reduced Intensity Alternative**

- A. Description of Alternative:** A Reduced Intensity Alternative would increase the amount of office/R&D allowed on the site by 55,000 square feet, resulting in the construction of a new approximately 87,000-square foot, three-story Building 5 (assuming the same footprint as the proposed Project and use of the remaining 32,238 square feet of yet unbuilt but entitled office/R&D capacity from the previous PD zoning). The building would likely not be visible from off-site as it would be shielded on all sides by existing, taller structures that are approximately six-stories in height. Pile driving may still occur with the Reduced Intensity alternative, but it is unknown the extent of pile driving that might be necessary for a three-story structure.
- B. Comparison of Environmental Impacts:** The Reduced Intensity Alternative would avoid the Project's transportation impacts; however, it would still result in the same impacts to biological resources, cultural resources, geology, hazardous materials, hydrology, and construction noise as the Project, though these impacts would also be less than significant with mitigation. Aesthetically, a three-story building would be mostly shielded by taller surrounding structures approximately 90 feet and six-stories in height and would not substantially contribute to the identified impacts to views from SR 237 and trails in the Alviso area.
- C. Finding:** As the environmentally superior alternative, the Reduced Intensity Alternative would avoid the Project's significant transportation and cumulative transportation impacts; however, significant impacts to biological resources, cultural resources, geology, hazardous materials, hydrology, and construction noise would not be avoided. These impacts would remain less than significant with mitigation.

The Reduced Intensity Alternative would meet some of the Project goals, in that it would provide development consistent with the General Plan and Alviso Master Plan, and intensify the development of the site and utilize the closed landfill area. However, this alternative would not fully meet all of the Project objectives related to providing efficient, large-scale development on a former landfill site. Additionally, approximately 400 fewer jobs would be provided on site compared to the proposed Project. At half the square footage and height of the proposed Project, it would not fully meet objectives related to the efficiencies of large-scale development on a former landfill site.

## MITIGATION MONITORING AND REPORTING PROGRAM

Attached to this Resolution as Exhibit "A" and incorporated and adopted as part of this Resolution herein is the Mitigation Monitoring and Reporting Program ("MMRP") for the Project required under Section 21081.6 of the CEQA Statute and Section 15097(b) of the CEQA Guidelines. The MMRP identifies impacts of the Project, corresponding mitigation, designation for responsibility for mitigation implementation and the agency responsible for the monitoring action.

## STATEMENT OF OVERRIDING CONSIDERATIONS

- A. **Significant Unavoidable Impacts.** With respect to the foregoing findings and in recognition of those facts that are included in the record, the City has determined that the Project will result a significant unmitigated or unavoidable impacts, as set forth above, associated with aesthetics, air quality, noise, and transportation.
- B. **Overriding Considerations.** The City Council specifically adopts and makes this Statement of Overriding Considerations that this Project has eliminated or substantially lessened all significant effects on the environment where feasible, and finds that the remaining significant, unavoidable impacts of the Project are acceptable in light of the economic, legal, environmental, social, technological or other considerations noted below, because the benefits of the Project outweigh its significant adverse environmental impact of the Project. The City Council finds that each of the overriding considerations set forth below constitutes a separate and independent basis for finding that the benefits of the Project outweigh its significant adverse environmental impacts and is an overriding consideration warranting approval of the Project. These matters are supported by evidence in the record that includes, but is not limited to, the Envision San José 2040 General Plan, Alviso Master Plan, and the San José Commercial Design Guidelines.
- C. **Benefits of the Project.** The City Council has considered the public record of proceedings on the proposed Project and other written materials presented to the City as well as oral and written testimony at all public hearings related to the Project, and does hereby determine that implementation of the Project as specifically provided in the Project documents would result in the following substantial public benefits:
- **Increase Employment within San José.** The Project will advance goals of the Envision San José 2040 General Plan to increase the ratio

of jobs/employed residents to attain fiscal sustainability for the City. The Project will support San José's stated job creation and job retention objectives by allowing for the development of an approximately 192,350 square foot office building that would accommodate approximately 600 new jobs on the site.

The Project furthers redevelopment of an inactive landfill and associated former industrial and storage yard areas to a viable economic use in an economically developing area. Specifically, the Project will enhance building floor area capacity for various employment uses and provide near-term jobs to contribute to the City's long-term achievement of economic development and job growth goals.

- **Transportation Demand Management.** The Project has committed to implementing a Transportation Demand Management (TDM) program and making contributions toward transportation improvements, which will benefit the surrounding community in accordance with the City's policies, such as General Plan Policy TR-7.1, the Zoning Ordinance, and the Greenhouse Gas Reduction Strategy.
- **Increase Economic Development.** The Project will advance goals of the Envision San José 2040 General Plan by adding approximately 192,350 square feet of office development in the Alviso area. The Project will directly contribute to the tax base of the City through an increase in the assessed value of the Project property and will also contribute by sales tax revenues generated through businesses located on the Project site and provide economic benefits to the City.
- **General Plan Goals and Policies.** The Project supports a number of the General Plan's major strategies and encourages strategic growth by locating office development on employment lands adjacent to complementary other commercial/office facilities.
  - Major Strategy #4 Innovation/Regional Employment Center:  
The Project site is located within the Combined Industrial/Commercial designation in the Envision San José 2040 General Plan. The Project proposes the construction of new office buildings that would accommodate new jobs. It will increase the ratio of jobs/employed residents and provides greater flexibility for commercial activity. The Project will support job growth within the current America Center area, which contains existing office uses.

- Land Use and Employment Goal IE-1:

The Project will develop an approximately 192,350 square foot office building will contribute to the supply of jobs. It will contribute to a joint use of parking facilities for office, commercial and hotel uses within the America Center area. The Project will provide and enhance economic development and job growth in San José.

- Bay and Baylands Goal ER-3:

The Project will protect the 23.5 acres of permanent open space preserve established by the FEIR in 2000 between San Tomas Aquino Creek, the Guadalupe River, and the salt evaporation ponds. The open space preserve was established as mitigation for the Legacy Terrace Center project and provides important wildlife habitat in the South Bay. The Project will provide buffer areas and avoid direct or indirect impacts on riparian habitats because building and parking areas will be set back a minimum of 100 feet from the Guadalupe River and San Tomas Aquino Creek. The Project will preserve natural characteristics of the Bay and adjacent lands maintaining a healthy regional ecosystem.

The City Council has weighed each of the above benefits of the proposed Project against its unavoidable environmental risks and adverse environmental effects identified in the Final Subsequent Environmental Impact Report and hereby determines that those benefits outweigh the risks and adverse environmental effects of the Project and, therefore, further determines that these risks and adverse environmental effects are acceptable and overridden.

### **LOCATION AND CUSTODIAN OF RECORDS**

The documents and other materials that constitute the record of proceedings on which the City Council based the foregoing findings and approval of the Project are located at the City's Department of Planning, Building and Code Enforcement, City Hall, 200 East Santa Clara Street, 3<sup>rd</sup> Floor Tower, San José, California, 95113. The City Council hereby designates the City's Director of Planning, Building, and Code Enforcement at his office at 200 East Santa Clara Street, San José California 95113, as the custodian of documents and records of proceedings on which this decision is based.

//

ADOPTED this \_\_\_\_ day of \_\_\_\_\_, 2018, by the following vote:

AYES:

NOES:

ABSENT:

DISQUALIFIED:

---

SAM LICCARDO  
Mayor

ATTEST:

---

TONI J. TABER, CMC  
City Clerk



# **MITIGATION MONITORING AND REPORTING PROGRAM**

---

## **America Center Phase III Project Subsequent Environmental Impact Report File No. PDC15-058 and PD15-053 December 2017**

---



# PREFACE

Section 21081.6 of the California Environmental Quality Act (CEQA) requires a Lead Agency to adopt a Mitigation Monitoring and Reporting Program whenever it approves a project for which measures have been required to mitigate or avoid significant effects on the environment. The purpose of the monitoring and reporting program is to ensure compliance with the mitigation measures during project implementation.

The Subsequent Environmental Impact Report (SEIR) prepared for the America Center Phase III Project concluded that the implementation of the project could result in significant effects on the environment and mitigation measures were incorporated into the proposed project or are required as a condition of project approval. This Mitigation Monitoring and Reporting Program addresses those measures in terms of how and when they will be implemented.

This document does *not* discuss those subjects for which the SEIR concluded that the impacts from implementation of the project would be less than significant.

I, \_\_\_\_\_, the applicant, on the behalf of \_\_\_\_\_, hereby agree to fully implement the mitigation measures described below which have been developed in conjunction with the preparation of an SEIR for my proposed project. I understand that these mitigation measures or substantially similar measures will be adopted as conditions of approval with my development permit request to avoid or significantly reduce potential environmental impacts to a less than significant level, where feasible.

Project Applicant's Signature \_\_\_\_\_

Date \_\_\_\_\_

Adopted Mitigation Measures	MONITORING AND REPORTING PROGRAM					
	Documentation of Compliance (Applicant/Proponent Responsibility)			Documentation of Compliance (Lead Agency Responsibility)		
	Responsibility for Implementation	Method of Compliance or Mitigation Action	Timing of Compliance	Oversight Responsibility	Actions/Reports	Monitoring Timing or Schedule
<b>Air Quality</b>						
<b>Impact AIR-1:</b> The project would contribute to an impact as a result of exceedance of Bay Area Air Quality Management District standards for operational nitrogen oxide (NOx) emissions, as previously identified for the buildout of the America Center site in the Legacy Terrace Final Environmental Impact Report.						
<b>MM AIR-1.1:</b> The project applicant shall include the following updated measures from the Legacy Terrace FEIR: <ul style="list-style-type: none"> <li>• Provide physical improvements, such as sidewalk improvements, landscaping and bicycle parking which would encourage pedestrian and bicycle modes of travel;</li> <li>• Connect site with regional bicycle/pedestrian trail system;</li> <li>• Provide shuttle bus service to the Tasman/Lafayette light rail and Altamont Corridor Express (ACE) rail system; and</li> <li>• Implement other feasible transportation demand management (TDM) program measures; including a ride-matching program, guaranteed ride home programs, coordination with regional ride-sharing organizations, and a transit incentives program. The Project applicant shall submit a Transportation Demand Management (TDM) Plan to the satisfaction of the Transportation Manager of</li> </ul>	Project applicant	<p>Incorporate pedestrian and bicycle improvements and trails into building and landscape plans.</p> <p>Prepare and submit a TDM program that results in a 10 percent reduction in projected weekday mobile emissions. Submit annual TDM monitoring reports.</p>	<p>Prior to issuance of any building permits</p> <p>Prior to approval of a Planned Development Permit</p>	<p>Supervising Environmental Planner of the Department of Planning, Building, and Code Enforcement (PBCE)</p> <p>Transportation Manager of the Department of Public Works and the PBCE Supervising Environmental Planner</p>	<p>Review building and landscape plans</p> <p>Review proposed TDM program and annual TDM monitoring reports.</p>	<p>Prior to issuance of any building permits</p> <p>Prior to approval of a Planned Development Permit</p>

the Department of Public Works and the PBCE Supervising Environmental Planner prior to approval of a Planned Development Permit. The TDM Plan shall contain components or equivalent measures to result in a 10 percent reduction in projected weekday mobile emissions. The project will be required to submit an annual monitoring report to the Transportation Manager of the Department of Public Works and the PBCE Supervising Environmental Planner to measure the effectiveness of the TDM plan. Additional TDM measures may be required if the TDM measures are not effective.						
<b>Impact AIR-2:</b> Odors could occur as a result of drilling holes for support piles that penetrate the landfill cap and impact sensitive receptors in the area.						
<b>MM AIR-2.1:</b> The project applicant shall prepare and implement an odor-control plan prior to the onset of construction which includes the following odor-control elements: <ul style="list-style-type: none"> <li>• Scheduling of construction phasing such that the amount of uncovered/disturbed waste at one time is minimized;</li> <li>• Controlling odors by covering any exposed landfill material with soil, foam, or other suitable material (including application of deodorant or other odor-control materials);</li> <li>• Considering seasonal weather conditions that can concentrate odors or direct odors towards sensitive receptors; and</li> <li>• Providing the Summerset Mobile Estates residents and the Department of Planning, Building and Code Enforcement, with the name and phone number of a Project Contact who shall respond to any complaints about dust, odors, or other nuisances associated with waste excavation and relocation operations.</li> </ul>	Project applicant and construction crewmembers	Preparation of an odor-control plan and implementation during construction  Name and phone number of a Project Contact who shall respond to any complaints about dust, odors, or other nuisances	Prior to issuance of any building permits	City of San Jose, Local Enforcement Agency  Supervising Environmental Planner of the Department of Planning, Building, and Code Enforcement	Review and approval of the odor-control plan.  Coordination with Project Contact in responding to odor complaints	Prior to issuance of any building permits

## Biological Resources

**Impact BIO-1:** If present, construction activities could cause disturbance to birds nesting and foraging in the project area.

<p><b>MM BIO-1.1:</b> The project applicant shall implement the following measures to avoid impacts to nesting birds on and adjacent to the site during construction.</p> <ul style="list-style-type: none"> <li>To the extent feasible, construction activities shall be scheduled to avoid the nesting season. If construction activities are scheduled to occur outside the nesting season, all impacts on nesting birds protected under the Migratory Bird Treaty Act (MBTA) and California Fish and Game Code shall be avoided. The nesting season for most birds in Santa Clara County extends from February 1st to August 31st.</li> <li>If it is not possible to schedule construction activities between September 1st and January 31st then pre-construction surveys for nesting birds shall be conducted by a qualified ornithologist to ensure that no nests will be disturbed during project implementation. This survey shall be completed no more than 14 days prior to the initiation of construction activities during the early part of the breeding season (February 1st through April 30th) and no more than 30 days prior to the initiation of these activities during the late part of the breeding season (May 1st through August 31st). During this survey, the ornithologist shall inspect all trees and other potential nesting habitats (e.g., trees, shrubs, ruderal grasslands, buildings) in and immediately adjacent to the impact areas for nests. If an active nest is found sufficiently close to work areas to be disturbed by these activities, the ornithologist in consultation with California Department of Fish and Wildlife (CDFW), will determine the extent of a construction-free</li> </ul>	Project applicant	<p>Avoidance of construction activities during nesting seasons</p> <p>If avoidance of construction activities during nesting seasons is not feasible, a pre-construction nesting bird survey shall be conducted by a qualified biologist and construction-free buffer zones shall be designated around any discovered nest.</p> <p>Following completion of the preconstruction survey, the project applicant shall prepare a report to document the results of the survey and any designated construction-free buffer zones.</p>	Prior to issuance of any grading, demolition, and/or building permit	Supervising Environmental Planner of the City of San José Department of Planning, Building, and Code Enforcement	<p>Confirm that demolition and construction activities are scheduled outside of the avian nesting season.</p> <p>Review the preconstruction survey report indicating the results of the survey and any designated buffer-zones.</p>	Prior to issuance of any grading, demolition, and/or building permit.
-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------	-------------------	----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------	----------------------------------------------------------------------	------------------------------------------------------------------------------------------------------------------	-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------	-----------------------------------------------------------------------

<p>buffer zone to be established around the nest (typically 300 feet for raptors and 100 feet for other species) to ensure that no nests of species protected by the MBTA and California Fish and Game Code will be disturbed during project implementation.</p> <ul style="list-style-type: none"> <li>A report summarizing results of the pre-construction surveys and subsequent efforts to protect nesting raptors or birds (if found to be present) shall be submitted to the City of San José Supervising Environmental Planner.</li> </ul>						
<b>Impact BIO-2:</b> If present, construction activities could cause disturbance to burrowing owls nesting and foraging in the project area.						
<p><b>MM BIO-2.1:</b> The project applicant shall implement the following measures to avoid impacts to nesting or non-nesting burrowing owls on or immediately adjacent to the site, consistent with Condition 15 of Chapter 6 of the Santa Clara Valley Habitat Conservation Plan.</p> <ul style="list-style-type: none"> <li>Prior to any site disturbance, staging, or construction-related activities, a qualified biologist shall conduct burrowing owl preconstruction surveys in all suitable habitat areas on the project site and within 250 feet of all construction activity. The purpose of the preconstruction surveys is to document the presence or absence of burrowing owls on the project site and within 250 feet of construction activity in order to avoid direct impacts to burrowing owls. To maximize the likelihood of detecting owls, the preconstruction survey shall last a minimum of three hours. The survey shall begin one hour before sunrise and continue until two hours after sunrise (three hours total) or begin two hours before sunset and continue until one hour after sunset. Additional time may be required for large project sites. A minimum of two surveys shall</li> </ul>	Project applicant and qualified biologist	<p>Conduct preconstruction surveys in all suitable habitat areas on the project site and within 250 feet of all construction activity</p> <p>Maintain a 250-foot buffer zone around occupied burrow(s) as determined by a qualified biologist</p>	<p>Prior to any site disturbance, staging, or construction-related activities</p> <p>Surveys shall conclude no more than two calendar days prior to site disturbance, staging, or construction-related activities.</p>	<p>Supervising Environmental Planner of the Department of Planning, Building, and Code Enforcement</p> <p>CDFW</p>	<p>Review the preconstruction survey report indicating the results of the survey and any designated buffer-zones.</p>	<p>Prior to issuance of any grading, demolition, and/or building permits</p>

<p>be conducted (if owls are detected on the first survey, a second survey is not needed). All owls observed shall be counted and their locations mapped.</p> <ul style="list-style-type: none"> <li>• Surveys shall conclude no more than two calendar days prior to site disturbance, staging, or construction-related activities. Therefore, the project applicant must begin surveys no more than four days prior to construction (two days of surveying plus up to two days between surveys and construction). To avoid last-minute changes in schedule or contracting that may occur if burrowing owls are found, the project applicant may also conduct a preliminary survey up to 14 days before construction. This preliminary survey may count as the first of the two required surveys as long as the second survey concludes no more than two calendar days in advance of construction.</li> <li>• If burrowing owls are present during the nonbreeding season (September 1st to January 31st), a 250-foot buffer zone shall be maintained around the occupied burrow(s) as determined by a qualified biologist, if feasible. If maintaining such a buffer is not feasible, then the buffer must be great enough to avoid injury or mortality of individual owls based on monitoring results. During the breeding season (generally February 1st to August 31st), a 250-foot buffer, within which no newly initiated project-related activities shall be permissible, shall be maintained between project activities and occupied burrows. Owls present between February 1st and August 31st will be assumed to be nesting, and the 250-foot protected area shall remain in effect until August 31st. If monitoring evidence indicates that the owls are no longer nesting or the young owls are foraging independently, the buffer may be</li> </ul>						
-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------	--	--	--	--	--	--

<p>reduced based on monitoring results, in consultation with the City and CDFW.</p> <ul style="list-style-type: none"> <li>If nesting owls are determined to be present on the site, and project activities cannot feasibly avoid disturbance of the area within 250 feet of the occupied burrow during the nesting season (i.e., February 1st through August 31st) due to other seasonal constraints, a qualified biologist shall be present during all activities within 250 feet of the nest to monitor the owls' behavior. If, in the opinion of the qualified biologist, the owls are unduly disturbed (i.e., disturbed to the point of harm or reduced reproductive success), all work within 250 feet of the occupied burrow will cease until the nest is determined to no longer be active by a qualified biologist.</li> </ul>						
<b>Geology and Soils</b>						
<b>Impact GEO-1:</b> Differential settlement could result in structural damage to the proposed development.						
<p><b>MM GEO-1.1:</b> The project applicant shall complete a design-level geotechnical investigation for the project site prior to issuance of any grading permits for individual site improvements to address the potential geologic hazards. Design-level engineering studies shall be submitted to the City's Public Works Department for review and approval.</p> <p>Building foundations shall be designed based on this geotechnical investigation. Building loads shall be supported on driven pile foundations as appropriate to support the building loads without significant damage due to settlement. Foundation piles shall be designed to accommodate downdrag loads caused by the subsidence of landfill materials due to the natural degradation of landfill components, and under the weight of the final cover and/or soil placed on the site for development purposes. In addition, a reinforced concrete "skirt wall" around the perimeter of each building shall be</p>	Project applicant	Preparation of a geotechnical investigation, design-level engineering studies, and a settlement map	<p>Prior to issuance of any grading permit</p> <p>Post-construction grade monitoring for settling</p>	Public Works Department	The geotechnical investigation design-level engineering studies, and updated settlement map	<p>Prior to issuance of any building permit</p> <p>Site grades shall be monitored every three months for the first two years. After two years, the monitoring duration shall be reevaluated based on the settlement rates and site characteristics.</p>



<p>installed to resist lateral loads placed on the building during a seismic event. Settlement adjacent to the skirt wall shall be monitored and soil replaced to avoid the loss of lateral support as fill settles.</p> <p>Conceptual foundation installation procedures in the South Campus area include: predrilling each pile location to the full depth of the landfill (50 to 60 feet); and/or installing a cased shaft at each pile location from the ground surface to the bottom of the landfill, removing landfill materials from the cased hole, and driving the foundation piles through the casing into the supporting soil below the landfill. Alternatively, non-displacement type piles, such as steel H-piles, could be driven directly through landfill materials. Corrosion control measures to protect steel and/or concrete piles shall be included in the design-level geotechnical investigation.</p> <p><b>MM GEO-1.2:</b> An updated settlement map shall be prepared based upon site monitoring and additional surveys prior to the completion of the design-level geotechnical investigation. The updated settlement map shall confirm appropriate post settlement grades on the site. The map shall be provided to the City of San José Public Works Department for review and approval.</p> <p><b>MM GEO-1.3:</b> To allow for settlement between structures and the surrounding ground at building entrances, "hinged slabs" or interlocking pavers shall be used. For hinged slabs, one end of the hinged slab will be fixed to the pile-supported structure and the other end will rest in the earth fill that will settle with time. The design of the hinged slab shall be based on the maximum operation slope of the slab. For pedestrian slabs, the estimated finished grade after settlement shall be based on a maximum slope required by the</p>						
------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------	--	--	--	--	--	--

<p>Americans with Disabilities Act. For vehicular slabs, the estimated finished grade after settlement shall be based on a maximum gradient differential of 11 percent between the slab and the stationary foundation, which allow use of the parking structure entrances without scraping the bottom of vehicles. Alternatively, interlocking pavers installed at building entrances can be easily adjusted to grade after settlement has occurred. Pavers shall be monitored at more frequent intervals than hinged slabs and regraded at regular intervals to avoid tripping hazards. The design of hinged slabs or pavers shall be completed using the current settlement map for the site.</p> <p><b>MM GEO-1.4:</b> Roadways and other paving systems shall utilize flexible materials such as asphaltic concrete, interlocking paving units, and avoid or limit the use of Portland cement concrete and other non-flexible materials. Where concrete is utilized, adequate expansion and spacing joints shall be used to accommodate differential settlement. Geotextile fabric or other materials shall be placed below the subgrade base section to provide bridging over localized “soft” areas determined by the geotechnical engineer during compaction of the fill material. Joints shall be adequately sealed between differing materials (i.e., asphalt and concrete curbs) to prevent water infiltration.</p> <p><b>MM GEO-1.5:</b> Pavements and other surface improvements shall be designed with adequate slope so that after settlement, reversals of stormwater flow direction or adverse flattening of the roadway pavement surface does not occur.</p> <p><b>MM GEO-1.6:</b> On-site utilities which operate via gravity shall be designed based upon the anticipated settlement on the site. These utilities shall be designed with adequate slope so that after</p>						
----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------	--	--	--	--	--	--

<p>settlement, reversal or flattening of the slope of utility lines does not occur.</p> <p><b>MM GEO-1.7:</b> Pipe materials which can accommodate differential settlement without separation of pipe joints or leakage shall be used on the site. Piping could utilize high density polyethylene or, in some cases, dual contained polyvinyl chloride pipe. For either type of pipe system, metallic fittings, valves, and flexible connections could be housed inside vaults for corrosion protection and to aid leak detection.</p> <p><b>MM GEO-1.8:</b> Under slab utilities, shall be connected to the structural slab using hangers constructed of a non-corrosive material, such as stainless steel. To counter the effect of soil in the utility trench settling and dislocating the utility line from the hanger, a non-cohesive backfill, such as pea gravel, shall be used in the trench. As the ground settles, the non-cohesive backfill shall be able to move around the pipe. Alternatively, no backfill shall be placed in the trench, with plywood or other materials being used to prevent concrete from the structural slab pour from entering the utility trench. As the surrounding ground settles, the utility pipe would be supported by hangers.</p> <p><b>MM GEO-1.9:</b> To accommodate the difference in settlement between the building and surrounding ground, flexible utility connections contained within a settlement vault shall be employed.</p>						
<p><b>MM GEO-1.10:</b> The project applicant shall prepare and implement an Operations and Maintenance Program for the building, utilities, and pavement, and shall include a site grade monitoring schedule. Site grades shall be monitored every three months for the first two years. After two years, the monitoring duration shall be reevaluated based on the settlement rates and site</p>	Project applicant	Preparation and implementation of an Operations and Maintenance Program	Prior to issuance of any grading permit	Public Works Department	Review and approval of the Operations and Maintenance Program	Prior to issuance of any building permit

characteristics. The Operations and Maintenance Program shall specify the types of repairs to be made in the event that indications of localized depressions, slope changes or cracking of pavements are found.						
Hazards and Hazardous Materials						
<b>Impact HAZ-1:</b> Methane gas within landfill materials could result in flammable or explosive conditions. Toxic compounds, including volatile organic compounds, and asbestos, could be present and pose health risks to construction workers and/or the public.						
<b>MM HAZ-1.1:</b> The project applicant shall follow the U.S. Environmental Protection Agency (EPA)-approved Soil Management Plan (SMP) developed for closure, capping, maintenance, and post-capping construction activities at the Highway 237 Landfill to reduce or eliminate exposure risk to human health and the environment, specifically, potential risks associated with the presence of methane, hydrogen sulfide, and volatile organic compounds (VOCs) in soil gas and potential unknown conditions. The SMP shall be followed for any development related activities that penetrate the low-permeability layer of the landfill cap, such as pile installation or excavations. As required by the SMP, prior notification of planned activities that trigger implementation of SMP protocols shall be provided to the Department of Planning, Building and Code Enforcement, Environmental Services Department, Local Enforcement Agency (LEA), and other regulatory agencies providing oversight (such as the Regional Water Quality Control Board [RWQCB] and CalRecycle) prior to issuance of a grading permit.  <b>MM HAZ-1.2:</b> Contractors and subcontractors at the project site shall develop a Health and Safety Plan specific to their scope of work and based upon the known environmental conditions for the site.	Project applicant and contractors, Site Safety and Health Officer	Implementation of the U.S. EPA-approved SMP; preparation and implementation of project Health and Safety Plan(s).	Prior to issuance of any grading permit; During construction	Supervising Environmental Planner of the Department of Planning, Building, and Code Enforcement, Environmental Services Department, LEA, RWQCB, and CalRecycle	Preparation and implementation of the SMP and Health and Safety Plan	Prior to issuance of any grading permits  As specified within the approved SMP during construction and operation

<p>Each health and safety plan shall be implemented under the direction of a Site Safety and Health Officer and provided to all regulatory agencies providing oversight (such as the LEA, CalRecycle, or RWQCB).</p> <p><b>MM HAZ-1.3:</b> The project site is on a former landfill and shall follow environmental monitoring procedures required by CalRecycle and the LEA.</p>						
<p><b>MM HAZ-1.4:</b> The project applicant shall ensure that where an irrigation system is installed for landscaping it shall be designed to optimize watering using the most current automatic irrigation equipment and monitoring methods. To help minimize infiltration, subdrains shall be constructed for all tree planting areas. The trees shall have subdrains that discharge to the storm drain system.</p>	Project applicant	Incorporation of an irrigation system into the project landscape and utility plans	Prior to issuance of any building permits	Supervising Environmental Planner of the Department of Planning, Building, and Code Enforcement	Review of the project landscape and utility plans	Prior to issuance of any building permits
<p><b>MM HAZ-1.5:</b> Prior to issuance of any grading permit for site improvements, the project applicant shall provide the LEA, CalRecycle, and an appropriate oversight agency (such as the DTSC or RWQCB and the City's Department of Public Works) with a project-level, engineering analysis that addresses, in sufficient detail, the following elements of the final project design:</p> <ul style="list-style-type: none"> <li>• Soil gas mitigation and monitoring systems, including structure monitoring and perimeter monitoring systems;</li> <li>• Differential settlement;</li> <li>• Site surface drainage and final grading; and</li> <li>• Any other elements of the design as required by the LEA or Department of Public Works, including specialized analysis that may be warranted by the City. The project applicant shall bear the responsibility for providing any such specialized analysis.</li> </ul>	Project applicant	Preparation of a project-level engineering analysis, and incorporation into building permit plans	Prior to issuance of any grading permits	Supervising Environmental Planner of the Department of Planning, Building, and Code Enforcement, Environmental Services Department (as the LEA), Department of Public Works, RWQCB, and CalRecycle	Review of project-level engineering analysis and building permit plans	Prior to issuance of any grading permits

<p><b>MM HAZ-1.6:</b> The project applicant shall incorporate a landfill gas control system into all buildings constructed as a part of the project. Proposed structures shall be constructed with a sub-slab soil gas mitigation system to vent landfill gases and other soil vapor. The soil gas mitigation system may consist of perforated pipes placed in a permeable granular layer under building and garage concrete slabs. The perforated pipes shall be connected to a system that discharges vapor to the building's exterior. The system shall include a methane sensor/venting system that is capable of venting soil vapor out from beneath the building, and a low-permeable barrier layer, such as Liquid Boot, shall be installed in the buildings and certain areas in the parking garage that have the potential to accumulate landfill gas in order to prevent soil vapors from intruding into the structures. The low-permeable vapor barrier membrane shall be located above the permeable granular layer. The membrane shall be sealed around foundation piles, grade beams, and slab penetrations (such as utility lines). The project applicant shall also install a landfill gas venting system beneath hardscape areas near the proposed structures.</p> <p>The landfill gas control system for the office buildings shall include an integrated methane sensor/blower system that is capable of actively drawing soil vapor from beneath the building. Methane sensors provided at the discharge point of each soil gas mitigation system shall control the active operation of the venting system.</p> <p>The project applicant shall prepare and implement an Operations and Maintenance Program for the soil gas mitigation systems. The Plan shall include</p>	Project applicant	<p>Incorporation of control systems and utility trenches into building permit plans</p> <p>Operations and Maintenance Program for the soil gas mitigation systems</p>	Prior to issuance of any building permits	Supervising Environmental Planner of the Department of Planning, Building, and Code Enforcement, Environmental Services Department (as the LEA)	Review of building permit plans and Operations and Maintenance Program	Prior to issuance of any building permits
------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------	-------------------	-----------------------------------------------------------------------------------------------------------------------------------------------------------------------	-------------------------------------------	-------------------------------------------------------------------------------------------------------------------------------------------------	------------------------------------------------------------------------	-------------------------------------------

instructions for how to ensure that the system functions properly.  <b>MM HAZ-1.7:</b> The project applicant shall construct site utility trenches with landfill gas cut-offs to prevent landfill gas from migrating along utility trenches. Below-grade electrical facilities shall be designed for explosive conditions, in accordance with the California Building Standards Code.						
<b>Noise</b>						
<b>Impact NOI-1:</b> Pile driving noise generated in the eastern portion of the site could impact residents at Summerset Mobile Estates and commercial uses along Gold Street in the short-term.						
<b>MM NOI-1.1:</b> Prior to the start of construction, the project applicant shall prepare and implement a noise logistics plan to reduce construction noise levels as low as practical. The noise logistics plan shall be submitted to the Supervising Environmental Planner of the Planning, Building and Code Enforcement Department for review and approval. The noise logistics plan would include, but not be limited to, the following measures: <ul style="list-style-type: none"> <li>• Construction hours within 500 feet of residential uses will be limited to the hours of 7:00 a.m. and 7:00 p.m. weekdays, with no construction on weekends or holidays. Pile driving shall be limited to the hours of 8:00 a.m. to 5:00 p.m. Monday through Friday.</li> <li>• Utilize ‘quiet’ models of air compressors and other stationary noise sources where technology exists.</li> <li>• Equip all internal combustion engine-driven equipment with mufflers, which are in good condition and appropriate for the equipment.</li> <li>• Locate all stationary noise-generating equipment, such as air compressors and portable power generators, as far away as</li> </ul>	Project applicant, construction crewmembers	Preparation of a noise logistics plan and implementation during construction.  Prepare a detailed construction plan identifying a schedule of major noise generating construction activities, and identify a noise control disturbance coordinator and procedure for coordination with nearby communities.	Prior to issuance of any grading permits	Supervising Environmental Planner of the Department of Planning, Building, and Code Enforcement	Review of the noise logistics plan	Prior to issuance of any grading permits

<p>possible from adjacent land uses.</p> <p>Locate staging areas and construction material areas as far away as possible from adjacent land uses.</p> <ul style="list-style-type: none"> <li>• Prohibit all unnecessary idling of internal combustion engines.</li> <li>• If impact pile driving is proposed, multiple-pile drivers shall be considered to expedite construction. Although noise levels generated by multiple pile drivers would be higher than the noise generated by a single pile driver, the total duration of pile driving activities would be reduced.</li> <li>• If impact pile driving is proposed, temporary noise control blanket barriers shall shroud pile drivers or be erected in a manner to shield the adjacent land uses. Such noise control blanket barriers can be rented and quickly erected.</li> <li>• The contractor shall prepare a detailed construction plan identifying a schedule of major noise generating construction activities. This plan shall identify a noise control disturbance coordinator and procedure for coordination with the adjacent noise sensitive facilities so that construction activities can be scheduled to minimize noise disturbance. This plan shall be made publicly available for interested community members. The disturbance coordinator shall be responsible for responding to any local complaints about construction noise. The disturbance coordinator will determine the case of the noise complaint and will require that reasonable measures warranted to correct the problem be implemented. The telephone number for the disturbance coordinator at the construction site shall be posted and included in the notice sent</li> </ul>						
-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------	--	--	--	--	--	--



to residences and commercial businesses within 100 feet regarding the construction schedule.						
<b>Transportation and Traffic</b>						
<b>Impact TRA-1:</b> The added trips as a result of the proposed project would cause the critical-movement delay to increase by four or more seconds and the demand-to-capacity ratio to increase by 0.01 or more at the Lafayette Street and Gold Street Connector intersection (City of San José's jurisdiction) during the AM peak hour.						
<p><b>MM TRA-1.1:</b> Prior to Public Works Clearance, the project applicant shall pay a fair share amount towards improvements (including full cost for design, construction, etc.) to the Lafayette Street and Gold Street Connector intersection. Improvements would include with the addition of a second northbound left-turn lane in a vacant area between the Gold Street Connector and SR 237. The improvement shall require widening of the Gold Street Connector and shifting of travel lanes to the south by approximately 12 feet to accommodate a second receiving lane for the second northbound left-turn lane. The roadway widening would also require the relocation of the Highway 237 Bikeway, south of the Gold Street Connector. The Director of Public Works shall determine the fair share based on the cost of the improvement at the time the payment is due and the project's contribution to the impact (an estimated 5 percent). The fair share amount shall be paid to the City of San José Public Works Depositors Fund.</p> <p>During construction of the intersection improvement, a trail detour shall be provided and/or the Highway 237 Bikeway relocated prior to construction of the road widening.</p>	Project applicant	Payment of fair share amount to the City of San José Public Works Depositors Fund, as determined by City of San José and in coordination with City of Santa Clara.	Prior to issuance of Public Works Clearance	Transportation Manager of the Department of Public Works  Supervising Environmental Planner of the Department of Planning, Building, and Code Enforcement	Determine fair share based on the cost of the improvement at the time the payment is due and the project's contribution to the impact. Confirm transfer to City of Santa Clara.	Prior to issuance of Public Works Clearance  Transfer payment to the City of Santa Clara upon issuance of improvement plan permit.

<b>Impact TRA(C)-1:</b> The proposed project would result in a cumulatively considerable contribution to traffic impacts at two San José intersections (Lafayette Street and Gold Street Connector, and Great America Parkway and Eastbound SR 237) based on cumulative impact criteria.	
<b>MM TRA (C)-1.1: Lafayette Street and Gold Street Connector</b> - See MM TRA-1.1.	See implementation of MM TRA-1.1, above
<b>MM TRA (C)-1.2: Great America Parkway and Eastbound State Route(SR) 237</b> - Improvements to the Great America Parkway/State Route 237 intersection include the addition of a third left-turn lane and second right-turn lane to the westbound approach to the intersection (SR 237 off-ramp), and would reduce the project and cumulative impact at this intersection to a less than significant level. These improvements are fully funded and will be constructed by the City of Santa Clara's City Place development, as a condition of approval.	Implemented by the City of Santa Clara's City Place development, as a condition of approval.

Source: City of San José. America Center Phase III, Subsequent Environmental Impact Report. June 2017.

**DRAFT**

ORDINANCE NO. \_\_\_\_\_

**AN ORDINANCE OF THE CITY OF SAN JOSE REZONING CERTAIN REAL PROPERTY OF APPROXIMATELY 63 GROSS ACRES SITUATED AT THE TERMINUS OF AMERICA CENTER DRIVE, (APNs: 015-45-013, 024, 032, 048, 049, 050, 051, 051, 052, & 053) FROM THE A(PD) PLANNED DEVELOPMENT ZONING DISTRICT TO A(PD) PLANNED DEVELOPMENT ZONING DISTRICT**

**WHEREAS**, all rezoning proceedings required under the provisions of Chapter 20.120 of Title 20 of the San José Municipal Code have been duly had and taken with respect to the real property hereinafter described; and

**WHEREAS**, a Subsequent Environmental Impact Report (SEIR) to the Legacy Terrace Development Planned Development Rezoning and Prezoning Final EIR certified by the City Council on February 15, 2000, by Resolution No. 69392 all in conformance with the California Environmental Quality Act of 1970 (CEQA), as amended, was prepared and approved by the City Council for the subject rezoning; and

**WHEREAS**, the City Council of the City of San José is the decision-making body for the proposed subject rezoning to A(PD) Planned Development Zoning District; and

**WHEREAS**, this Council of the City of San José has considered and approves the application and use of said SEIR and related Mitigation Monitoring and Reporting Program as the appropriate environmental clearance for this proposed project prior to taking any approval actions on this project;

**NOW, THEREFORE**, BE IT ORDAINED BY THE COUNCIL OF THE CITY OF SAN JOSE:

**SECTION 1.** The recitals above are incorporated herein.

**SECTION 2.** All that real property hereinafter described in this section, hereinafter referred to as "subject property," is hereby rezoned as A(PD) Planned Development Zoning District. The base district zoning of the subject property shall be the Agricultural Zoning District. The Planned Development Zoning of the subject property shall be that development plan for the subject property entitled, "General Development Plan – Exhibit C, dated received on December 20, 2017 ("General Development Plan").

Said General Development Plan is on file in the office of the Director of Planning and is available for inspection by anyone interested therein, and said General Development Plan is by this reference adopted and incorporated herein the same as if it were fully set forth herein.

The subject property referred to in this section is all that real property situated in the County of Santa Clara, State of California, described in Exhibit "A" attached hereto and incorporated herein by this reference.

**SECTION 3.** The district map of the City is hereby amended accordingly.

**SECTION 4.** The land development approval that is the subject of City File No. PDC15-058 is subject to the operation of Part 2.75 of Chapter 15.12 of Title 15 of the San José Municipal Code. The applicant for or recipient of such land use approval hereby acknowledges receipt of notice that the issuance of a building permit to implement such land development approval may be suspended, conditioned or denied where the City Manager has determined that such action is necessary to remain within the aggregate operational capacity of the sanitary sewer system available to the City of San José or to meet the discharge standards of the sanitary sewer system imposed by the California

Regional Water Quality Control Board for the San Francisco Bay Region.

PASSED FOR PUBLICATION of title this \_\_\_\_\_ day of \_\_\_\_\_, 2018 by the following vote:

AYES:

NOES:

ABSENT:

DISQUALIFIED:

---

SAM LICCARDO  
Mayor

ATTEST:

---

TONI J. TABER, CMC  
City Clerk

RESOLUTION NO. \_\_\_\_\_

**A RESOLUTION OF THE COUNCIL OF THE CITY OF SAN JOSE APPROVING, SUBJECT TO CONDITIONS, A PLANNED DEVELOPMENT PERMIT TO CONSTRUCT A SIX-STORY, 192,350 SQUARE FOOT OFFICE BUILDING, PARKING STRUCTURE ADDITION TO ACCOMMODATE A TOTAL OF 1,870 PARKING SPACES, LANDSCAPING AND ASSOCIATED SITE IMPROVEMENTS, ON AN APPROXIMATELY 63 GROSS ACRE SITE LOCATED AT THE EAST SIDE OF AMERICA CENTER DRIVE, APPROXIMATELY 500 FEET NORTH OF STATE ROUTE 237 (0 AMERICA CENTER DRIVE, APN: 015-45-044)**

**FILE NO. PD15-053**

**WHEREAS**, pursuant to the provisions of Chapter 20.100 of Title 20 of the San José Municipal Code, on October 29, 2015, an application (File No. PD15-053) was filed by the applicant, SteelWave LLC, with the City of San José for a Planned Development Permit to construct a 192,350 square new office building, an addition to the existing parking structure to accommodate a total of 1,870 parking spaces, landscaping and associated site improvements, on that certain real property situated in the A(PD) Agriculture Planned Development Zoning District and located at the east side of America Center Drive, approximately 500 feet northerly of State Route 237 (0 America Center Drive, APN: 015-45-044, San José, which real property is sometimes referred to herein as the “subject property”); and

**WHEREAS**, the subject property is all that real property more particularly described in Exhibit "A," entitled “Legal Description,” which is attached hereto and made a part hereof by this reference as if fully set forth herein; and

**WHEREAS**, pursuant to and in accordance with Chapter 20.100 of Title 20 of the San José Municipal Code, the Planning Commission conducted a hearing on said application on January 10, 2018, notice of which was duly given; and

**WHEREAS**, at said hearing, the Planning Commission gave all persons full opportunity to be heard and to present evidence and testimony respecting said matter; and

**WHEREAS**, at said hearing, the Planning Commission made a recommendation to the City Council respecting said matter based on the evidence and testimony; and

**WHEREAS**, pursuant to and in accordance with Chapter 20.100 of Title 20 of the San José Municipal Code, this City Council conducted a hearing on said application, notice of which was duly given; and

**WHEREAS**, at said hearing, this City Council gave all persons full opportunity to be heard and to present evidence and testimony respecting said matter; and

**WHEREAS**, at said hearing this City Council received and considered the reports and recommendations of the City's Planning Commission and City's Director of Planning, Building and Code Enforcement; and

**WHEREAS**, at said hearing, this City Council received in evidence a development plan for the subject property entitled, "America Center," dated received on December 20, 2017, said plan is on file in the Department of Planning, Building and Code Enforcement and is available for inspection by anyone interested herein, and said plan is incorporated herein by this reference, the same as if it were fully set forth herein; and

**WHEREAS**, said public hearing before the City Council was conducted in all respects as required by the San José Municipal Code and the rules of this City Council; and

**WHEREAS**, this City Council has heard and considered the testimony presented to it at

the public hearing, and has further considered written materials submitted on behalf of the project applicant, City staff, and other interested parties;

**NOW, THEREFORE,** BE IT RESOLVED BY THE COUNCIL OF THE CITY OF SAN JOSE THAT:

After considering all of the evidence presented at the Public Hearing, the City Council finds that the following are the relevant facts regarding this proposed project:

1. **Site Description and Surrounding Uses.** The project site is a partially developed 63.7 gross-acre site, known as the America Center site, which is currently developed with two occupied office buildings, a hotel, two additional office buildings under construction, a parking structure with 800 parking spaces, the connector roads and surface parking lots with 2,124 parking spaces. The area north of the site is designated as Open Space Preserve pursuant to the original zoning Planned Development Zoning File No. PDC99-044. The site is bordered by State Route 237 to the south, the San Tomas Aquino Creek to the west, salt evaporation ponds along the edge of the San Francisco Bay to the northwest, and the Southern Pacific railroad tracks to the east. Across San Tomas Aquino Creek is a wetlands mitigation site. There is an extended-stay hotel and two office buildings to the east of the Union Pacific Railroad (UPRR) train tracks.
2. **Project Description.** On October 29, 2015, the following applications were filed:
  - Planned Development Rezoning (PDC15-058): The proposed Planned Development Rezoning would maintain the existing designation of A(PD) Planned Development Zoning District and would include the removal of the approximately 6.7 acre River Commercial area adjacent to the Guadalupe River/Alviso Slough which was covered under a separate Planned Development Zoning (PDC15-016), approved in March 2016; adjustment in the boundaries for the remaining land use areas to reflect minor lot line adjustments; and an increase in the allowed commercial office/R&D land use of 190,000 square feet. The existing allowed height of 90 feet will remain in place; **and**
  - Planned Development (PD15-053): The six-story office building (Building 5) would be up to 83 feet tall at the top of roof, and would contain approximately 192,350 square feet of floor space. Additional parking would be accommodated through a five-story expansion of the previously approved parking structure resulting in a total of 1,870 structured parking spaces and 1,740 surface parking spaces. Onsite circulation will remain the same, with the main access point located at the southern end of the site.



3. **General Plan Conformance.** The Project has an Envision San José 2040 General Plan Land Use/Transportation Diagram designation of Combined Industrial/Commercial. This land use designation is intended for a wide variety of commercial, office, industrial developments, or a compatible mix of these uses. The project is also consistent with the following General Plan policies:

1. Land Use Policy LU-8.1: In areas that are designated for mixed industrial and commercial uses, allow only commercial uses that are compatible with industrial uses. Non-employment uses are prohibited in these areas.
2. Land Use Policy LU-8.2: Encourage more large-format commercial uses in Combined Industrial/ Commercial lands, since these development typologies are typically similar to the development scale of industrial development in the same area. Discourage small-scale and strip commercial shopping centers in the Combined Industrial/ Commercial area.

*Analysis: The proposed Rezoning and PD Permit would accommodate an additional 190,000 square feet for Building 5 and the expansion of the parking structure would accommodate the additional required parking. The requested changes would make for a larger single or multi user campus. The additional square footage will provide office space and employment opportunities. The proposed building and parking are compatible with the existing buildings and other commercial uses on the site.*

3. Environmental Resources Policy ER-2.1: Ensure that new public and private development adjacent to riparian corridors in San José are consistent with the provisions of the City's Riparian Corridor Policy Study and any adopted Santa Clara Valley Habitat Plan/ Natural Communities Conservation Plan.
4. Environmental Resources Policy ER-2.2: Ensure that a 100-foot setback from riparian habitat is the standard to be achieved in all but a limited number of instances, only where no significant environmental impacts would occur.
5. Environmental Resources Policy ER-2.3: Design new development to protect adjacent riparian corridors from encroachment of lighting, exotic landscaping, noise and toxic substances into the riparian zone.
6. Environmental Resources Policy ER-4.4: Require that development projects incorporate mitigation measures to avoid and minimize impacts to individuals of special-status species.
7. Environmental Resources Policy ER-5.1: Avoid implementing activities that result in the loss of active native birds' nests, including both direct loss and indirect loss through abandonment, of native birds. Avoidance of activities that could result in impacts to nests during the breeding season or maintenance of buffers between such activities and active nests would avoid such impacts.
8. Environmental Resources Policy ER-5.2: Require that development projects

incorporate measures to avoid impacts to nesting migratory birds.

9. Environmental Resources Policy ER-6.5: Prohibit use of invasive species, citywide, in required landscaping as part of the discretionary review of proposed development.
10. Environmental Resources Policy ER-7.1: In the area north of Highway 237 design and construct buildings and structures using bird friendly design and practices to reduce the potential for bird strikes for species associated with the Baylands or the riparian habitats of lower Coyote Creek.

*Analysis: The project would respect the 100-foot riparian setback specified within of General Plan Policy ER-2.1, ER-2.2, ER-2.3 and Council Policy 6-34. Landscape plans would be in compliance with Policy ER-6.5. Mitigation measures MM BIO-1.1 and MM BIO-1.2 would be implemented consistent with General Plan Policy ER-4.4, ER-5.1, and ER-5.2. Bird-safe design features are included in the building and are reviewed by city staff as part of the overall design review process for consistency with City Council Policy 6-34 and Policy ER-7.1.*

4. **Alviso Master Plan – Land Use:** The site is designated Combined Industrial/Commercial by the Alviso Master Plan, which allows for a wide range of industrial uses, including warehousing, office, research and development, and light manufacturing. This land use designation also allows for retail, restaurant, hotel, or other commercial establishments. The proposed project fits into the parameters of this land use designation. The project is also consistent with several Objectives, Goals and Policies of the Alviso Master Plan.

- a) Industrial/Non-Industrial Relationships Objective: Setbacks and buffers should be established to protect environmental resources (e.g., Coyote Creek) and "sensitive uses" (e.g., residential, day care, and school uses) from potential negative impacts of industrial use.

*Analysis: The proposed Rezoning and PD Permits maintain the adopted setbacks and established open space areas included in the original Planned Development Zoning. The proposed Building 5 location is located at the interior of the site, between existing buildings. The proposed parking garage expansion is located along the existing rail line and away from designated open space areas.*

- b) River Orientation Policy 3: New Buildings adjacent to the Guadalupe River/Alviso Slough should be of an appropriate scale and character to enhance this waterway as a public-oriented recreation resource and as a natural riparian corridor.

*Analysis: The Alviso Master Plan has a height limit in this area of 90 feet, which the Rezoning is consistent with and the PD Permit further limits the height to approximately 83 feet. As noted above, the building and garage locations are located at the interior of the site or adjacent to the existing rail line, away from existing waterways.*

- c) Industrial/Non-Industrial Relationship Objective: Setbacks and buffers should be established to protect environmental resources and “sensitive uses” from potential negative impacts of industrial use.

*Analysis: The proposed use is not a typical industrial use that has operational effects on “sensitive uses.” The proposed office building and parking garage addition will not be storing, handling, disposing of, and/or using acutely hazardous materials. Moreover, the proposed project site is located outside of the Riparian Corridor, which is located west of the existing office buildings on the site and creating a buffer between the proposed project and the riparian areas.*

- d) Gateway Entrances Objective: Development located near Highway 237 along both sides of Gold Street, First Street, and Zanker Road should foster a “gateway” feel through building orientation, signs, trees, landscaping, and other features.

*Analysis: The proposed Building 5 and garage addition will further enhance the overall America Center site. The constructed and entitled office buildings on the site have a high degree of visibility from State Route 237 and are designed to have a distinctive high-quality, “modern” architectural style that will support the image of a high-tech, high-quality office at Gold Street.*

5. **Zoning Ordinance Compliance.** The proposed Planned Development Rezoning would maintain the existing zoning of A(PD) Agriculture Planned Development Zoning District and includes the Land Use Plan and Development Standards allowed on the subject site including allowable land uses, setbacks, height and mitigation. The allowable uses in the designated Commercial, Office, Research and Development area include professional and/or research and development offices and parking structure uses. All uses permitted under the IP Industrial, CO Commercial Office, and CP Commercial Pedestrian districts are also permitted. Day care, conferencing, dining rooms, personal business service retail, restaurants and satellite and broadcast reception facilities are also permitted. The new office building will maintain a minimum of a 40-foot separation between the other buildings on the America Center site and the garage addition will maintain a minimum 20-foot setback from the east property line. The maximum building height is 90 feet, consistent with the Alviso Master Plan. Parking is required to provide a minimum of 1 space per 250 square feet (net square feet).

The proposed Building 5 is an office/R&D building that provides more than 40 feet of separation from all adjacent buildings and is 83 feet in height at the top of roof. The parking garage addition maintains the 20-foot east property line setback and is a maximum height of 54 feet. Parking required for Building 5 is 770 parking spaces, which is accommodated in the addition to the parking garage. Therefore, the proposed development complies with the Development Standards of the proposed and existing Planned Development Zoning.

In addition, the proposed rezoning includes the removal of the area encompassed in PDC15-016, which was for the Hotel (Residence Inn) at the terminus of America Center Court. This area has its own Rezoning, Land Use Plan and Development Standards that were adopted as part of PDC15-016. The area proposed to be removed from the America Center Planned Development Zoning is noted in the Zoning Map.

## **6. Council Policy 6-34 Riparian Corridor Protection and Bird Safe Design.**

Council Policy 6-34 (adopted in August of 2016) provides guidance, consistent with the General Plan, for protecting and restoring riparian habitat; limiting the creation of new impervious surface within Riparian Corridor setbacks; and encouraging bird-safe design in Bayland and riparian habitats of lower Coyote Creek north of SR 237. This policy supplements the regulations for riparian corridor protection already contained within the Habitat Plan, Municipal Code, and other existing City policies that may provide for riparian protection and bird-safe design.

Specific guidance pertaining to setbacks, allowed activities, and materials and lighting in riparian areas are included within Council Policy 6-34. Further, bird-safe design guidelines for structures north of SR 237 advise that buildings:

- Avoid use of mirrors and large areas of reflective glass;
- Avoid use of transparent glass skyways, walkways, or entryways, free-standing glass walls, and transparent building corners;
- Avoid funneling open space to a building façade;
- Strategically place landscaping to reduce reflection and views of foliage inside or through glass;
- Avoid or minimize up-lighting and spotlights; and
- Turn non-emergency lighting off, or shield it, at night to minimize light from buildings that is visible to birds, especially during bird migration season (February through May and August through November).

The project would respect the 100-foot riparian setback specified within of General Plan Policy ER-2.1, ER-2.2, ER-2.3 and Council Policy 6-34. Landscape plans would be in compliance with Policy ER-6.5. Mitigation measures MM BIO-1.1 and MM BIO-1.2 would be implemented consistent with General Plan Policy ER-4.4, ER-5.1, and ER-5.2. Bird-safe design features are included in the building and are reviewed by city staff as part of the overall design review process for consistency with City Council Policy 6-34 and Policy ER-7.1.

## **7. Planned Development Permit Findings: Chapter 20.100 of the San José Municipal**

Code establishes evaluation criteria for issuance of a Planned Development Permit. These criteria are applied to the project based on the above-stated findings related to General Plan, Zoning and CEQA conformance and for the reasons stated below, and subject to the conditions set forth in the permit. The City Council determines that:

- a) The Planned Development Permit, as issued, is consistent with and furthers the policies of the General Plan;

*Analysis: As described above, the project is consistent with and furthers the policies of the General Plan and Alviso Master Plan.*

- b) The Planned Development Permit, as issued, conforms in all respects to the Planned Development Zoning of the property;

*Analysis: The Planned Development Permit conforms in all respects to the proposed A(PD) Agriculture Planned Development Zoning of the property, including uses, setbacks and height.*

- c) The Planned Development Permit, as approved, is consistent with applicable City Council policies, or counterbalancing considerations justify the inconsistency;

*Analysis: As discussed below, the project is in conformance with City Council Policy 6-30 Public Outreach. Staff received confirmation that the Project applicant posted an on-site sign at the subject property, in accordance with the City Council Policy 6-30 on November 5, 2015 and a community meeting was held on October 16, 2016. In addition, the project is also consistent with City Council Policy 6-34 in that the proposed project includes View Dynamic Glass which provides varied tinting, Building 5 is located with the center of site and no glass has been proposed on the parking garage addition. Lastly, the project complies with City Council Policy 8-14 Post-Construction Hyrdromodification Management through the proposed stormwater treatment management system for the project site.*

- d) The interrelationship between the orientation, location, mass and scale of building volumes, and elevations of proposed buildings, structures and other uses on-site are appropriate, compatible and aesthetically harmonious;

*Analysis: The orientation, location, similar height, mass and scale of the proposed Building 5 is harmonious with the surrounding built buildings and will be constructed of high quality materials. Additionally, the garage extension will be harmonious with the existing parking garage in that it will use the same materials and proportions.*

- e) The environmental impacts of the project, including, but not limited to aesthetics, air quality, noise, and transportation/traffic, even if insignificant for purposes of the California Environmental Quality Act (CEQA), will not have an unacceptable negative effect on adjacent property or properties.

*Analysis: A Subsequent Environmental Impact Report (SEIR) was prepared for the project in compliance with the California Environmental Quality Act (CEQA) and the CEQA Guidelines. The proposed project impacts are discussed below. Based on the review of the proposed project by all of the various department, there are no non-CEQA related impacts anticipated for this project. The proposed project would result in new impacts related to aesthetics, air quality, noise, and transportation as discussed below. These impacts are significant and unavoidable. A Statement of Overriding Considerations has been prepared which discusses the projects benefits. Other identified impacts can be mitigated to a less than significant level.*

8. **Environmental Review.** A Draft Subsequent Environmental Impact Report (DSEIR) for the America Center Phase III Project was prepared by the Director of Planning, Building and Code Enforcement in accordance to CEQA. The DSEIR analyzed project level environmental impacts and discussed alternatives to the proposed project.

The America Center Phase III Project DSEIR is a Subsequent EIR to the previously certified Final Environmental Impact Report for the Legacy Terrace Development Planned Development Rezoning and Prezoning (Legacy Terrace FEIR), SCH# 99082004. The Legacy Terrace FEIR was adopted by City Council Resolution No. 69392 on February 15, 2000. The proposed project is within the boundaries of the Legacy Terrace site (renamed America Center prior to completion of the first phase of the project). The DSEIR provides an updated evaluation of the existing environmental setting and identifies new significant environmental effects that were not previously identified in the Legacy Terrace FEIR.

#### *Identified Significant and Unavoidable Impacts*

The Legacy Terrace FEIR identified the following significant, unavoidable impacts that would be the same under the proposed project:

- a) Aesthetics: views of the site from SR 237 and recreational trails in the vicinity.
- b) Air Quality: vehicle-related operational emissions of NOx.
- c) Noise: short-term, construction noise at sensitive residential receptors.
- d) Transportation/Traffic: mixed-flow lane freeway segment traffic at SR 237 between Great America Parkway and North First Street (PM peak hour).

Implementation of the proposed project would also result in the following new or substantially more severe significant impacts, which are significant and unavoidable.

- a) Transportation: impacts at the following locations:

- i. Mixed-flow lane freeway segment impacts:
  - Eastbound SR 237 between Great America Parkway and North First Street (PM Peak Hour)
  - Eastbound SR 237 between North First Street and Zanker Road (PM peak hour)
  - Westbound SR 237 between I-880 and McCarthy Boulevard (AM peak hour)
  - Westbound SR 237 between McCarthy Boulevard and Zanker Road (AM and PM peak hours)
- ii. High Occupancy Vehicle (HOV) lane freeway segment impacts:
  - Westbound SR 237 between I-880 and McCarthy Boulevard (AM peak hour)

#### *Less than Significant Impact with Mitigation*

The proposed project impacts listed below would be reduced to a less than significant level with mitigation:

- Air Quality: construction-related dust
- Biological Resources: impacts to nesting birds and burrowing owls
- Geology and Soils: differential settlement and seismicity
- Hazards and Hazardous Materials: soil contamination and landfill gas
- Hydrology and Water Quality: contaminated stormwater runoff
- Transportation/Traffic: intersection impacts at Lafayette Street and the Gold Street Connector

The proposed CEQA resolution, Mitigation Monitoring and Reporting Program (MMRP), and SEIR further explains in detail how these mitigations reduce each identified above to a less than significant level.

#### *Alternatives*

As required under CEQA, the DSEIR evaluated two no-project alternatives and one design alternative. These alternatives are as follows:

1. No Project – No Development Alternative: Under the No Project – No Development Alternative, the buildings and parking lots existing and under construction at the site would remain. Building 5 would not be constructed. The site would remain as zoned and approved for Buildings 1 through 4 and the parking

garage.

2. No Project – Develop Under Current PD Zoning Alternative: The project site is currently designated Combined/Industrial Commercial in the City’s General Plan and is located within a Planned Development zoning district. Under the current Planned Development zoning (PDC99-044), 900,000 square feet of development is allowed for the Commercial Office/R&D portion of the project site. Of that total, 867,762 square feet have been constructed or is currently under construction and 32,238 square feet of entitlement remains.

Under the current Planned Development zoning, a 32,238-square-foot office building could be constructed. A potential project under the No Project – Develop Under Current Planned Development Zoning Alternative would likely be one story and would occupy the footprint of the proposed Building 5. The building would likely not be visible from SR 237 and nearby recreational trails as it would be shielded on all sides by existing, much taller structures. Pile driving would still be required for a smaller Building 5 and parking garage extension.

3. Reduced Intensity Alternative: A Reduced Intensity Alternative would potentially allow for 55,000 additional feet of development resulting in an approximately 87,000-square foot, three-story Building 5 (assuming the same footprint as the proposed project and use of the remaining 32,238 square feet of yet unbuilt but entitled square footage at the site). The building would likely not be visible as it would be shielded on all sides by existing, much taller structures. It is unknown at this time the extent of pile driving that might be necessary for a three-story structure.

*These alternatives were thoroughly discussed and evaluated in the DSEIR. Staff does not recommend adopting any of the alternatives because Building 5 would be located within the existing development minimizing visibility and impacts to the adjacent area. The parking garage addition will accommodate the additional parking needed for the development, reducing the amount of surface parking.*

#### *Circulation and Public Comments*

The DSEIR was circulated for public review and comment for 45 days from June 12, 2017 to July 27, 2017. The City received a total of five written comment letters during the public circulation period. Comments received were from the California Department of Transportation (Caltrans); Santa Clara Valley Transportation Authority (VTA); Santa Clara Valley Audubon Society; MR Wolfe & Associates, P.C. on behalf of the Organizacion Comunidad de Alviso; and Steve Dunn, SteelWave. Issues raised in these comment letters include the following:

- Transportation analysis and impacts for project-generated traffic.
- Transportation improvements (Great America Parkway/State Route 237 and



Lafayette Street/Gold Street Connector intersections).

- Traffic mitigation measures (MM TRA-1.1 and MM TRA(C)-1.2).
- Transportation Demand Management (TDM) program.
- Queuing analysis and queuing reductions.
- Traffic congestion impacts.
- Surrounding traffic improvements.
- Improvements to pedestrian and bicycle access.
- Bicycle parking and connectivity.
- Transit access and ridership.
- Impacts to potential sensitive archaeological sites and Native American consultation.
- Nitrogen deposition impacts.
- Air pollutant monitoring and toxic air contaminants.
- Greenhouse gas emissions assessment.

A First Amendment to the DSEIR was prepared that provided responses to the comments submitted during the public circulation period and revisions to the text of the DSEIR. The First Amendment together with the DSEIR constitute the Final Environmental Impact Report (FEIR) for the proposed project. The DSEIR and First Amendment to the DSEIR are available for review on the City's Active EIRs website at: <http://www.sanjoseca.gov/index.aspx?NID=5230>

### *EIR Recirculation Unnecessary*

The comments received do not identify substantive inadequacies in the DSEIR or new previously unidentified significant impacts that require recirculation. The recirculation of an EIR is required when significant new information is added to the EIR after public notice is given of the availability of the draft EIR for public review but before certification. "Information" can include changes in the project or environmental setting as well as additional data or other information. New information added to an EIR is not "significant" unless the EIR is changed in a way that deprives the public of meaningful opportunity to comment on a substantial adverse environmental effect of the project or a feasible way to mitigate or avoid such an effect (CEQA Guidelines Section 15088.5).

In accordance with CEQA Guidelines Section 15088, the First Amendment to the DSEIR for the project includes written responses to all comments received during the public review period for the DSEIR. As required by Section 15132 of the CEQA Guidelines, the responses in the First Amendment to the DSEIR address significant

environmental points and comments on the content and adequacy of the EIR. The responses and comments provide clarification and refinement of information presented in the DSEIR and, in some cases, correct or update information in the DSEIR. No significant new information has been added to the DSEIR since publication of the DSEIR; therefore, the DSEIR does not need to be recirculated.

## CONDITIONS

1. **Acceptance of Permit.** Per Section 20.100.290(B) of Title 20 of the San José Municipal Code, should the permittee fail to file a timely and valid appeal of this Planned Development Permit within the applicable appeal period, such inaction by the permittee shall be deemed to constitute all of the following on behalf of the permittee:
  - a. Acceptance of the Planned Development Permit; and
  - b. Agreement by the permittee to be bound by, to comply with, and to do all things required of or by the permittee pursuant to all of the terms, provisions, and conditions of this Permit or other approval and the provisions of Title 20 of the San José Municipal Code applicable to such Permit.
2. **Permit Expiration.** The Planned Development Permit shall automatically expire two (2) years from and after the date of issuance hereof by the City Council, if within such time period, a Building Permit has not been obtained or the use, if no Building Permit is required, has not commenced, pursuant to and in accordance with the provision of this Permit. The date of issuance is the date this Permit is approved by the City Council. However, the Director of Planning may approve a Permit Adjustment/Amendment to extend the validity of this Permit in accordance with Title 20 of the San José Municipal Code. The Permit Adjustment/Amendment must be approved prior to the expiration of this Planned Development Permit.
3. **Building Permit/Certificate of Occupancy.** Procurement of a Building Permit and/or Certificate of Occupancy from the Building Official for the structures described or contemplated under this Planned Development Permit shall be deemed acceptance of all conditions specified in this permit and the permittee's agreement to fully comply with all of said conditions. No change in the character of occupancy or change to a different group of occupancies as described by the "Building Code" shall be made without first obtaining a Certificate of Occupancy from the Building Official, as required under San José Municipal Code Section 24.02.610, and any such change in occupancy must comply with all other applicable local and state laws.
4. **Sewage Treatment Demand.** Chapter 15.12 of Title 15 of the San José Municipal Code requires that all land development approvals and applications for such approvals in the City of San José shall provide notice to the applicant for, or recipient

of, such approval that no vested right to a Building Permit shall accrue as the result of the granting of such approval when and if the City Manager makes a determination that the cumulative sewage treatment demand of the San José-Santa Clara Regional Wastewater Facility represented by approved land uses in the area served by said Facility will cause the total sewage treatment demand to meet or exceed the capacity of San José-Santa Clara Regional Wastewater Facility to treat such sewage adequately and within the discharge standards imposed on the City by the State of California Regional Water Quality Control Board for the San Francisco Bay Region. Substantive conditions designed to decrease sanitary sewage associated with any land use approval may be imposed by the approval authority.

5. **Conformance to Plans.** The development of the site shall conform to the approved Planned Development Permit plans entitled, “America Center,” dated received on December 20, 2017, on file with the Department of Planning, Building and Code Enforcement, as may be amended subject to City’s approval, and to the San José Building Code (San José Municipal Code, Title 24), as amended. The plans are referred to herein as the “Approved Plan Set”.
6. **Compliance with Local and State Laws.** The subject use shall be conducted in full compliance with all local and state laws. No part of this approval shall be construed to permit a violation of any part of the San José Municipal Code. The Permit shall be subject to revocation if the subject use is conducted in such a manner as to cause a nuisance, as conditioned.
7. **Discretionary Review.** The Director of Planning, Building and Code Enforcement maintains the right of discretionary review of requests to alter or amend structures, conditions, or restrictions of this Permit incorporated by reference in accordance with Chapter 20.100 of Title 20 of the San José Municipal Code.
8. **Nuisance.** This use shall be operated in a manner that does not create a public or private nuisance or that adversely affects the peace, health, safety, morals or welfare of persons residing or working in the surrounding area or be detrimental to public health, safety or general welfare. Any such nuisance shall be abated immediately upon notice by the City.
9. **Anti-Litter.** The site and surrounding area shall be maintained free of litter, refuse, and debris. Cleaning shall include keeping all publicly used areas free of litter, trash, cigarette butts and garbage.
10. **Anti-Graffiti.** During construction, the permittee shall remove all graffiti from buildings, walls and other surfaces within 48 hours of defacement. Upon project completion and/or transfer of ownership, the property owner, and/or Maintenance District shall remove all graffiti from buildings and wall surfaces within 48 hours of defacement.
11. **Loitering.** Loitering shall not be allowed in the public right-of-way adjacent to the subject site.

12. **Refuse.** All trash areas shall be effectively screened from view and covered and maintained in an orderly state to prevent water from entering the garbage container. No outdoor storage is allowed / permitted unless designated on the approved plan set. Trash areas shall be maintained in a manner to discourage illegal dumping.
13. **Sign Approval.** No signs are approved at this time. All proposed signs shall be subject to review and approval by the Director of Planning through a subsequent Permit Adjustment.
14. **Building and Property Maintenance.** The property owner or management company shall maintain the property in good visual and functional condition. This shall include, but not be limited to all exterior elements of the buildings such as paint, roof, paving, signs, lighting and landscaping.
15. **Perimeter Fencing.** Fence height and materials shall be provided as shown on the approved plans. Changes to the approved fencing shall require review by the Director of Planning, Building, and Code Enforcement.
16. **Colors and Materials.** All building colors and materials are to be those specified on the Approved Plan Set. Any change in building colors and materials shall require a Permit Adjustment.
17. **Window Glazing.** Unless otherwise indicated on the approved plan, all windows shall consist of a transparent glass.
18. **Building Division Clearance for Issuing Permits.** Prior to the issuance of any Building Permit, the following requirements must be met to the satisfaction of the Chief Building Official:
  - a. *Construction Plans.* The permit file number, PD15-053 shall be printed on all construction plans submitted to the Building Division.
  - b. *Americans with Disabilities Act.* The permittee shall provide appropriate access as required by the Americans with Disabilities Act (ADA), including paths of travel connecting all buildings on the site.
  - c. *Emergency Address Card.* The permittee shall file an Emergency Address Card, Form 200-14, with the City of San José Police Department.
  - d. *Construction Plan Conformance.* A project construction plan conformance review by the Planning Division is required. Planning Division review for project conformance will begin with the initial plan check submittal to the Building Division. Prior to any building permit issuance, building permit plans shall conform to the approved Planning development permits and applicable conditions.
  - e. *Project Addressing Plan.* Prior to issuance of any Building Permit, the following requirements must be met to the satisfaction of the Chief Building Official: The permittee shall submit an addressing plan for approval for the subject

development (residential, mixed use, complex commercial or industrial).

- f. *Adjust of easements and property lines.* The proposed parking garage addition is located over existing parcel lines. A lot merger or parcel map will be required to correct the parcel lines and the map should show all easements around Building 5. The map will need to be approved and recorded prior to issuance of building permit for the garage or Building 5.

19. **Access Control.** When access control devices including bars, grates, gates, electric and/or magnetic locks, or similar devices which would inhibit rapid fire department emergency access to the building are installed, such devices shall be approved by the Chief. All access control devices shall be provided with an approved means for deactivation or unlocking by the Fire Department. Access control devices shall also comply with CFC/CBC Chapter 10 for exiting.

20. **Security Gate Access.** All access control vehicle security gate(s) shall be a minimum of 20 feet clear width and may have a clear with of 14 feet for each direction of travel when split by median. Gates shall be recessed beyond the turning radius required by the San José Fire Department (SJFD) and without obstruction of any median island dividers. All gate installing shall be reviewed and approved prior to construction. Electric gate operators must have UL 325 listing and it gate must comply with ASTM F2200.

21. **Emergency Vehicle Access.** Width, length, and grade of the fire apparatus access roads, streets, avenues, and the like. Every portion of all building exterior walls shall be within 150 feet of an access road. To the satisfaction of the Fire Chief, the fire access shall include the following:

- a. An approved all weather surface;
- b. Access road that are at least 20 feet wide;
- c. Dimensions with a minimum 13 feet, 6 inches vertical clearance;
- d. Load bearing designs that are maintained to support the loads of fire apparatus of at least 75,000 pounds;
- e. Maintain a minimum inside turning radius of 30 feet and an outside turning radius of 50 feet;
- f. Designs with approved provisions for turnaround of fire apparatus if access way has dead ends and is in excess of 150 feet;
- g. Maintain a maximum grade of 15 percent;
- h. Provision of a second point of access is required when a fire apparatus road exceeds 1,000 feet;
- i. Curbs are required to be painted red and marked as "Fire Lane - No Parking" under the following conditions: (show exact locations on plan)

- j. Roads, streets, avenues, and the like that are 20 feet wide to less than 26 feet wide measured from face-of-curb to face-of-curb shall have curbs on both sides of the road painted and marked
- k. Roads, streets, avenues, and the like that are 26 feet wide to less than 32 feet wide measured from face-of-curb to face-of-curb shall have one curb painted and marked.

**22. Fire Hydrant Location & Spacing.** The Fire Hydrant Location & Spacing shall meet the requirements of CFC Appendix C with City of San José Amendments. Show the location and spacing of the fire hydrants on the plans meeting the following general requirements.

- a. Determine the required number and spacing of fire hydrants per CFC Appendix C, Table C102.1; (or Refer to San José Fire Flow and Hydrant Policy <http://sanjoseca.gov/DocumentCenter/View/61703>). For the purposes of determining the required number and spacing of fire hydrants, no reduction in the Required Fire Flow is allowed even for buildings with sprinkler systems.
- b. Show the spacing of the fire hydrants along the fire apparatus access roads on the plans.
- c. Demonstrate on plans that all exterior walls of the building(s) are within 400 feet from a fire hydrant. The distance should be calculated from a fire hydrant on a fire apparatus access road, as measured along the path of travel around the exterior of the building (and not by drawing a 400 feet radius around the fire hydrant).

**23. Available Fire Flow.** Provide a copy of the letter from the property's water company that indicates the Available Fire Flow in gallons per minute (gpm). Approval of Fire Hydrant Location & Spacing by SJFD is required before requesting Fire Flow data from the water company. Verify with the water company that a minimum of 1,000 gpm from a new proposed fire hydrant is available at minimum pressure of 20 psi.

**24. Fire sprinkler system** is required in all units and will be a deferred submittal after building permit is issued.

**25. Public Works Clearance for Building Permit(s) or Map Approval:** Prior to the approval of the Tract or Parcel Map (if applicable) by the Director of Public Works, or the issuance of Building permits, whichever occurs first, the applicant will be required to have satisfied all of the following Public Works conditions. The applicant is strongly advised to apply for any necessary Public Works permits prior to applying for Building permits. Standard review timelines and submittal instructions for Public Works permits may be found at the following:  
<http://www.sanjoseca.gov/index.aspx?nid=2246>.

**26. Construction Agreement:** The public improvements conditioned as part of this permit require the execution of a Construction Agreement that guarantees the completion of the public improvements to the satisfaction of the Director of Public Works. This agreement includes privately engineered plans, bonds, insurance, a completion deposit, and engineering and inspection fees.

**27. Grading/Geology:**

- a) A grading permit is required prior to the issuance of a Public Works Clearance.
- b) All on-site storm drainage conveyance facilities and earth retaining structures 4' foot in height or greater (top of wall to bottom of footing) or is being surcharged (slope of 3:1 or greater abutting the wall) shall be reviewed and approved under Public Works grading and drainage permit prior to the issuance of Public Works Clearance. The drainage plan should include all underground pipes, building drains, area drains and inlets. The project shall provide storm drainage calculations that adhere to the 2013 California Plumbing Code or submit a stamped and signed alternate engineered design for Public Works discretionary approval and should be designed to convey a 10 year storm event.
- c) If the project proposes to haul more than 10,000 cubic yards of cut/fill to or from the project site, a haul route permit is required. Prior to issuance of a grading permit, contact the Department of Transportation at (408) 535-3850 for more information concerning the requirements for obtaining this permit.
- d) Because this project involves a land disturbance of one or more acres, the applicant is required to submit a Notice of Intent to the State Water Resources Control Board and to prepare a Storm Water Pollution Prevention Plan (SWPPP) for controlling storm water discharges associated with construction activity. Copies of these documents must be submitted to the City Project Engineer prior to issuance of a grading permit.
- e) The Project site is within the State of California Seismic Hazard Zone. A geotechnical investigation report addressing the potential hazard of liquefaction must be submitted to, reviewed and approved by the City Geologist prior to issuance of a grading permit or Public Works Clearance. The report should also include, but not limited to: foundation, earthwork, utility trenching, retaining and drainage recommendations. The investigation should be consistent with the guidelines published by the State of California (CGS Special Publication 117A) and the Southern California Earthquake Center (SCEC, 1999). A recommended depth of 50 feet should be explored and evaluated in the investigation.
- f) Since this is a landfill site, and is currently under the permitting power of the State of California, (LEA), there must be monitoring by the State of California Inspector regarding any alterations or penetrations into the soils on this site. Appropriate conditions regarding such will be included in any grading permit approvals.
- g) Provide a final Land Fill Closure Soils Report:

- i) Because this project rests upon a sanitary land fill site, a soils report specifically addressing the land fill closure, techniques to be used in driving piles through sanitary/solid waste, and mitigation measures which will prevent or mitigate migration of waste water into adjacent wetlands and creek areas must be submitted prior to issuance of a grading plan.
- h) Engineering Analysis:
  - i) Prior to issuance of grading permits for individual site improvements, the applicant shall provide to the Local Enforcement Agency and the Department of Public Works a project-level, engineering analysis that addresses, in full and sufficient detail, the following elements of the final project design:
    - a) Landfill gas mitigation and monitoring systems, including structure monitoring and landfill perimeter monitoring system;
    - b) Landfill settlement/differential settlement;
    - c) Seismic stability/foundation design;
    - d) Site surface drainage and final grading;
    - e) Final landfill cap design, including landfill gas protection elements of the foundation design;
  - i) Any other elements of the design as required by the Local Enforcement Agency (LEA) or Department of Public Works, including any specialized analysis that may be warranted by the City. The applicant shall bear the responsibility for providing any such specialized analysis.

- 28. Stormwater Runoff Pollution Control Measures:** This project must comply with the City's Post-Construction Urban Runoff Management Policy (Policy 6-29) which requires implementation of Best Management Practices (BMPs) which includes site design measures, source controls and numerically-sized Low Impact Development (LID) stormwater treatment measures to minimize stormwater pollutant discharges. Final inspection and maintenance information on the post-construction treatment control measures must be submitted prior to issuance of a Public Works Clearance.
- a) The project's Stormwater Control Plan and numeric sizing calculations have been reviewed and this project will be in conformance with City Policy 6-29.
  - b) Final inspection and maintenance information on the post-construction treatment control measures must be submitted prior to issuance of a Public Works Clearance.
  - c) A post construction Final Report is required by the Director of Public Works from a Civil Engineer retained by the owner to observe the installation of the BMPs and stating that all post construction storm water pollution control BMPs have been installed as indicated in the approved plans and all significant changes have been reviewed and approved in advance by the Department of Public Works.



- 29. Transportation:** A Traffic Impact Analysis has been performed for this project based on 299 AM and 288 PM peak hour trips. We conclude that the subject project will be in conformance with the City of San Jose Transportation Level of Service Policy (Council Policy 5-3). See separate Traffic memo dated 12/19/17 for additional information. The following are the project conditions included in the Traffic memo:
- a) Pay a fair share amount towards improvements (including full cost for design, construction, etc.) to the Lafayette Street/Gold Street Connector intersection. Improvements would include with the addition of a second northbound left-turn lane in a vacant area between the Gold Street Connector and SR 237. The improvement will require widening of the Gold Street Connector and shifting of travel lanes to the south by approximately 12 feet to accommodate a second receiving lane for the second northbound left-turn lane. The roadway widening would also require the relocation of the Highway 237 Bikeway, south of the Gold Street Connector. The Director of Public Works shall determine the fair share based on the cost of the improvement at the time the payment is due and the project's contribution to the impact (an estimated 5 percent).
- 30. Flood: Zone X:** Although the project site is located within a designated Federal Emergency Management Agency (FEMA) 100-year floodplain, the property has been removed from the Special Flood Hazard Area (SFHA) by a Letter of Map Revision (LOMR). The property is now in flood zone X. Zone X is an area of moderate or minimal flood hazard. There are no City floodplain requirements for zone X.
- 31. Sewage Fees:** In accordance with City Ordinance all storm sewer area fees, sanitary sewer connection fees, and sewage treatment plant connection fees, less previous credits, are due and payable.
- 32. Municipal Water:** In accordance with City Ordinance #23975, Major Water Facilities Fee is due and payable. Contact Amado Valdez at (408) 794-6769 for further information.
- 33. Sanitary:** This project is within an area of San Jose which discharges through a sanitary sewer and pump system owned and operated by the City of Santa Clara. An agreement is currently in place which allows an expected discharge which will accommodate the proposed building and use. Any additional discharge or change in use may require additional negotiation and amendment of the existing agreement.
- 34. Referrals:** This project should be referred to the California Department of Transportation (CalTrans) and Santa Clara Valley Water District.
- 35. Conformance to Mitigation Monitoring and Reporting Program.** This project shall conform to all applicable requirements of the Mitigation Monitoring and

Reporting Program (MMRP) approved for this development by City Council Resolution No. \_\_\_\_\_.

### **36. Standard Environmental Conditions:**

#### **1. AIR QUALITY.**

1.1. Consistent with the 2017 BAAQMD CEQA Air Quality Guidelines, GP Policy MS-13.1, and current City requirements, the project shall implement the following standard permit conditions during all phases of construction on the project site, to reduce dustfall emissions:

- All exposed surfaces (e.g., parking areas, staging areas, soil piles, graded areas, and unpaved access roads) shall be watered twice daily.
- All haul trucks transporting soil, sand, and other loose material off-site shall be covered.
- All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited.
- All vehicle speeds on unpaved roads shall be limited to 15 miles per hour.
- All roadways, driveways, and sidewalks to be paved shall be completed as soon as possible. Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used.
- Idling times shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to five minutes (as required by the California airborne toxics control measure Title 13, Section 2485 of California Code of Regulations [CCR]). Clear signage shall be provided for construction workers at all access points.
- All construction equipment shall be maintained and properly tuned in accordance with manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operations.
- Post a publicly visible sign with the telephone number and person to contact at the Lead Agency regarding dust complaints. This person shall respond and take corrective action within 48 hours. The Air District's phone number shall also be visible to ensure compliance with applicable regulations.

## **2. BIOLOGICAL RESOURCES.**

- 2.1. Habitat Conservation Plan. Consistent with the General Plan FPEIR analysis on indirect impacts to sensitive serpentine habitats, the project will contribute to the Habitat Plan nitrogen deposition fee program. The project applicant shall submit the Santa Clara Valley Habitat Plan Application for Nitrogen Deposition-Only Projects and the payment of the nitrogen deposition fees (based on a fee rate per new daily vehicle trip) to the Supervising Environmental Planner of the Department of Planning, Building, and Code Enforcement for review and approval prior to issuance of any grading permit.

## **3. CULTURAL RESOURCES.**

- 3.1. Archaeological Resources Impacts. In the event that any significant cultural materials (including prehistoric or historic resources or vertebrate fossils) are encountered during excavation and/or grading of the site, all activity within a 50-foot radius of the find shall be stopped, the Director of Planning, Building and Code Enforcement shall be notified, and an archaeologist will examine the find and make appropriate recommendations regarding the significance of the find and appropriate mitigation. Recommendations could include collection, recordation, and analysis of any significant cultural materials. A report of findings documenting any data recovery during monitoring would be submitted to the Director of Planning, Building, and Code Enforcement.

In the event that human remains are discovered during excavation and/or grading of the site, activities occurring within a 50-foot radius of the find shall be stopped. The Santa Clara County Coroner shall be notified and make a determination as to whether the remains are of Native American origin or whether an investigation into the cause of death is required. If the remains are determined to be Native American, the Coroner will notify the NAHC immediately. Once the NAHC identifies the most likely descendants, the descendants will make recommendations regarding proper burial, which will be implemented in accordance with Section 15064.5(e) of the CEQA Guidelines.

- 3.2. Paleontological Resources Impacts. If vertebrate fossils are discovered during construction, all work on the site will stop immediately until a qualified professional paleontologist can assess the nature and importance of the find and recommend appropriate treatment. Treatment may include preparation and recovery of fossil materials so that they can be housed in an appropriate museum or university collection and may also include preparation of a report for publication describing the finds. The project proponent will be responsible

for implementing the recommendations of the qualified professional paleontologist.

#### **4. GEOLOGY AND SOILS.**

4.1. Seismicity Impacts. Seismic hazards will be reduced by utilizing design and construction practices in accordance with seismic building criteria, as described in the current City of San José Building Standards Code and Fire Code. A design-level geotechnical investigation report addressing the potential hazards of liquefaction, lateral deformations for the Designed Based Earthquake, and seismic shaking shall be submitted to, reviewed and approved by the City of San José Geologist and City of San José Building Division prior to issuance of a Grading Permit or Public Works Clearance. The investigation should be consistent with the guidelines published by the State of California (California Geological Survey Special Publication 117A) and the Southern California Earthquake Center (SCEC, 1999).

#### **5. HYDROLOGY AND WATER QUALITY.**

5.1. Water Quality Construction Impacts. The following project-specific measures, based on RWQCB BMPs, have been included in the project to reduce construction and development-related water quality impacts. BMPs would be implemented prior to and during earthmoving activities on-site and would continue until the construction is complete, and during the post-construction period, as appropriate:

- Burlap bags filled with drain rock shall be installed around storm drains to route sediment and other debris away from the drains.
- Earthmoving or other dust-producing activities shall be suspended during periods of high winds.
- All exposed or disturbed soil surfaces shall be watered at least twice daily to control dust as necessary.
- Stockpiles of soil or other materials that can be blown by the wind shall be watered or covered.
- All trucks hauling soil, sand, and other loose materials shall be required to cover all trucks or maintain at least two feet of freeboard.
- All paved access roads, parking areas, staging areas and residential streets adjacent to the construction sites shall be swept daily (with water sweepers).
- Vegetation in disturbed areas shall be replanted as quickly as possible.

- All unpaved entrances to the site shall be filled with rock to knock mud from truck tires prior to entering City streets. A tire wash system may also be employed at the request of the City.
- The project applicant shall comply with the City of San José Grading Ordinance, including implementing erosion and dust control during site preparation and with the City of San José Zoning Ordinance requirements for keeping adjacent streets free of dirt and mud during construction.
- A NPDES General Construction Storm Water Permit will be administered by the RWQCB. Prior to construction grading for the proposed land uses, the project proponent will file an NOI to comply with the General Permit and prepare a SWPPP which addresses measures that would be included in the project to minimize and control construction and post-construction runoff. Measures will include, but are not limited to, the aforementioned RWQCB BMPs.
- The certified SWPPP will be posted at the project site and will be updated to reflect current site conditions. Copies of the SWPPP shall be submitted to the City of San José Department of Public Works. The following measures shall be included in the SWPPP:
  - Preclude non-stormwater discharges to the stormwater system.
  - Incorporate effective, site-specific BMPs for erosion and sediment control during the construction and post-construction periods.
  - Cover soil, equipment, and supplies that could contribute pollution prior to rainfall events or monitor runoff.
  - Perform monitoring of discharges to the stormwater system.
  - When construction is complete, a Notice of Termination (NOT) for the General Permit for Construction will be filed with the SWRCB. The NOT will document that all elements of the SWPPP have been executed, construction materials and waste have been properly disposed of, and a post-construction stormwater management plan is in place as described in the SWPPP for the site.
- Provide permanent cover to stabilize the disturbed surfaces after construction has been completed.

**37. Revocation, Suspension, Modification.** This Planned Development Permit and Development Exception may be revoked, suspended or modified by the Planning Commission, or by the City Council on appeal, at any time regardless of who is the owner of the subject property or who has the right to possession thereof or who is using the same at such time, whenever, after a noticed hearing in accordance with Part 2 of Chapter 20.100 of Title 20 of the San José Municipal Code it finds:

- a. A violation of any conditions of the Planned Development Permit was not abated,

- corrected or rectified within the time specified on the notice of violation; or
- b. A violation of any City ordinance or State law was not abated, corrected or rectified within the time specified on the notice of violation; or
  - c. The use as presently conducted creates a nuisance.

## EFFECTIVE DATE

The effective date of this Permit (File No. PD15-053) shall be the effective date of the A(PD) Planned Development Rezoning Ordinance for File No. PDC15-058 and shall be no earlier than the effective date of said Planned Development Rezoning Ordinance.

In accordance with the findings set forth above, a permit to use the subject property for said purpose specified above is hereby **approved**.

ADOPTED this \_\_\_\_\_ day of \_\_\_\_\_, 2018, by the following vote:

AYES:

NOES:

ABSENT:

DISQUALIFIED:

\_\_\_\_\_  
SAM LICCARDO  
Mayor

ATTEST:

\_\_\_\_\_  
TONI J. TABER, CMC  
City Clerk

## NOTICE TO PARTIES

*The time within which judicial review must be sought to review this decision is governed by the provisions of the California Code of Civil Procedure Section 1094.6.*

# **FILE NO. PDC15-058**

## **DEVELOPMENT STANDARDS**

\* In any case where graphic plans and text may differ, this text take precedence.

### **I. USES**

1. THE COMMERCIAL OFFICE/RESEARCH & DEVELOPMENT AREA SHALL PERMIT PROFESSIONAL AND/OR RESEARCH & DEVELOPMENT OFFICES AND PARKING STRUCTURE USES. ALL USES PERMITTED UNDER THE IP INDUSTRIAL, CO COMMERCIAL OFFICE, AND CP COMMERCIAL PEDESTRIAN DISTRICTS ARE ALSO PERMITTED. DAY CARE, CONFERENCING, DINING ROOMS, PERSONAL BUSINESS SERVICE RETAIL, RESTAURANTS AND SATELLITE AND BROADCAST RECEPTION FACILITY(IES) ARE ALSO PERMITTED IN THE COMMERCIAL OFFICE/RESEARCH & DEVELOPMENT AREAS. ALSO PERMITTED WITHIN THE COMMERCIAL OFFICE/RESEARCH & DEVELOPMENT AREA INCLUDING BUT NOT LIMITED TO DINING ROOM, HEALTH CLUB, CONFERENCE FACILITIES, AND MEETING ROOMS.
2. THE COMMERCIAL/HOTEL AREA SHALL PERMIT A MAXIMUM 175-ROOM HOTEL, INCLUDING RESTAURANT/DINING AND MEETING ROOMS, POOL, GARDEN AND RECREATION AREAS, COMMERCIAL SERVICE, RETAIL AND ANY SUPPORT SERVICES ESSENTIAL TO THE OPERATION OF THE HOTEL. ALSO PERMITTED WITHIN THE COMMERCIAL/HOTEL AREA ARE COMMON FACILITIES SUPPORTING THE COMMERCIAL OFFICE/RESEARCH & DEVELOPMENT AREA INCLUDING BUT NOT LIMITED TO DINING ROOM, HEALTH CLUB, CONFERENCING FACILITIES & MEETING ROOMS.
3. THE OPEN SPACE PRESERVE SHALL BE SET ASIDE FOR ENVIRONMENTAL AND EDUCATIONAL USES.

### **II. DEVELOPMENT STANDARDS**

1. DEVELOPMENT SIZE AND DENSITY
  - A. A MAXIMUM OF 1,09,000 GROSS SQUARE FEET (GSF) SHALL BE PERMITTED OF OFFICE/RESEARCH & DEVELOPMENT (R&D) USES.
  - B. PARKING SPACES SHALL BE PER THE ZONING CODE (OR AS AMENDED), AT GRADE AND IN THE PARKING STRUCTURE WITHIN THE COMMERCIAL OFFICE/RESEARCH & DEVELOPMENT ZONE.
  - C. A MAXIMUM 175 ROOM HOTEL INCLUDING DINING ROOMS, POOL AND GARDEN AREAS, AND ANY SUPPORT SERVICES REQUIRED FOR THE OPERATION OF THE ESTABLISHMENT.

2. HEIGHT

A. NO BUILDING TO EXCEED 90' IN HEIGHT TO TOP OF PARAPET. ROOFTOP EQUIPMENT SHALL BE SCREENED FROM PUBLIC VIEW. SUCH EQUIPMENT AND SCREEN WALLS SHALL BE EXEMPT FROM THE HEIGHT LIMIT, BUT SHALL NOT EXCEED 16'-0" IN HEIGHT ABOVE THE ROOF.

3. PERIMETER BUILDING SETBACKS

A. SETBACK FROM THE EAST PROPERTY LINE SHALL BE 20'-0" MINIMUM.

B. OFFICE/R&D BUILDINGS TO HAVE A MINIMUM SEPARATION OF 40 FEET. FUTURE SUBDIVISIONS WILL RESULT IN A MINIMUM SETBACK OF 20 FEET FROM ALL PARCEL LINES.

C. A 60' SETBACK FROM PERIMETER PROPERTY LINES ALONG HIGHWAY 237 AND SAN TOMAS AQUINO CREEK SHALL BE MAINTAINED.

D. THE RIPARIAN SETBACK FROM THE GUADALUPE RIVER SHALL BE 100' WITH THE FOLLOWING EXCEPTIONS:

4. OFF-STREET PARKING & LOADING

A. THE FOLLOWING MINIMUM OFF-STREET PARKING REQUIREMENTS SHALL BE USED FOR THE OFFICE/RESEARCH & DEVELOPMENT USES: CITY OF SAN JOSE PARKING STANDARDS TO BE MET; AS AMENDED:

B. THE HOTEL SHALL MAKE USE OF PARKING PROVIDED FOR THE OFFICE/RESEARCH & DEVELOPMENT; NO ADDITIONAL PARKING IS REQUIRED FOR THE HOTEL.

C. A MINIMUM OF ONE OFF-STREET LOADING SPACE IS REQUIRED FOR EACH OFFICE/RESEARCH & DEVELOPMENT BUILDING, AND TWO SPACES ARE REQUIRED FOR THE HOTEL.

5. PATHS AND BIKEWAYS

A. A NETWORK OF PATHWAYS CONNECTING TO THE SIDEWALKS ALONG INTERNAL PRIVATE STREETS SHALL BE PROVIDED TO CONNECT THE PLANNED DEVELOPMENT TO OTHER LOCAL AND REGIONAL PATHS AND BIKEWAYS.

B. A HIERARCHY OF MAJOR AND MINOR PRIVATE INTERNAL CIRCULATION STREETS AS ILLUSTRATED ON THIS PLAN SHALL BE MAINTAINED AS ILLUSTRATED ON THE PREVIOUSLY APPROVED PLAN SET PDC99-044 AND



CARRIED THROUGH ONTO THE CURRENT PLAN SET FOR FILE NUMBER PDC15-058.

### **III. EASEMENTS**

STRUCTURES TO BE CLEAR OF ALL P.U.C. EASEMENTS & RIPARIAN AREAS AS ILLUSTRATED ON THE LAND USE PLAN.

### **IV. PERFORMANCE STANDARDS**

OFFICE USE/HOTEL USE

ALL OFFICE DEVELOPMENT SHALL CONFORM TO THE PERFORMANCE STANDARDS OF THE IP INDUSTRIAL PARK ZONING DISTRICT AS AMENDED.

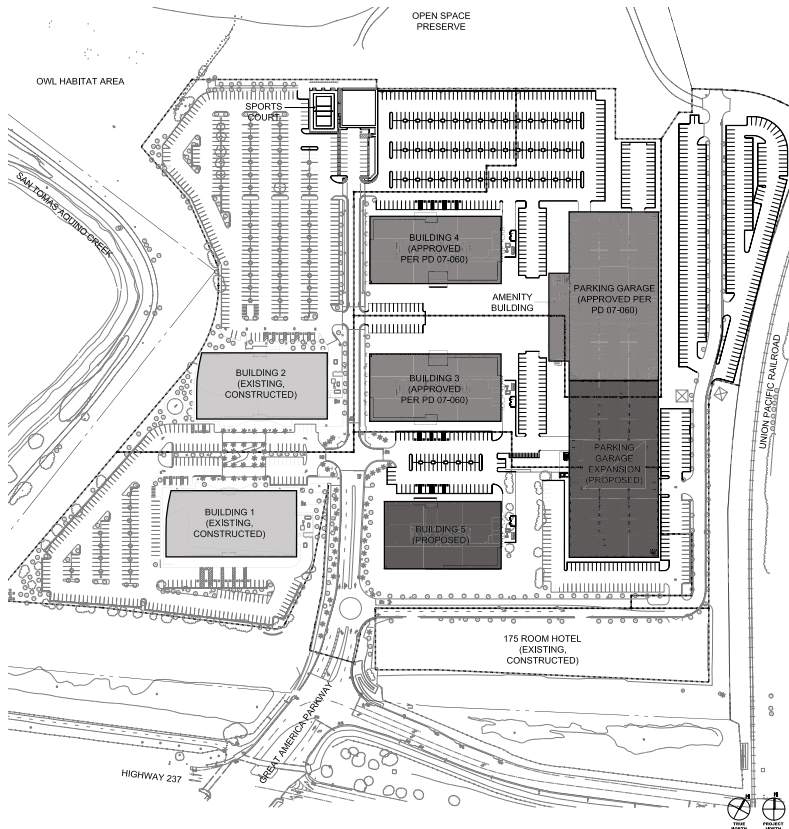
### **V. OFF-SITE IMPROVEMENTS**

#### **1. NORTH SAN JOSE DEFICIENCY PLAN**

THIS PROJECT IS LOCATED ADJACENT TO THE NORTH SAN JOSE DEFICIENCY PLAN AREA AND IS SUBJECT TO THE DEFICIENCY PLAN'S SITE ACTIONS IN ADDITION TO PARTICIPATING IN THE PAYMENT OF THE NORTH SAN JOSE DEFICIENCY PLAN FEE PRIOR TO PUBLIC WORKS CLEARANCE.

### **VI. ENVIRONMENTAL MITIGATION**

#### **1. IMPLEMENT THE SUBSEQUENT ADOPTED MITIGATION MONITORING AND REPORTING PROGRAM, AS AMENDED.**



# GENERAL DEVELOPMENT PLAN AMERICA CENTER

PLANNED DEVELOPMENT ZONING APPLICATION  
CITY OF SAN JOSE, CALIFORNIA  
PDC15-058

## PROJECT DESCRIPTION

PLANNED DEVELOPMENT REZONING AND PLANNED DEVELOPMENT PERMIT TO REZONE FROM THE AP(D) PLANNED DEVELOPMENT ZONING DISTRICT TO THE AP(D) PLANNED DEVELOPMENT ZONING DISTRICT TO ALLOW THE ADDITION OF 192,350 SQUARE FEET OF COMMERCIAL OFFICE SPACE AND TO ALLOW THE EXPANSION OF AN EXISTING PARKING GARAGE ON A 70.5 GROSS ACRE SITE.

## PREVIOUS APPROVALS

PDC09-044; PDC00-025; PD07-060; PDC03-024; AND PD12-048  
BUILDINGS 1, 2, 3, 4 AND THE HOTEL - CURRENT STATUS - ACTIVE

## AREA CALCULATIONS

TOTAL ACRES OF SUBJECT PROPERTY (INCLUDES OPEN SPACE PRESERVE)	
GROSS	70.5 ACRES
TOTAL BUILDING AREAS (GROSS SF)	
(NON-RESIDENTIAL USE, EXCLUDED PARKING GARAGE)	
OFFICE BUILDINGS	1,044,112 SF
AMENITY BUILDING	16,000 SF
HOTEL	81,350 SF
<b>TOTAL</b>	<b>1,141,462 SF</b>
TOTAL FOOTPRINT AREA (SF) OF BUILDINGS	
(INCLUDES HOTEL, PARKING GARAGE, AND OFFICE BUILDINGS)	
OFFICE BUILDINGS	174,513 SF
AMENITY BUILDING	8,000 SF
HOTEL	18,026 SF
<b>TOTAL</b>	<b>200,539 SF</b>
TOTAL ACRES DEVOTED TO LANDSCAPE AREA	37
PERCENTAGE OF SITE AREA	52.5%
FLOOR AREA RATIO FOR NON-RESIDENTIAL USES (SF OF BUILDINGS / SF OF SITE) (1,141,462 SF / 3,076,980 SF)	0.37

## SHEET INDEX

SHEET NUMBER	SHEET DESCRIPTION
1	TITLE SHEET
2	LAND USE PLAN
2A	DEVELOPMENT STANDARDS
3	OVERALL SITE PLAN
3A	ACCESSIBLE PATH OF TRAVEL SITE PLAN
3B	FIRE ACCESS SITE PLAN
4/01	FINAL GRADING PLAN
4/02	UTILITY PLAN
4/03	CIVIL DETAILS
4/04	SETTLEMENT EXHIBIT
5/01	STORMWATER PLAN
5/02	STORMWATER CONTROL PLAN
5/03	STORMWATER DETAILS
6	(NOT USED)
7A	BUILDING 5 ELEVATIONS
7B	GARAGE ELEVATIONS
8	(NOT USED)
9A	BUILDING 5 FLOOR PLANS
9B	GARAGE FLOOR PLANS
9C	GARAGE FLOOR PLANS
9D	GARAGE FLOOR PLANS
10/01	MATERIAL LEGEND
10/02	MATERIAL PLAN - SOUTH
10/03	MATERIAL PLAN - NORTH
10/04	CONSTRUCTION DETAILS
10/05	CONSTRUCTION DETAILS
10/06	CONSTRUCTION DETAILS
10/07	PLANTING LEGEND AND NOTES
10/08	PLANTING PLAN - SOUTH
10/09	PLANTING PLAN - NORTH
10/10	PLANTING DETAILS
10/11	IRRIGATION LEGEND & NOTES
10/12	IRRIGATION PLAN - SOUTH
10/13	IRRIGATION PLAN - NORTH
10/14	IRRIGATION DETAILS
10/15	IRRIGATION DETAILS
10/16	IRRIGATION DETAILS
11/01	OVERALL ELECTRICAL SITE PLAN
11/01	ELECTRICAL SITE LIGHTING PLAN
11/01	ELECTRICAL DETAILS

## PARKING SUMMARY

BUILDING AREA SUMMARY	
EXISTING BUILDING AREA (PHASE 1) APPROVED	420,094 SF
PROPOSED BUILDING AREA (PHASE 2) APPROVED	431,668 SF
PROPOSED BUILDING AREA (PHASE 3)	192,350 SF
<b>TOTAL AREA:</b>	<b>1,044,112 SF</b>
<b>TOTAL NET AREA:</b>	<b>887,496 SF</b>
PARKING SPACES REQUIRED	
NET BUILDING AREA / 250:	3,550 SPACES
PARKING SPACES PROVIDED	
SURFACE PARKING:	1,740 SPACES
GARAGE (PHASE 2) APPROVED PARKING:	800 SPACES
GARAGE (PHASE 3) PROPOSED:	1,070 SPACES
<b>TOTAL:</b>	<b>3,610 SPACES</b>
PARKING SUMMARY	
COMPACT SPACES ALLOWED (40%):	1,420 SPACES
COMPACT SPACES PROVIDED:	268 SPACES (GARAGES ONLY)
ACCESSIBLE PARKING SPACES REQUIRED (2%):	71 SPACES (12 VAN SPACES)
ACCESSIBLE PARKING SPACES PROVIDED:	71 SPACES
FUEL EFFICIENT / VEHICLE PARKING REQUIRED (8%):	284 SPACES
FUEL EFFICIENT / VEHICLE PARKING PROVIDED:	284 SPACES
MOTORCYCLE SPACES REQUIRED (1 PER 50 SPACES):	71 SPACES
MOTORCYCLE SPACES PROVIDED:	71 SPACES
BICYCLE SPACES REQUIRED (5% PER CALGREN):	176 SPACES
BICYCLE SPACES PROVIDED:	176 SPACES
OFF STREET LOADING SPACES REQUIRED:	2 PER BUILDING
OFF STREET LOADING SPACES PROVIDED:	2 PER BUILDING

[LRG]

Architecture | Interiors | Planning

1001 SOUTH ROBERTSON BLVD., 10TH FLOOR  
LOS ANGELES, CALIFORNIA 90005  
T: 310.262.9900  
F: 310.262.9900

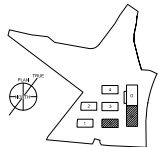
### OWNER

STEELWAVE  
8001 EAST 10TH AVE., SUITE 300  
DENVER, CO 80231  
TEL: 303.733.7000

### CONSULTANTS

**CIVIL ENGINEERING**  
BRY ENGINEERS  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.398.0000

**LANDSCAPE ARCHITECT**  
CARNOCHI AND ASSOCIATES  
1001 CALIFORNIA STREET, 11TH FLOOR  
SAN FRANCISCO, CA 94108  
TEL: 415.398.0000



NO.	ISSUE	DATE
1	NO ADJUSTMENT	03/01/12
2	CITY RESPONSE	03/26/12
3	NO ADJUSTMENT	03/26/12
4	NO ADJUSTMENT	03/26/12
5	NO ADJUSTMENT	03/26/12
6	NO ADJUSTMENT	03/26/12
7	NO ADJUSTMENT	03/26/12
8	NO ADJUSTMENT	03/26/12
9	NO ADJUSTMENT	03/26/12
10	NO ADJUSTMENT	03/26/12
11	NO ADJUSTMENT	03/26/12
12	NO ADJUSTMENT	03/26/12
13	NO ADJUSTMENT	03/26/12
14	NO ADJUSTMENT	03/26/12
15	NO ADJUSTMENT	03/26/12
16	NO ADJUSTMENT	03/26/12
17	NO ADJUSTMENT	03/26/12
18	NO ADJUSTMENT	03/26/12
19	NO ADJUSTMENT	03/26/12
20	NO ADJUSTMENT	03/26/12
21	NO ADJUSTMENT	03/26/12
22	NO ADJUSTMENT	03/26/12
23	NO ADJUSTMENT	03/26/12
24	NO ADJUSTMENT	03/26/12
25	NO ADJUSTMENT	03/26/12
26	NO ADJUSTMENT	03/26/12
27	NO ADJUSTMENT	03/26/12
28	NO ADJUSTMENT	03/26/12
29	NO ADJUSTMENT	03/26/12
30	NO ADJUSTMENT	03/26/12
31	NO ADJUSTMENT	03/26/12
32	NO ADJUSTMENT	03/26/12
33	NO ADJUSTMENT	03/26/12
34	NO ADJUSTMENT	03/26/12
35	NO ADJUSTMENT	03/26/12
36	NO ADJUSTMENT	03/26/12
37	NO ADJUSTMENT	03/26/12
38	NO ADJUSTMENT	03/26/12
39	NO ADJUSTMENT	03/26/12
40	NO ADJUSTMENT	03/26/12
41	NO ADJUSTMENT	03/26/12
42	NO ADJUSTMENT	03/26/12
43	NO ADJUSTMENT	03/26/12
44	NO ADJUSTMENT	03/26/12
45	NO ADJUSTMENT	03/26/12
46	NO ADJUSTMENT	03/26/12
47	NO ADJUSTMENT	03/26/12
48	NO ADJUSTMENT	03/26/12
49	NO ADJUSTMENT	03/26/12
50	NO ADJUSTMENT	03/26/12
51	NO ADJUSTMENT	03/26/12
52	NO ADJUSTMENT	03/26/12
53	NO ADJUSTMENT	03/26/12
54	NO ADJUSTMENT	03/26/12
55	NO ADJUSTMENT	03/26/12
56	NO ADJUSTMENT	03/26/12
57	NO ADJUSTMENT	03/26/12
58	NO ADJUSTMENT	03/26/12
59	NO ADJUSTMENT	03/26/12
60	NO ADJUSTMENT	03/26/12
61	NO ADJUSTMENT	03/26/12
62	NO ADJUSTMENT	03/26/12
63	NO ADJUSTMENT	03/26/12
64	NO ADJUSTMENT	03/26/12
65	NO ADJUSTMENT	03/26/12
66	NO ADJUSTMENT	03/26/12
67	NO ADJUSTMENT	03/26/12
68	NO ADJUSTMENT	03/26/12
69	NO ADJUSTMENT	03/26/12
70	NO ADJUSTMENT	03/26/12
71	NO ADJUSTMENT	03/26/12
72	NO ADJUSTMENT	03/26/12
73	NO ADJUSTMENT	03/26/12
74	NO ADJUSTMENT	03/26/12
75	NO ADJUSTMENT	03/26/12
76	NO ADJUSTMENT	03/26/12
77	NO ADJUSTMENT	03/26/12
78	NO ADJUSTMENT	03/26/12
79	NO ADJUSTMENT	03/26/12
80	NO ADJUSTMENT	03/26/12
81	NO ADJUSTMENT	03/26/12
82	NO ADJUSTMENT	03/26/12
83	NO ADJUSTMENT	03/26/12
84	NO ADJUSTMENT	03/26/12
85	NO ADJUSTMENT	03/26/12
86	NO ADJUSTMENT	03/26/12
87	NO ADJUSTMENT	03/26/12
88	NO ADJUSTMENT	03/26/12
89	NO ADJUSTMENT	03/26/12
90	NO ADJUSTMENT	03/26/12
91	NO ADJUSTMENT	03/26/12
92	NO ADJUSTMENT	03/26/12
93	NO ADJUSTMENT	03/26/12
94	NO ADJUSTMENT	03/26/12
95	NO ADJUSTMENT	03/26/12
96	NO ADJUSTMENT	03/26/12
97	NO ADJUSTMENT	03/26/12
98	NO ADJUSTMENT	03/26/12
99	NO ADJUSTMENT	03/26/12
100	NO ADJUSTMENT	03/26/12

STEELWAVE

AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

## TITLE SHEET

PROJECT 2015-001	SHEET NUMBER 1
SCALE N.T.S.	
DRAWN BY DATE 09/11/15	
DATE 09/11/15	

## OWNER

### PROJECT NAME

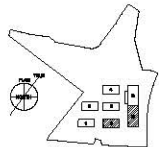
300 SOUTH FORTY-SEVENTH AVENUE, 3RD FLOOR  
LOS ANGELES, CALIFORNIA 90008  
P: 213.462.1800  
F: 213.462.1801

## CONSULTANT

### CIVIL ENGINEERING

300 SOUTH FORTY-SEVENTH AVENUE, 3RD FLOOR  
LOS ANGELES, CALIFORNIA 90008  
P: 213.462.1800  
F: 213.462.1801

LANDSCAPE ARCHITECT  
300 SOUTH FORTY-SEVENTH AVENUE, 3RD FLOOR  
LOS ANGELES, CALIFORNIA 90008  
P: 213.462.1800  
F: 213.462.1801

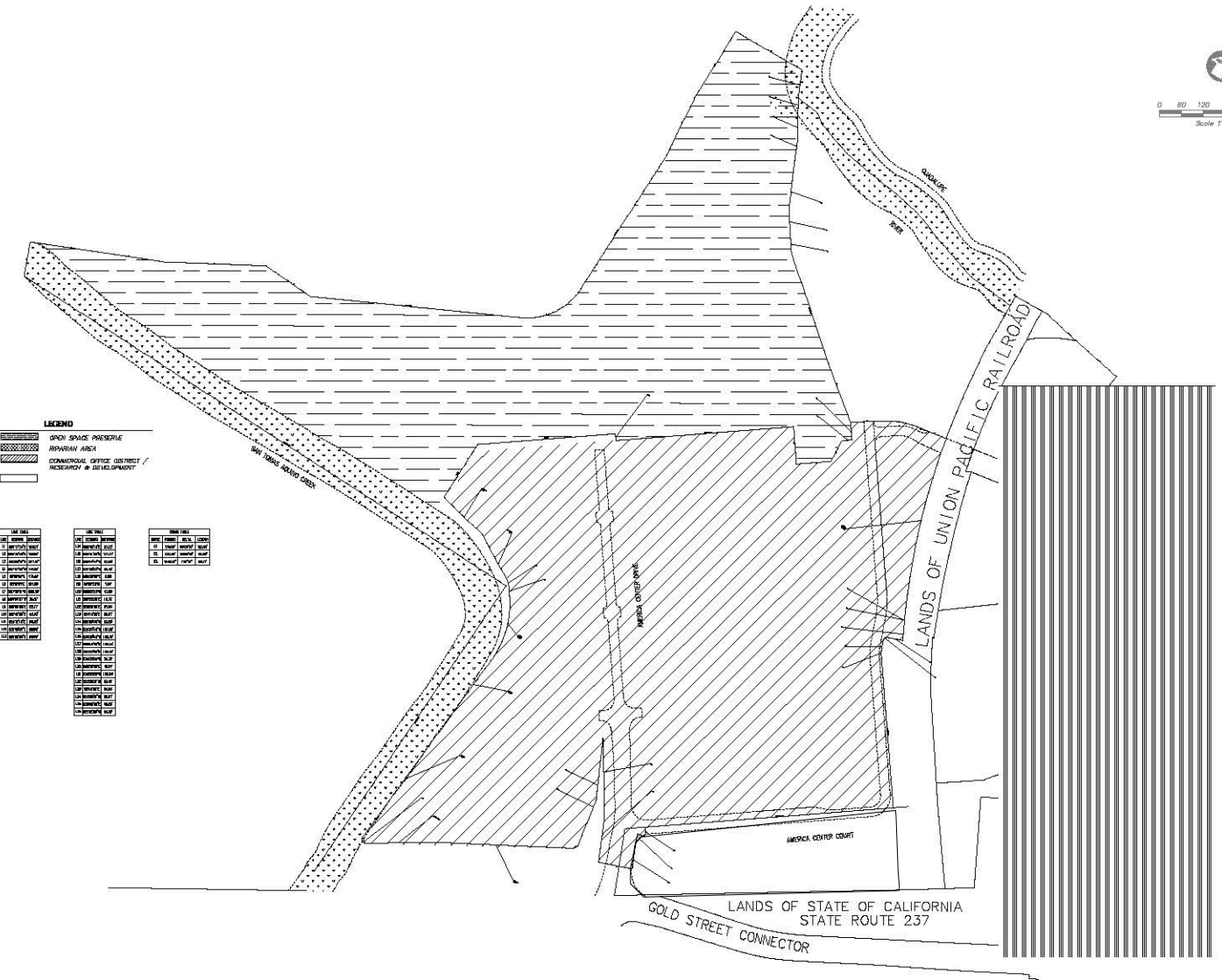
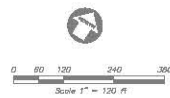


NO.	DATE	DATE
1	NOV 14, 2018	11/14/2018
2	NOV 14, 2018	11/14/2018
3	NOV 14, 2018	11/14/2018
4	NOV 14, 2018	11/14/2018
5	NOV 14, 2018	11/14/2018
6	NOV 14, 2018	11/14/2018
7	NOV 14, 2018	11/14/2018
8	NOV 14, 2018	11/14/2018
9	NOV 14, 2018	11/14/2018
10	NOV 14, 2018	11/14/2018
11	NOV 14, 2018	11/14/2018
12	NOV 14, 2018	11/14/2018
13	NOV 14, 2018	11/14/2018
14	NOV 14, 2018	11/14/2018
15	NOV 14, 2018	11/14/2018
16	NOV 14, 2018	11/14/2018
17	NOV 14, 2018	11/14/2018
18	NOV 14, 2018	11/14/2018
19	NOV 14, 2018	11/14/2018
20	NOV 14, 2018	11/14/2018



AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

## LAND USE PLAN



**LEGEND**

SPACED SPACE PATTERNS  
HATCHED AREA  
COMMERCIAL OFFICE DISTRICT / RESEARCH & DEVELOPMENT

LAND USE	LAND USE	LAND USE
1. OFFICE	2. OFFICE	3. OFFICE
4. OFFICE	5. OFFICE	6. OFFICE
7. OFFICE	8. OFFICE	9. OFFICE
10. OFFICE	11. OFFICE	12. OFFICE
13. OFFICE	14. OFFICE	15. OFFICE
16. OFFICE	17. OFFICE	18. OFFICE
19. OFFICE	20. OFFICE	21. OFFICE
22. OFFICE	23. OFFICE	24. OFFICE
25. OFFICE	26. OFFICE	27. OFFICE
28. OFFICE	29. OFFICE	30. OFFICE
31. OFFICE	32. OFFICE	33. OFFICE
34. OFFICE	35. OFFICE	36. OFFICE
37. OFFICE	38. OFFICE	39. OFFICE
40. OFFICE	41. OFFICE	42. OFFICE
43. OFFICE	44. OFFICE	45. OFFICE
46. OFFICE	47. OFFICE	48. OFFICE
49. OFFICE	50. OFFICE	51. OFFICE
52. OFFICE	53. OFFICE	54. OFFICE
55. OFFICE	56. OFFICE	57. OFFICE
58. OFFICE	59. OFFICE	60. OFFICE
61. OFFICE	62. OFFICE	63. OFFICE
64. OFFICE	65. OFFICE	66. OFFICE
67. OFFICE	68. OFFICE	69. OFFICE
70. OFFICE	71. OFFICE	72. OFFICE
73. OFFICE	74. OFFICE	75. OFFICE
76. OFFICE	77. OFFICE	78. OFFICE
79. OFFICE	80. OFFICE	81. OFFICE
82. OFFICE	83. OFFICE	84. OFFICE
85. OFFICE	86. OFFICE	87. OFFICE
88. OFFICE	89. OFFICE	90. OFFICE
91. OFFICE	92. OFFICE	93. OFFICE
94. OFFICE	95. OFFICE	96. OFFICE
97. OFFICE	98. OFFICE	99. OFFICE
100. OFFICE	101. OFFICE	102. OFFICE

FOR REFERENCE ONLY

12.12.06

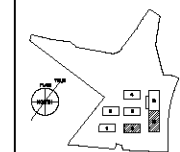
**OTHER**

STEEL BRIDGE  
4001 EAST TOWNSHIP AVENUE, SUITE 200  
PORTER, OH, 44089  
TEL. 800/572-2000

## CONSULTANTS

**CIVIL ENGINEERING**  
**INT. ENGINEERING**  
 380 CALIFORNIA STREET  
 SAN FRANCISCO, CA 94111  
 TEL: 415/398-7200

**LANDSCAPE ARCHITECT**  
OVERSIC AND ASSOCIATES  
800 BIRCH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94103  
415 / 398-7800

[illegible]

**AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA**

## DEVELOPMENT STANDARDS

PROJECT  
2015-001  
SCALE  
MTB  
EXAMIN BY  
-  
DATE  
08/11/15

**© 2007 WILEY**

**2A**

08/12/15  
01/07/2016

ONE SOUTH FIRST STREET, SUITE 200  
SAN JOSE, CALIFORNIA 95113  
TEL: 408.281.1111  
WWW.LRGARCHITECTS.COM

OWNER

AMERICA CENTER  
201 SOUTH FIRST STREET, SUITE 200  
SAN JOSE, CALIFORNIA 95113  
TEL: 408.281.1111

CONSULTANT

CIVIL ENGINEERING  
101 CALIFORNIA STREET  
SAN FRANCISCO, CALIFORNIA 94111  
TEL: 415.774.1111

LANDSCAPE ARCHITECT  
101 CALIFORNIA STREET  
SAN FRANCISCO, CALIFORNIA 94111  
TEL: 415.774.1111



NO.	DATE	DATE
1	NO. 1	NO. 1
2	NO. 2	NO. 2
3	NO. 3	NO. 3
4	NO. 4	NO. 4
5	NO. 5	NO. 5
6	NO. 6	NO. 6
7	NO. 7	NO. 7
8	NO. 8	NO. 8
9	NO. 9	NO. 9
10	NO. 10	NO. 10
11	NO. 11	NO. 11
12	NO. 12	NO. 12
13	NO. 13	NO. 13
14	NO. 14	NO. 14
15	NO. 15	NO. 15
16	NO. 16	NO. 16
17	NO. 17	NO. 17
18	NO. 18	NO. 18
19	NO. 19	NO. 19
20	NO. 20	NO. 20
21	NO. 21	NO. 21
22	NO. 22	NO. 22
23	NO. 23	NO. 23
24	NO. 24	NO. 24
25	NO. 25	NO. 25
26	NO. 26	NO. 26
27	NO. 27	NO. 27
28	NO. 28	NO. 28
29	NO. 29	NO. 29
30	NO. 30	NO. 30
31	NO. 31	NO. 31
32	NO. 32	NO. 32
33	NO. 33	NO. 33
34	NO. 34	NO. 34
35	NO. 35	NO. 35
36	NO. 36	NO. 36
37	NO. 37	NO. 37
38	NO. 38	NO. 38
39	NO. 39	NO. 39
40	NO. 40	NO. 40
41	NO. 41	NO. 41
42	NO. 42	NO. 42
43	NO. 43	NO. 43
44	NO. 44	NO. 44
45	NO. 45	NO. 45
46	NO. 46	NO. 46
47	NO. 47	NO. 47
48	NO. 48	NO. 48
49	NO. 49	NO. 49
50	NO. 50	NO. 50
51	NO. 51	NO. 51
52	NO. 52	NO. 52
53	NO. 53	NO. 53
54	NO. 54	NO. 54
55	NO. 55	NO. 55
56	NO. 56	NO. 56
57	NO. 57	NO. 57
58	NO. 58	NO. 58
59	NO. 59	NO. 59
60	NO. 60	NO. 60
61	NO. 61	NO. 61
62	NO. 62	NO. 62
63	NO. 63	NO. 63
64	NO. 64	NO. 64
65	NO. 65	NO. 65
66	NO. 66	NO. 66
67	NO. 67	NO. 67
68	NO. 68	NO. 68
69	NO. 69	NO. 69
70	NO. 70	NO. 70
71	NO. 71	NO. 71
72	NO. 72	NO. 72
73	NO. 73	NO. 73
74	NO. 74	NO. 74
75	NO. 75	NO. 75
76	NO. 76	NO. 76
77	NO. 77	NO. 77
78	NO. 78	NO. 78
79	NO. 79	NO. 79
80	NO. 80	NO. 80
81	NO. 81	NO. 81
82	NO. 82	NO. 82
83	NO. 83	NO. 83
84	NO. 84	NO. 84
85	NO. 85	NO. 85
86	NO. 86	NO. 86
87	NO. 87	NO. 87
88	NO. 88	NO. 88
89	NO. 89	NO. 89
90	NO. 90	NO. 90
91	NO. 91	NO. 91
92	NO. 92	NO. 92
93	NO. 93	NO. 93
94	NO. 94	NO. 94
95	NO. 95	NO. 95
96	NO. 96	NO. 96
97	NO. 97	NO. 97
98	NO. 98	NO. 98
99	NO. 99	NO. 99
100	NO. 100	NO. 100



AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

DEVELOPMENT  
STANDARDS

OPEN SPACE  
PRESERVE

CONCEPTUAL

OWL HABITAT AREA

SPORTS  
COURT

BUILDING 4  
(APPROVED  
PER PD 07-060)

PARKING GARAGE  
(APPROVED PER  
PD 07-060)

AMENITY  
BUILDING

BUILDING 2  
(EXISTING,  
CONSTRUCTED)

BUILDING 3  
(APPROVED  
PER PD 07-060)

PARKING GARAGE  
EXPANSION  
(PROPOSED)

BUILDING 1  
(EXISTING,  
CONSTRUCTED)

BUILDING 5  
(PROPOSED)

175 ROOM HOTEL  
(EXISTING,  
CONSTRUCTED)

UNION PACIFIC RAILROAD

HIGHWAY 237

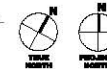
GREAT AMERICA PARKWAY

### LEGEND

--- PARCEL LINE /  
ASSUMED PROPERTY LINE BETWEEN BUILDINGS

### BUILDING AREA SUMMARY

PHASE 1 (APPROVED):	
BUILDING 1	210,047 SF
BUILDING 2	210,047 SF
TOTAL	420,094 SF
PHASE 2 (APPROVED):	
BUILDING 3	213,804 SF
BUILDING 4	213,804 SF
GARAGE AND AMENITY BUILDING	260,412 SF
TOTAL	688,020 SF
PHASE 3 (PROPOSED):	
BUILDING 5	192,300 SF
GARAGE	332,100 SF
TOTAL	524,400 SF



**01 OVERALL SITE PLAN**  
SCALE: 1" = 50'-0"

**[LRG]**

Architecture | Interiors | Planning

300 SOUTH SAN JOSE AVENUE, SUITE 100  
SAN JOSE, CA 95128  
TEL: (408) 281-1000  
WWW.LRGARCHITECTS.COM

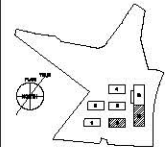
### SOURCES

ATLAS 2000  
2000 CITY OF SAN JOSE, CALIFORNIA  
2000 CITY OF SAN JOSE, CALIFORNIA  
2000 CITY OF SAN JOSE, CALIFORNIA

### CONSULTANTS

CIVIL ENGINEERING  
2000 CITY OF SAN JOSE, CALIFORNIA  
2000 CITY OF SAN JOSE, CALIFORNIA  
2000 CITY OF SAN JOSE, CALIFORNIA

LANDSCAPE ARCHITECT  
2000 CITY OF SAN JOSE, CALIFORNIA  
2000 CITY OF SAN JOSE, CALIFORNIA  
2000 CITY OF SAN JOSE, CALIFORNIA



NO.	DATE	DATE
1	10/10/10	10/10/10
2	10/10/10	10/10/10
3	10/10/10	10/10/10
4	10/10/10	10/10/10
5	10/10/10	10/10/10
6	10/10/10	10/10/10
7	10/10/10	10/10/10
8	10/10/10	10/10/10
9	10/10/10	10/10/10
10	10/10/10	10/10/10

STEELWAVE

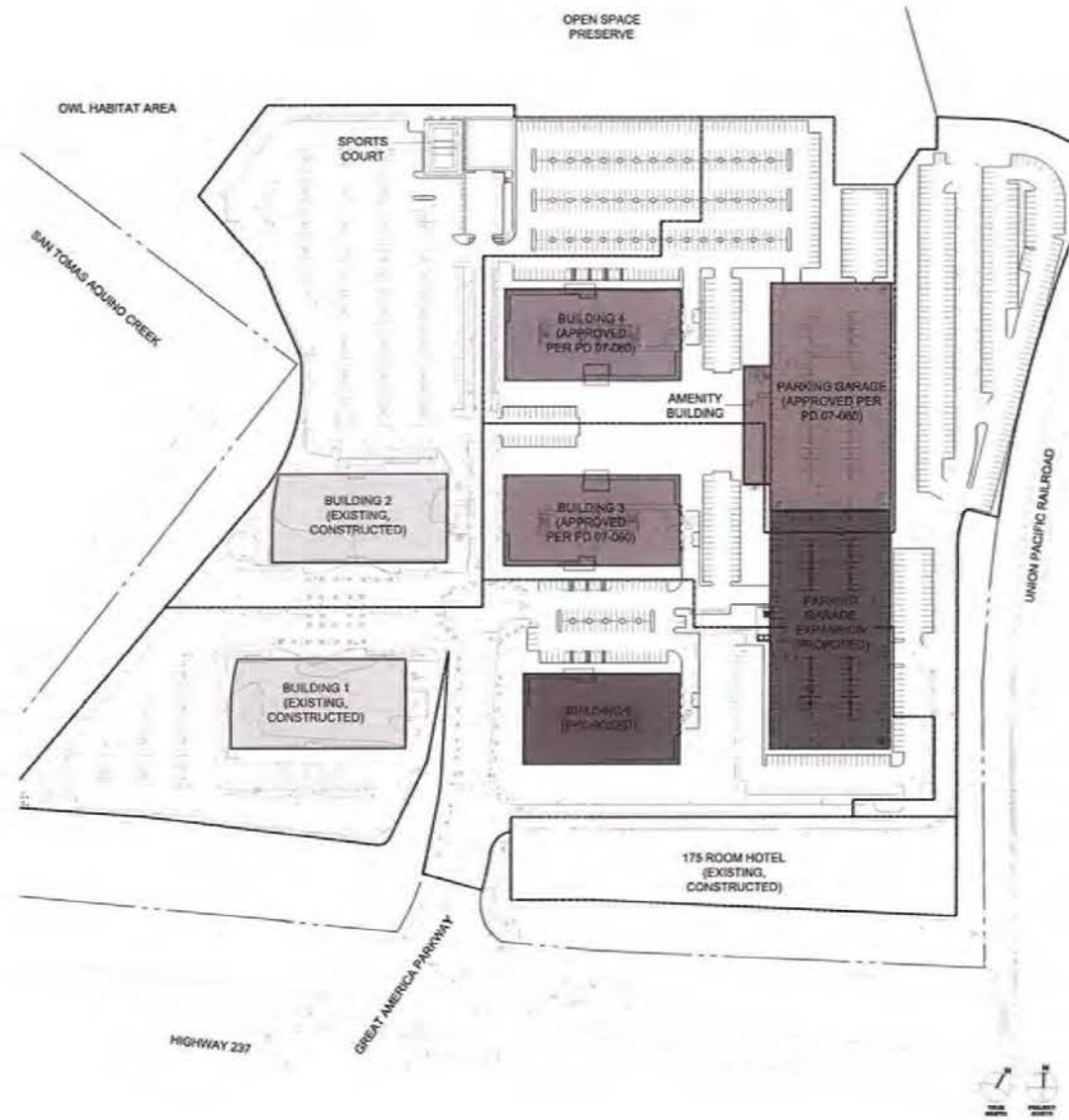
AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

CONCEPTUAL  
OVERALL SITE PLAN

PROJECT:  
2010-001  
SCALE:  
1"=50'  
DATE:  
09/11/10

3





## GENERAL DEVELOPMENT PLAN AMERICA CENTER

PLANNED DEVELOPMENT PERMIT APPLICATION  
CITY OF SAN JOSE, CALIFORNIA

### PROJECT DESCRIPTION

PLANNED DEVELOPMENT REZONING AND PLANNED DEVELOPMENT PERMIT TO REZONE FROM THE A(PD) PLANNED DEVELOPMENT ZONING DISTRICT TO THE A(PD) PLANNED DEVELOPMENT ZONING DISTRICT TO ALLOW THE ADDITION OF 192,350 SQUARE FEET OF COMMERCIAL OFFICE SPACE AND TO ALLOW THE EXPANSION OF AN EXISTING PARKING GARAGE ON A 70.5 GROSS ACRE SITE.

### PREVIOUS APPROVALS

POC99-044; POC00-025; POC07-050; POC03-024; AND POC12-048  
BUILDINGS 1, 2, 3, 4 AND THE HOTEL - CURRENT STATUS - ACTIVE

### AREA CALCULATIONS

TOTAL ACRES OF SUBJECT PROPERTY (INCLUDES OPEN SPACE PRESERVE):	
GROSS	70.5 ACRES
TOTAL BUILDING AREAS (GROSS SF)	
(NON-RESIDENTIAL USE, EXCLUDED PARKING GARAGE)	
OFFICE BUILDINGS	1,044,112 SF
AMENITY BUILDING	16,000 SF
HOTEL	84,350 SF
TOTAL	1,144,462 SF
TOTAL FOOTPRINT AREA (SF) OF BUILDINGS	
(INCLUDES HOTEL, PARKING GARAGE, AND OFFICE BUILDINGS)	
OFFICE BUILDINGS	174,513 SF
AMENITY BUILDING	8,000 SF
HOTEL	16,000 SF
TOTAL	200,539 SF
TOTAL ACRES DEVOTED TO LANDSCAPE AREA	37
PERCENTAGE OF SITE AREA	52.5%
FLOOR AREA RATIO FOR NON-RESIDENTIAL USES	0.37
(SF OF BUILDINGS / SF OF SITE)	(1,144,462 SF / 3,070,980 SF)

### SHEET INDEX

SHEET NUMBER	SHEET DESCRIPTION
1	TITLE SHEET
2	LAND USE PLAN
2A	DEVELOPMENT STANDARDS
3	OVERALL SITE PLAN
3A	ACCESSIBLE PATH OF TRAVEL SITE PLAN
3B	FIRE ACCESS SITE PLAN
4.01	FINAL GRADING PLAN
4.02	UTILITY PLAN
4.03	CIVIL DETAILS
4.04	SETTLEMENT EXHIBIT
5.01	STORMWATER PLAN
5.02	STORMWATER CONTROL PLAN
5.03	STORMWATER DETAILS
6	(NOT USED)
7A	BUILDING 5 ELEVATIONS
7B	GARAGE ELEVATIONS
8	(NOT USED)
9A	BUILDING 5 FLOOR PLANS
9B	GARAGE FLOOR PLANS
9C	GARAGE FLOOR PLANS
9D	GARAGE FLOOR PLANS
10.01	MATERIAL LEGEND
10.02	MATERIAL PLAN - SOUTH
10.03	MATERIAL PLAN - NORTH
10.04	CONSTRUCTION DETAILS
10.05	CONSTRUCTION DETAILS
10.06	PLANTING LEGEND AND NOTES
10.07	PLANTING PLAN - SOUTH
10.08	PLANTING PLAN - NORTH
10.09	PLANTING DETAILS
10.10	IRRIGATION LEGEND & NOTES
10.11	IRRIGATION PLAN - SOUTH
10.12	IRRIGATION PLAN - NORTH
10.13	IRRIGATION DETAILS
10.14	IRRIGATION DETAILS
10.15	IRRIGATION DETAILS
10.16	IRRIGATION DETAILS
11.01	OVERALL ELECTRICAL SITE PLAN
11.02	ELECTRICAL SITE LIGHTING PLAN
11.03	ELECTRICAL DETAILS

[LRG]

Architecture | Interiors | Planning

1001 SOUTH ROBERTSON BLVD. 2ND FLOOR  
LOS ANGELES, CALIFORNIA 90008  
P: 310.582.1888  
F: 310.582.1888

#### OWNER

STEELWAVE  
4000 EAST THIRD AVENUE, SUITE 500  
PACIFIC CITY, CA 94044  
TEL: 408.671.0000

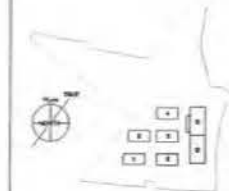
#### CONSULTANTS

##### CIVIL ENGINEERING

BAF ENGINEERS  
100 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.394.7888

##### LANDSCAPE ARCHITECT

CARBONET AND ASSOCIATES  
800 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94109  
TEL: 415.874.3888



NO.	ISSUE	DATE
1	PO ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/24/12
3	PO ADJUSTMENT	02/13/13
4	PO ADJUSTMENT	11/05/13
5	PO AMENDMENT	04/05/15
6	PO AMENDMENT	08/11/15
7	PO AMENDMENT REZONING	01/29/16

STEELWAVE

AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

### TITLE SHEET

PROJECT 2015-001	SHEET NUMBER 1
SCALE NTS	
DRAWN BY -	
DATE 08/11/15	

OWNER

STEELWAVE  
4000 EAST THIRD AVENUE, SUITE 500  
PORTER CITY, CA 94644  
TEL: 509.571.2200

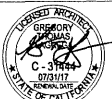
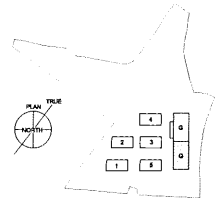
CONSULTANTS

CIVIL ENGINEERING

BNP ENGINEERS  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.261.7900

LANDSCAPE ARCHITECT

CARDUCCI AND ASSOCIATES  
245 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415.874.8900



NO.	ISSUE	DATE
	PD ADJUSTMENT	10/01/12
A	CITY RESPONSE	10/24/12
A	PD ADJUSTMENT	02/13/13
A	PD ADJUSTMENT	11/08/13
A	PD AMENDMENT	04/06/15
A	PD AMENDMENT	09/11/15
A	PD AMENDMENT RESUBMITTAL	01/29/16



AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

LAND USE PLAN

PROJECT

2015-001

SCALE

NTS

DRAWN BY

DATE

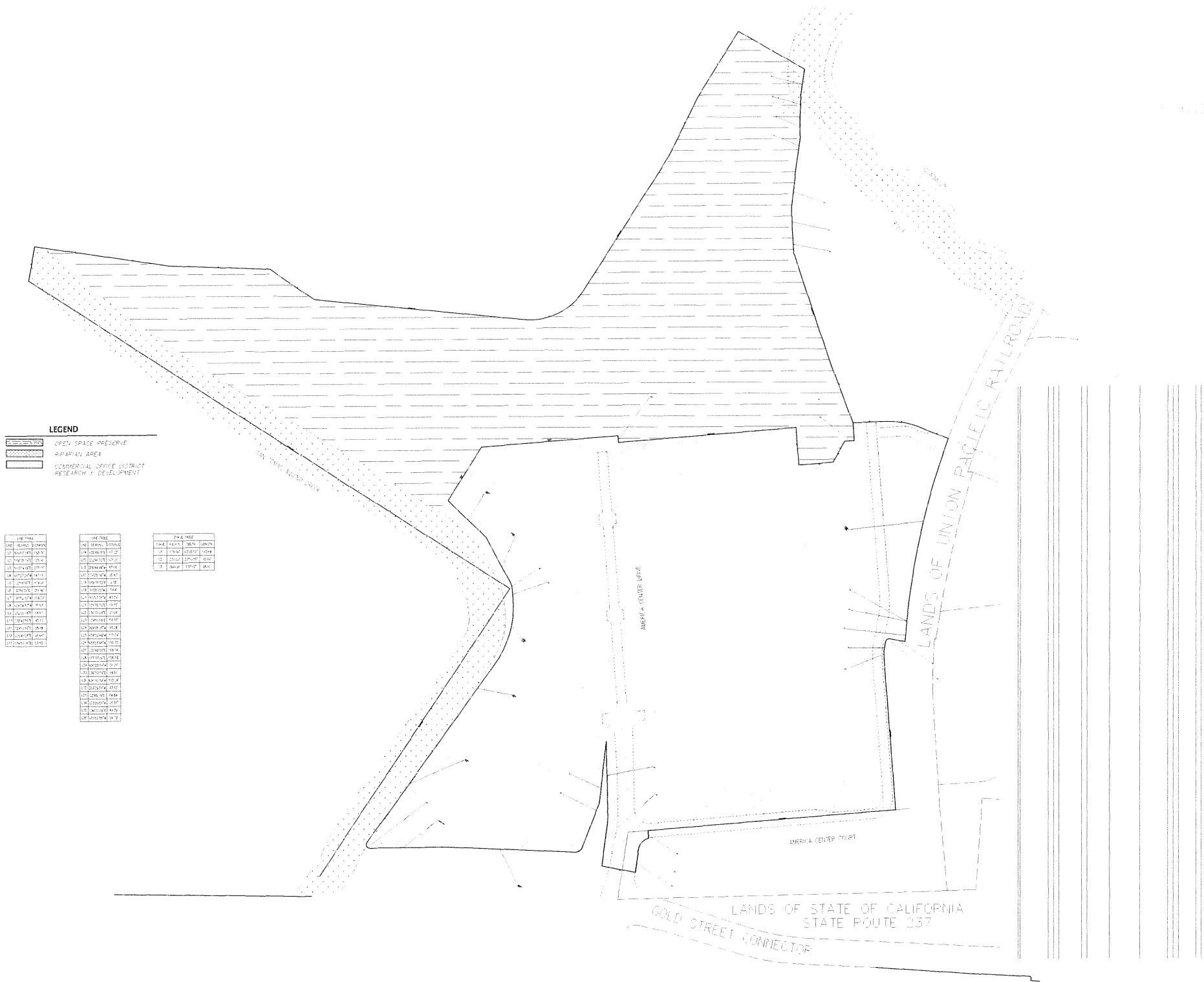
09/11/15

© LRG architecture

SHEET NUMBER

2

FOR REFERENCE ONLY



LEGEND

[Symbol]	OPEN SPACE PRESERVE
[Symbol]	RIPIARIAN AREA
[Symbol]	COMMERCIAL OFFICE DISTRICT
[Symbol]	RESEARCH & DEVELOPMENT

LINE TYPE	LINE TYPE	LINE TYPE
1. 0.0000 0.0000	2. 0.0000 0.0000	3. 0.0000 0.0000
4. 0.0000 0.0000	5. 0.0000 0.0000	6. 0.0000 0.0000
7. 0.0000 0.0000	8. 0.0000 0.0000	9. 0.0000 0.0000
10. 0.0000 0.0000	11. 0.0000 0.0000	12. 0.0000 0.0000
13. 0.0000 0.0000	14. 0.0000 0.0000	15. 0.0000 0.0000
16. 0.0000 0.0000	17. 0.0000 0.0000	18. 0.0000 0.0000
19. 0.0000 0.0000	20. 0.0000 0.0000	21. 0.0000 0.0000
22. 0.0000 0.0000	23. 0.0000 0.0000	24. 0.0000 0.0000
25. 0.0000 0.0000	26. 0.0000 0.0000	27. 0.0000 0.0000
28. 0.0000 0.0000	29. 0.0000 0.0000	30. 0.0000 0.0000
31. 0.0000 0.0000	32. 0.0000 0.0000	33. 0.0000 0.0000
34. 0.0000 0.0000	35. 0.0000 0.0000	36. 0.0000 0.0000
37. 0.0000 0.0000	38. 0.0000 0.0000	39. 0.0000 0.0000
40. 0.0000 0.0000	41. 0.0000 0.0000	42. 0.0000 0.0000
43. 0.0000 0.0000	44. 0.0000 0.0000	45. 0.0000 0.0000
46. 0.0000 0.0000	47. 0.0000 0.0000	48. 0.0000 0.0000
49. 0.0000 0.0000	50. 0.0000 0.0000	51. 0.0000 0.0000
52. 0.0000 0.0000	53. 0.0000 0.0000	54. 0.0000 0.0000
55. 0.0000 0.0000	56. 0.0000 0.0000	57. 0.0000 0.0000
58. 0.0000 0.0000	59. 0.0000 0.0000	60. 0.0000 0.0000
61. 0.0000 0.0000	62. 0.0000 0.0000	63. 0.0000 0.0000
64. 0.0000 0.0000	65. 0.0000 0.0000	66. 0.0000 0.0000
67. 0.0000 0.0000	68. 0.0000 0.0000	69. 0.0000 0.0000
70. 0.0000 0.0000	71. 0.0000 0.0000	72. 0.0000 0.0000
73. 0.0000 0.0000	74. 0.0000 0.0000	75. 0.0000 0.0000
76. 0.0000 0.0000	77. 0.0000 0.0000	78. 0.0000 0.0000
79. 0.0000 0.0000	80. 0.0000 0.0000	81. 0.0000 0.0000
82. 0.0000 0.0000	83. 0.0000 0.0000	84. 0.0000 0.0000
85. 0.0000 0.0000	86. 0.0000 0.0000	87. 0.0000 0.0000
88. 0.0000 0.0000	89. 0.0000 0.0000	90. 0.0000 0.0000
91. 0.0000 0.0000	92. 0.0000 0.0000	93. 0.0000 0.0000
94. 0.0000 0.0000	95. 0.0000 0.0000	96. 0.0000 0.0000
97. 0.0000 0.0000	98. 0.0000 0.0000	99. 0.0000 0.0000
100. 0.0000 0.0000	101. 0.0000 0.0000	102. 0.0000 0.0000
103. 0.0000 0.0000	104. 0.0000 0.0000	105. 0.0000 0.0000
106. 0.0000 0.0000	107. 0.0000 0.0000	108. 0.0000 0.0000
109. 0.0000 0.0000	110. 0.0000 0.0000	111. 0.0000 0.0000
112. 0.0000 0.0000	113. 0.0000 0.0000	114. 0.0000 0.0000
115. 0.0000 0.0000	116. 0.0000 0.0000	117. 0.0000 0.0000
118. 0.0000 0.0000	119. 0.0000 0.0000	120. 0.0000 0.0000
121. 0.0000 0.0000	122. 0.0000 0.0000	123. 0.0000 0.0000
124. 0.0000 0.0000	125. 0.0000 0.0000	126. 0.0000 0.0000
127. 0.0000 0.0000	128. 0.0000 0.0000	129. 0.0000 0.0000
130. 0.0000 0.0000	131. 0.0000 0.0000	132. 0.0000 0.0000
133. 0.0000 0.0000	134. 0.0000 0.0000	135. 0.0000 0.0000
136. 0.0000 0.0000	137. 0.0000 0.0000	138. 0.0000 0.0000
139. 0.0000 0.0000	140. 0.0000 0.0000	141. 0.0000 0.0000
142. 0.0000 0.0000	143. 0.0000 0.0000	144. 0.0000 0.0000
145. 0.0000 0.0000	146. 0.0000 0.0000	147. 0.0000 0.0000
148. 0.0000 0.0000	149. 0.0000 0.0000	150. 0.0000 0.0000
151. 0.0000 0.0000	152. 0.0000 0.0000	153. 0.0000 0.0000
154. 0.0000 0.0000	155. 0.0000 0.0000	156. 0.0000 0.0000
157. 0.0000 0.0000	158. 0.0000 0.0000	159. 0.0000 0.0000
160. 0.0000 0.0000	161. 0.0000 0.0000	162. 0.0000 0.0000
163. 0.0000 0.0000	164. 0.0000 0.0000	165. 0.0000 0.0000
166. 0.0000 0.0000	167. 0.0000 0.0000	168. 0.0000 0.0000
169. 0.0000 0.0000	170. 0.0000 0.0000	171. 0.0000 0.0000
172. 0.0000 0.0000	173. 0.0000 0.0000	174. 0.0000 0.0000
175. 0.0000 0.0000	176. 0.0000 0.0000	177. 0.0000 0.0000
178. 0.0000 0.0000	179. 0.0000 0.0000	180. 0.0000 0.0000
181. 0.0000 0.0000	182. 0.0000 0.0000	183. 0.0000 0.0000
184. 0.0000 0.0000	185. 0.0000 0.0000	186. 0.0000 0.0000
187. 0.0000 0.0000	188. 0.0000 0.0000	189. 0.0000 0.0000
190. 0.0000 0.0000	191. 0.0000 0.0000	192. 0.0000 0.0000
193. 0.0000 0.0000	194. 0.0000 0.0000	195. 0.0000 0.0000
196. 0.0000 0.0000	197. 0.0000 0.0000	198. 0.0000 0.0000
199. 0.0000 0.0000	200. 0.0000 0.0000	201. 0.0000 0.0000
202. 0.0000 0.0000	203. 0.0000 0.0000	204. 0.0000 0.0000
205. 0.0000 0.0000	206. 0.0000 0.0000	207. 0.0000 0.0000
208. 0.0000 0.0000	209. 0.0000 0.0000	210. 0.0000 0.0000
211. 0.0000 0.0000	212. 0.0000 0.0000	213. 0.0000 0.0000
214. 0.0000 0.0000	215. 0.0000 0.0000	216. 0.0000 0.0000
217. 0.0000 0.0000	218. 0.0000 0.0000	219. 0.0000 0.0000
220. 0.0000 0.0000	221. 0.0000 0.0000	222. 0.0000 0.0000
223. 0.0000 0.0000	224. 0.0000 0.0000	225. 0.0000 0.0000
226. 0.0000 0.0000	227. 0.0000 0.0000	228. 0.0000 0.0000
229. 0.0000 0.0000	230. 0.0000 0.0000	231. 0.0000 0.0000
232. 0.0000 0.0000	233. 0.0000 0.0000	234. 0.0000 0.0000
235. 0.0000 0.0000	236. 0.0000 0.0000	237. 0.0000 0.0000
238. 0.0000 0.0000	239. 0.0000 0.0000	240. 0.0000 0.0000
241. 0.0000 0.0000	242. 0.0000 0.0000	243. 0.0000 0.0000
244. 0.0000 0.0000	245. 0.0000 0.0000	246. 0.0000 0.0000
247. 0.0000 0.0000	248. 0.0000 0.0000	249. 0.0000 0.0000
250. 0.0000 0.0000	251. 0.0000 0.0000	252. 0.0000 0.0000
253. 0.0000 0.0000	254. 0.0000 0.0000	255. 0.0000 0.0000
256. 0.0000 0.0000	257. 0.0000 0.0000	258. 0.0000 0.0000
259. 0.0000 0.0000	260. 0.0000 0.0000	261. 0.0000 0.0000
262. 0.0000 0.0000	263. 0.0000 0.0000	264. 0.0000 0.0000
265. 0.0000 0.0000	266. 0.0000 0.0000	267. 0.0000 0.0000
268. 0.0000 0.0000	269. 0.0000 0.0000	270. 0.0000 0.0000
271. 0.0000 0.0000	272. 0.0000 0.0000	273. 0.0000 0.0000
274. 0.0000 0.0000	275. 0.0000 0.0000	276. 0.0000 0.0000
277. 0.0000 0.0000	278. 0.0000 0.0000	279. 0.0000 0.0000
280. 0.0000 0.0000	281. 0.0000 0.0000	282. 0.0000 0.0000
283. 0.0000 0.0000	284. 0.0000 0.0000	285. 0.0000 0.0000
286. 0.0000 0.0000	287. 0.0000 0.0000	288. 0.0000 0.0000
289. 0.0000 0.0000	290. 0.0000 0.0000	291. 0.0000 0.0000
292. 0.0000 0.0000	293. 0.0000 0.0000	294. 0.0000 0.0000
295. 0.0000 0.0000	296. 0.0000 0.0000	297. 0.0000 0.0000
298. 0.0000 0.0000	299. 0.0000 0.0000	300. 0.0000 0.0000



OWNER

STEELWAVE

4000 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650.571.2200

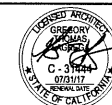
## CONSULTANTS

## CIVIL ENGINEERING

**CIVIL ENGINEERING**  
BKF ENGINEERS  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415 390 7800

## LANDSCAPE ARCHITECT

**LANDSCAPE ARCHITECTS**  
CARDUCCI AND ASSOCIATES  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415.874.0900

[illegible]

**AMERICA CENTER**  
AMERICA CENTER DRIVE  
SAN JOSE, CA

DEVELOPMENT  
STANDARDS

PROJECT  
2015-001  
SCALE  
NTS  
DRAWN BY  
-  
DATE  
09/11/15

SHEET NUMBER

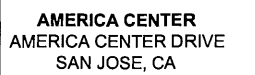
**2A**

**● LARGE**

OWNER

### CONSULTANTS

**LANDSCAPE ARCHITECT**  
CARDUCCI AND ASSOCIATES  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415.874.0990

[illegible]

PROJECT  
2015-001  
SCALE  
NTS  
DRAWN BY  
-  
DATE  
09/11/15

**2B**

## OWNER

STEELWAVE  
4000 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650.571.2200

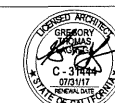
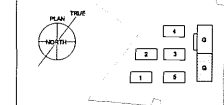
## CONSULTANTS

### CIVIL ENGINEERING

BNP ENGINEERS  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.395.7900

### LANDSCAPE ARCHITECT

CARDUCCI AND ASSOCIATES  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415.674.9960



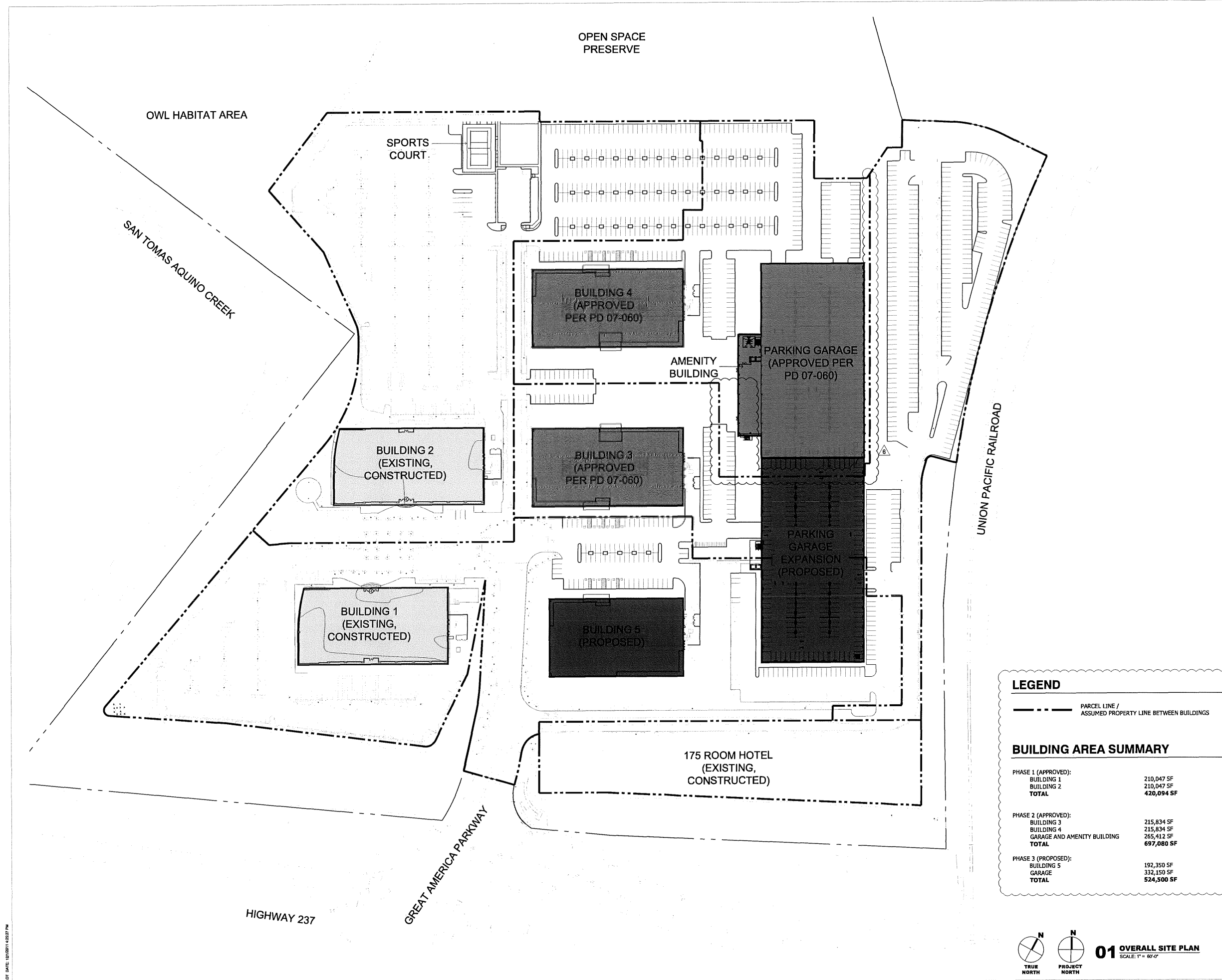
NO.	ISSUE	DATE
	PD ADJUSTMENT	10/01/12
A	CITY RESPONSE	10/24/12
A	PD ADJUSTMENT	02/13/13
A	PD ADJUSTMENT	11/08/13
A	PD AMENDMENT	04/06/15
A	PD AMENDMENT	09/11/15
A	PD AMENDMENT RESUBMITTAL	01/29/16

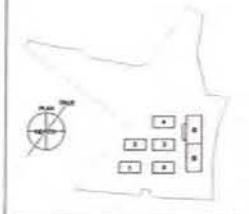


**AMERICA CENTER**  
AMERICA CENTER DRIVE  
SAN JOSE, CA

## OVERALL SITE PLAN

PROJECT 2015-001	SHEET NUMBER
SCALE 1:60	<b>3</b>
DRAWN BY	
DATE 09/11/15	



[illegible]

AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

### ACCESSIBLE PATH OF TRAVEL PLAN

PROJECT NO. 00-0000

2015-001  
SCALE

1:50

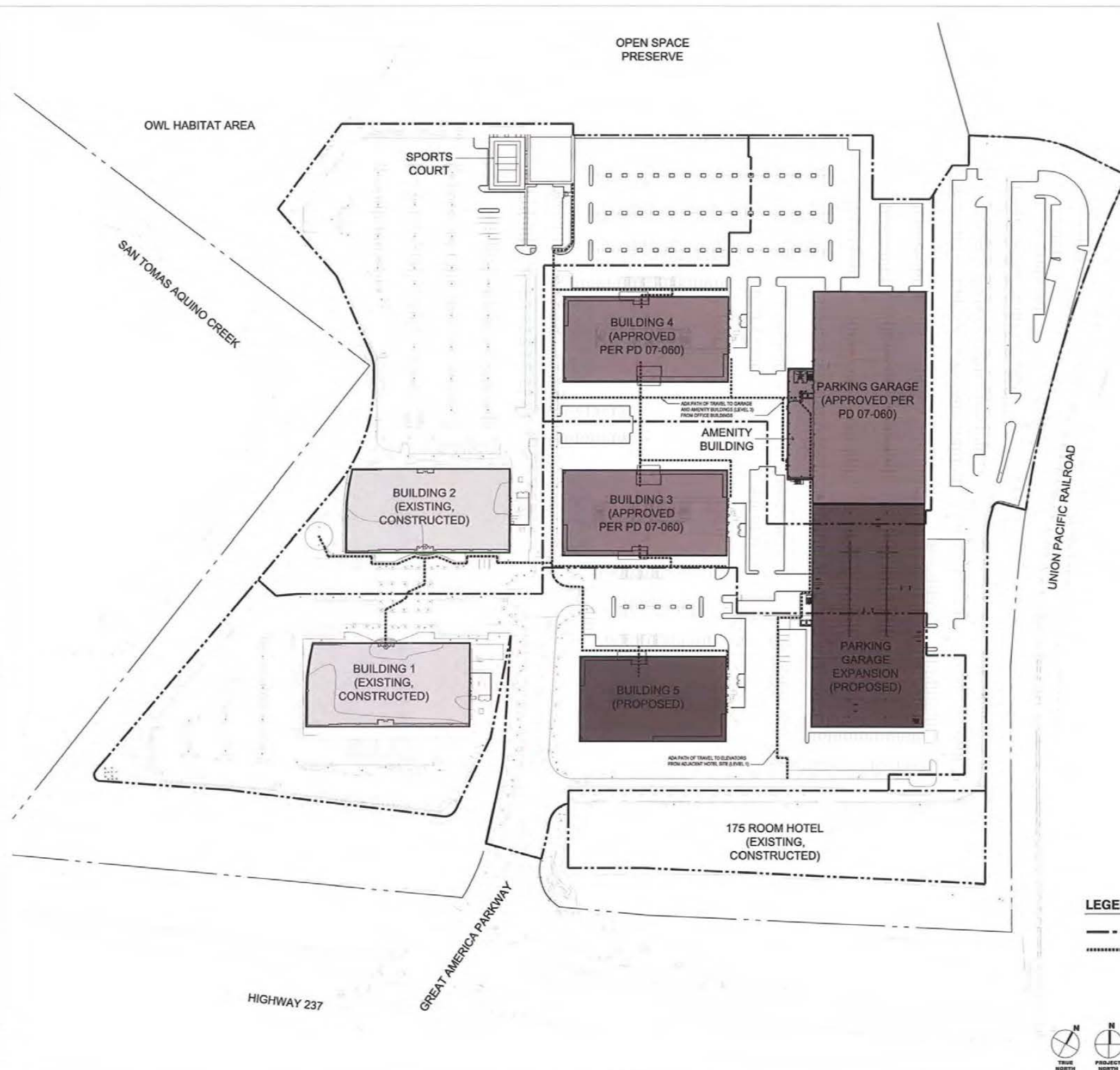
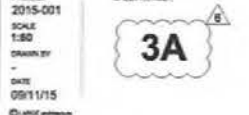
CRACKS IV

DATE \_\_\_\_\_  
CITY & STATE \_\_\_\_\_

09/11/15  
Client: [redacted]

SHEET NUMBER

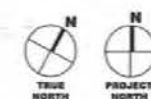
**3A**



### LEGEND

===== PARCEL LINE /  
ASSUMED PROPERTY LINE BETWEEN BUILDINGS

##### ADA PATH OF TRAVEL



## 01 ACCESSIBLE PATH OF TRAVEL PLAN





NO.	ISSUE	DATE
1	PO ADJUSTMENT	10/26/12
2	CITY RESPONSE	10/26/12
3	PO ADJUSTMENT	02/13/13
4	PO ADJUSTMENT	11/08/13
5	PO AMENDMENT	04/08/15
6	PO AMENDMENT	08/11/15
7	PO AMENDMENT RESUBMITTAL	01/29/16

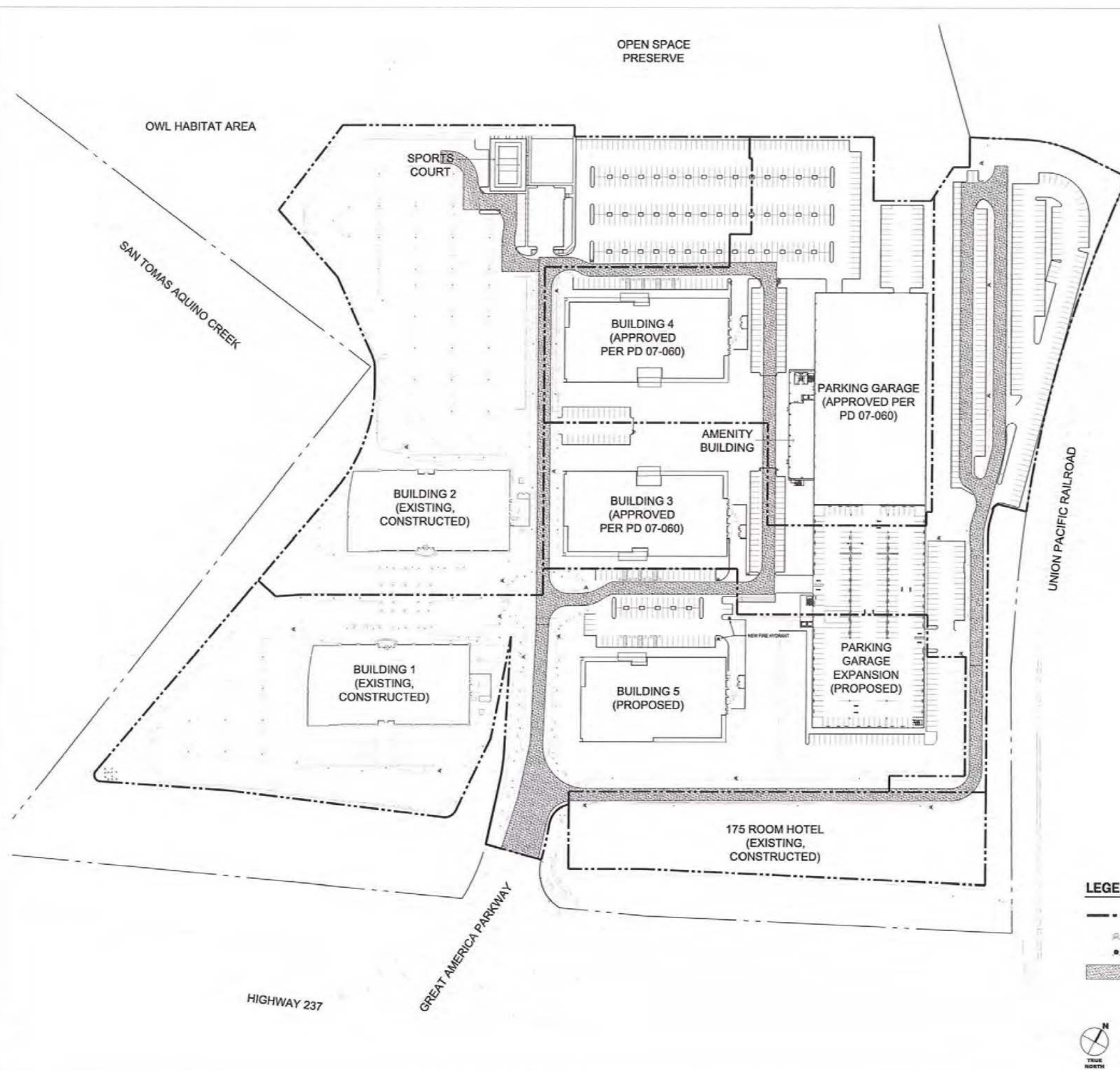


**AMERICA CENTER**  
AMERICA CENTER DRIVE  
SAN JOSE, CA

## FIRE ACCESS SITE PLAN

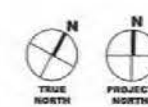
PROJECT  
2015-001  
SCALE  
1"=50'  
DRAWN BY  
DATE  
08/11/15

SHEET NUMBER  
**3B**



### LEGEND

- PARCEL LINE / ASSUMED PROPERTY LINE BETWEEN BUILDINGS
- EXISTING FIRE HYDRANT
- NEW FIRE HYDRANT
- FIRE TRUCK ACCESS



**01 FIRE ACCESS PLAN**  
SCALE 1" = 50'

STEELWAVE

4500 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650 571 2300

## CONSULTANTS

ARCHITE

**LARGE architecture**  
1551 SOUTH ROBERTSON BOULEVARD, 2ND FLOOR  
LOS ANGELES, CA 90035  
TEL: 310 553 1959

CIVIL ENGINEERING

**BKF ENGINEERS**

150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.398.7900

LANDSCAPE ARCHITECT

**CARDUCCI AND ASSOCIATE**

555 BEACH STREET, 4TH FLO.  
SAN FRANCISCO, CA 94133

TEL: 415 674 0900

[illegible]
**STEELWAVE**

AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

## FINAL GRADING PLAN

PROJECT

2015-001

SCALE

 $1^{\circ}=80'$ 

DRAWN BY  
HALL

DATE \_\_\_\_\_

04/06/15

 LARGE analysis

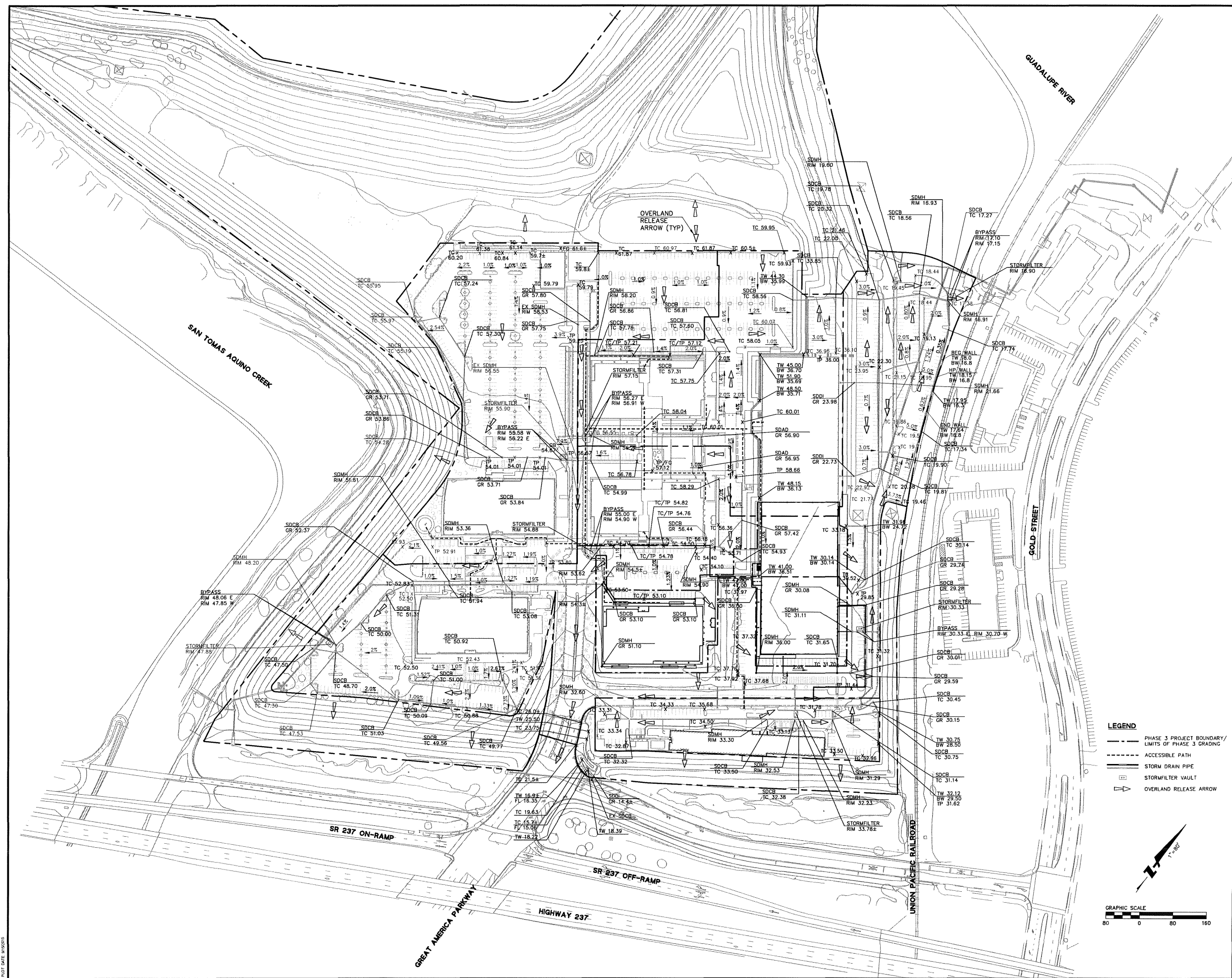
SHEET NUMBER

100% 90% 80% 70% 60% 50% 40% 30% 20% 10% 0%

401

## 4.01

## Notes





STEELWAY

4000 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650 671 2200

CONSULTANTS

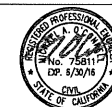
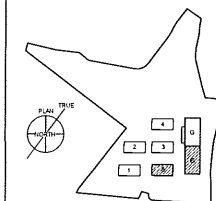
**ARCHITECT**  
LARGE architecture  
1551 SOUTH ROBERTSON BOULEVARD, 2ND FLOOR  
LOS ANGELES, CA 90035  
TEL. 310 553 1959

CIVIL ENGINEERING

**BXF ENGINEERS**  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415 930 7900

LANDSCAPE ARCHITECT

**CARDUCCI AND ASSOCIATES**  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415 674 0900

[illegible]

AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

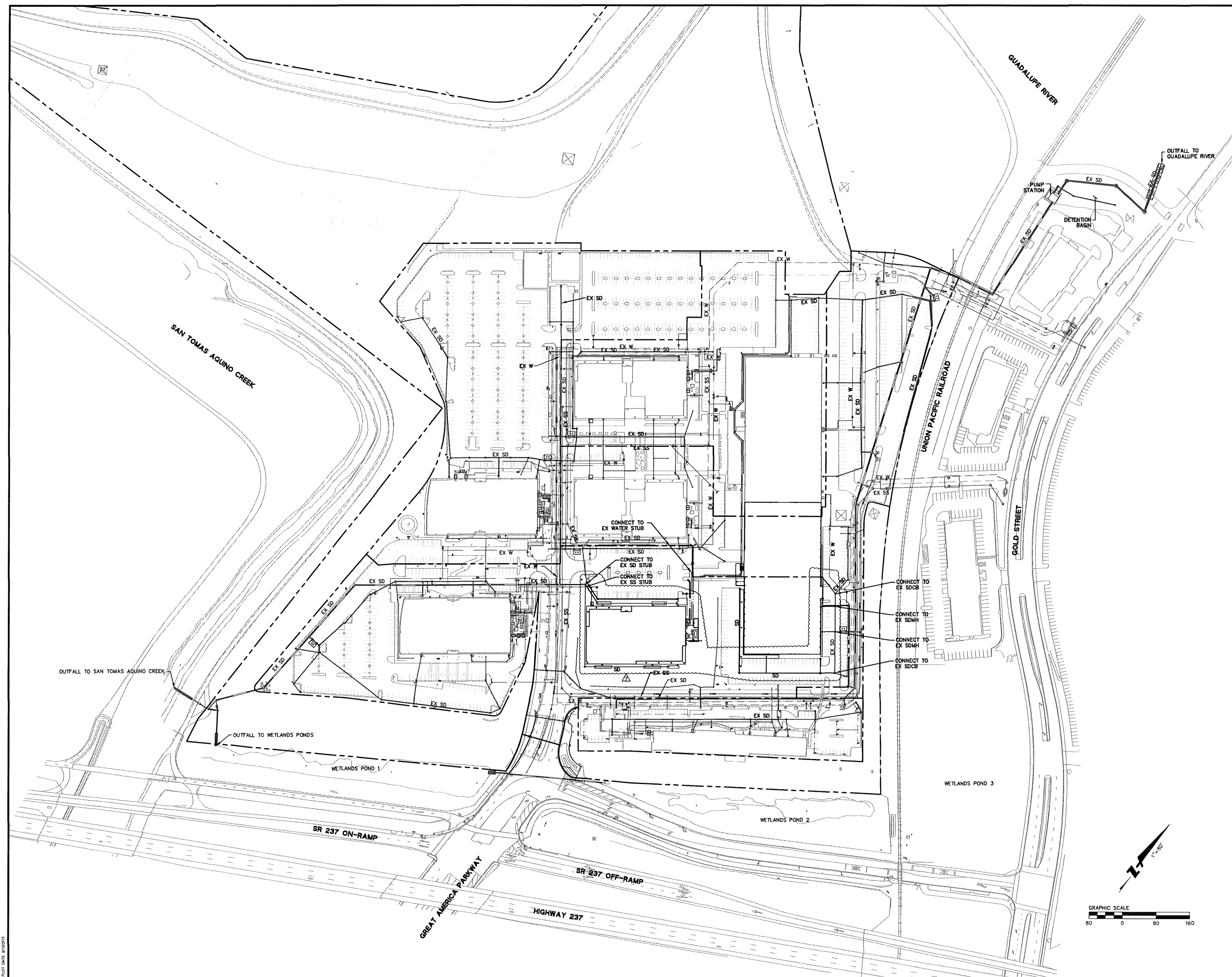
## UTILITY PLAN

PROJECT  
2015-001  
SCALE  
1"=80'  
DRAWN BY  
HALJ  
DATE  
04/06/15

**SWEET NUMBER**

## 4.02

© LANGE publishing



## OWNER

STEELWAVE  
400 EAST THIRD AVENUE, SUITE 200  
PACIFIC CITY, CALIFORNIA  
TEL: 949.571.2200

## CONSULTANTS

### ARCHITECT

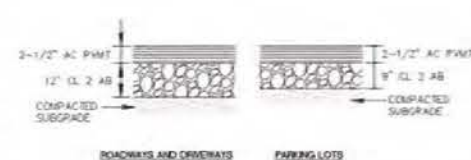
LARGE ARCHITECTS  
400 EAST THIRD AVENUE, SUITE 200  
PACIFIC CITY, CALIFORNIA  
TEL: 949.571.2200

### CIVIL ENGINEERING

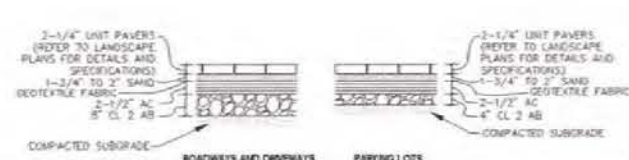
BOY ENGINEERS  
100 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.398.7800

### LANDSCAPE ARCHITECT

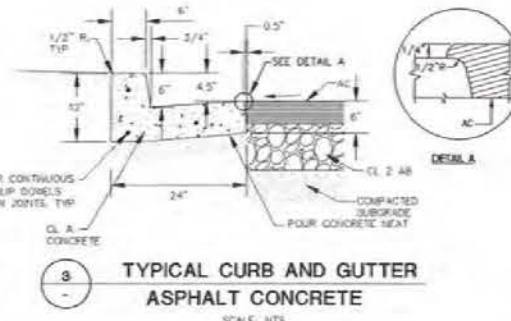
LANDSCAPE ARCHITECTS  
100 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.398.7800



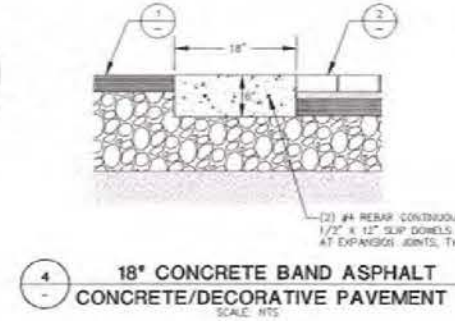
1 ASPHALT PAVEMENT SECTIONS  
SCALE: NTS



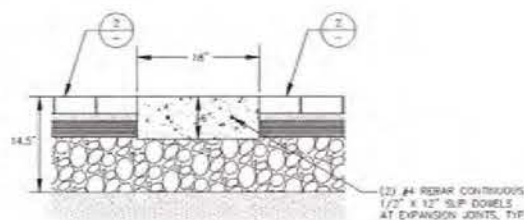
2 DECORATIVE PAVEMENT SECTIONS  
SCALE: NTS



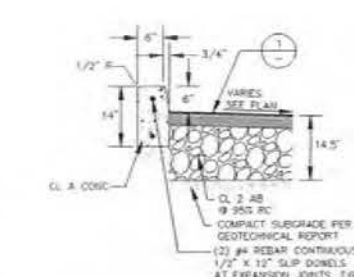
3 TYPICAL CURB AND GUTTER  
ASPHALT CONCRETE  
SCALE: NTS



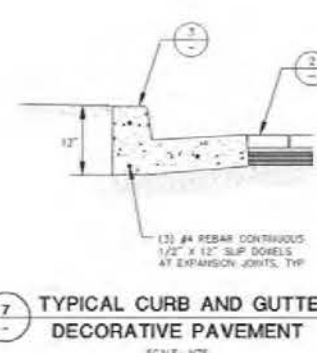
4 18" CONCRETE BAND ASPHALT  
CONCRETE/DECORATIVE PAVEMENT  
SCALE: NTS



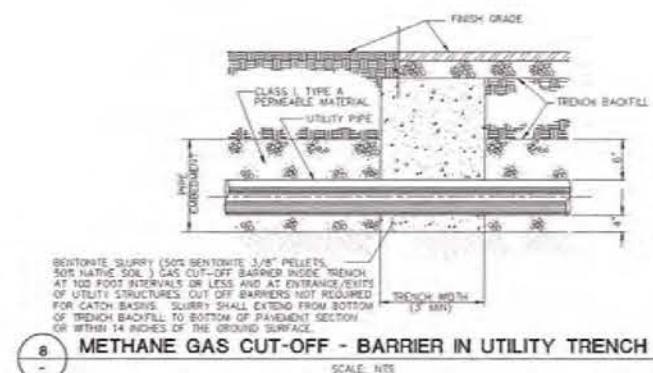
5 18" CONCRETE BAND  
DECORATIVE PAVEMENT  
SCALE: NTS



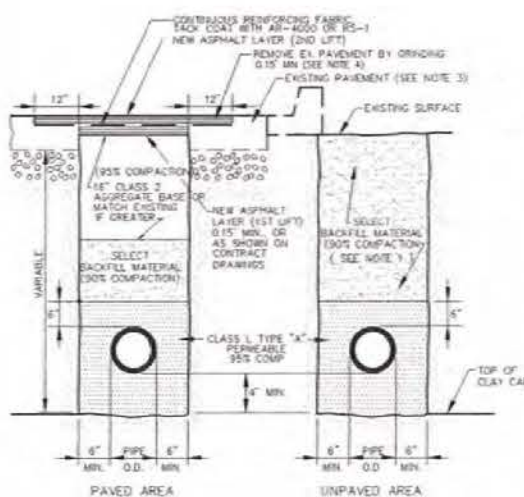
6 6" VERTICAL CURB  
SCALE: NTS



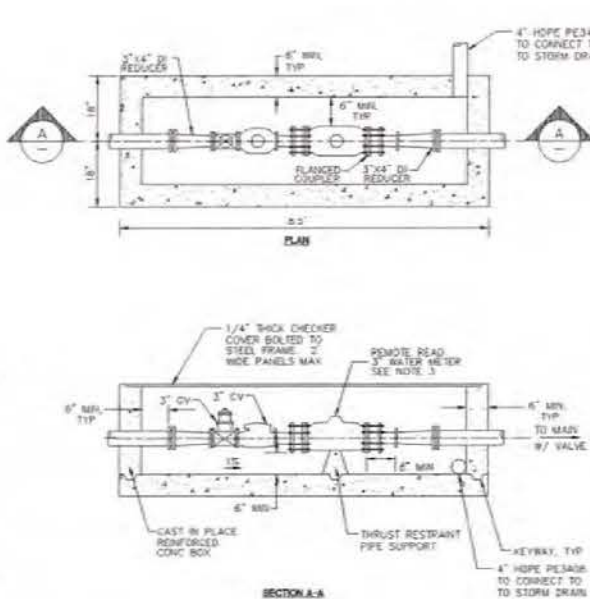
7 TYPICAL CURB AND GUTTER  
DECORATIVE PAVEMENT  
SCALE: NTS



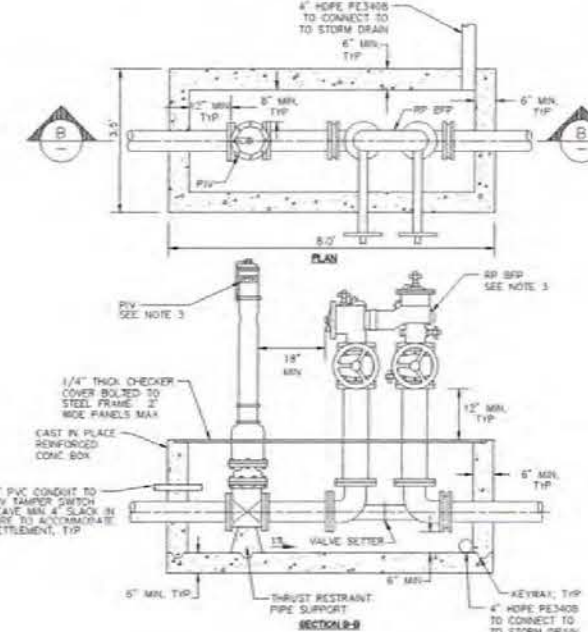
8 METHANE GAS CUT-OFF - BARRIER IN UTILITY TRENCH  
SCALE: NTS



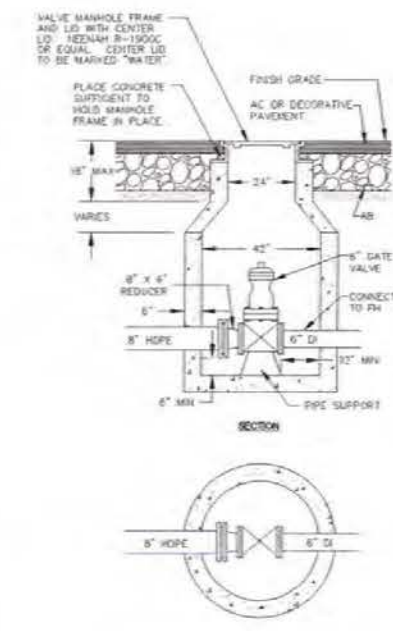
9 UTILITY TRENCH DETAIL  
SCALE: NTS



10A DOMESTIC WATER SERVICE VAULT  
SCALE: NTS



10B FIRE WATER SERVICE VAULT  
SCALE: NTS



11 6" GATE VALVE AND  
LEAK DETECTION MANHOLE  
SCALE: NTS



NO.	REVISION	DATE
1	FOR SUBMITTAL	10/20/10
2	FOR SUBMITTAL	10/20/10
3	FOR SUBMITTAL	10/20/10
4	FOR SUBMITTAL	10/20/10
5	FOR SUBMITTAL	10/20/10
6	FOR SUBMITTAL	10/20/10
7	FOR SUBMITTAL	10/20/10
8	FOR SUBMITTAL	10/20/10
9	FOR SUBMITTAL	10/20/10
10	FOR SUBMITTAL	10/20/10
11	FOR SUBMITTAL	10/20/10
12	FOR SUBMITTAL	10/20/10
13	FOR SUBMITTAL	10/20/10
14	FOR SUBMITTAL	10/20/10
15	FOR SUBMITTAL	10/20/10
16	FOR SUBMITTAL	10/20/10
17	FOR SUBMITTAL	10/20/10
18	FOR SUBMITTAL	10/20/10
19	FOR SUBMITTAL	10/20/10
20	FOR SUBMITTAL	10/20/10

STEELWAVE

AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

## CIVIL DETAILS

PROJECT

2015-001

SCALE

AS SHOWN

DRAWN BY

HALJ

DATE

04/06/15

CHECKED BY

HALJ

4.03



OWNER

STEELWAVE  
8000 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94424  
TEL: 650 571 1200

CONSULTANTS

ARCHITECT

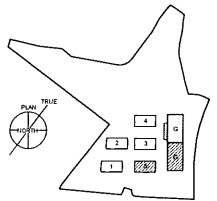
LARGE architecture  
1001 SOUTH ROBERTSON BOULEVARD, 2ND FLOOR  
LOS ANGELES, CA 90025  
TEL: 310 551 1959

CIVIL ENGINEERING

BMF ENGINEERS  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415 920 7300

LANDSCAPE ARCHITECT

CARRICO AND ASSOCIATES  
545 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415 574 6900



NO.	ISSUE	DATE
1	PD ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/24/12
3	PD ADJUSTMENT	02/13/13
4	PD ADJUSTMENT	11/08/13
5	PD AMENDMENT	04/06/15
6	PD AMENDMENT	09/11/15
7	PD AMENDMENT RESUBMITTAL	01/28/16
8	PD AMENDMENT RESUBMITTAL	04/06/16

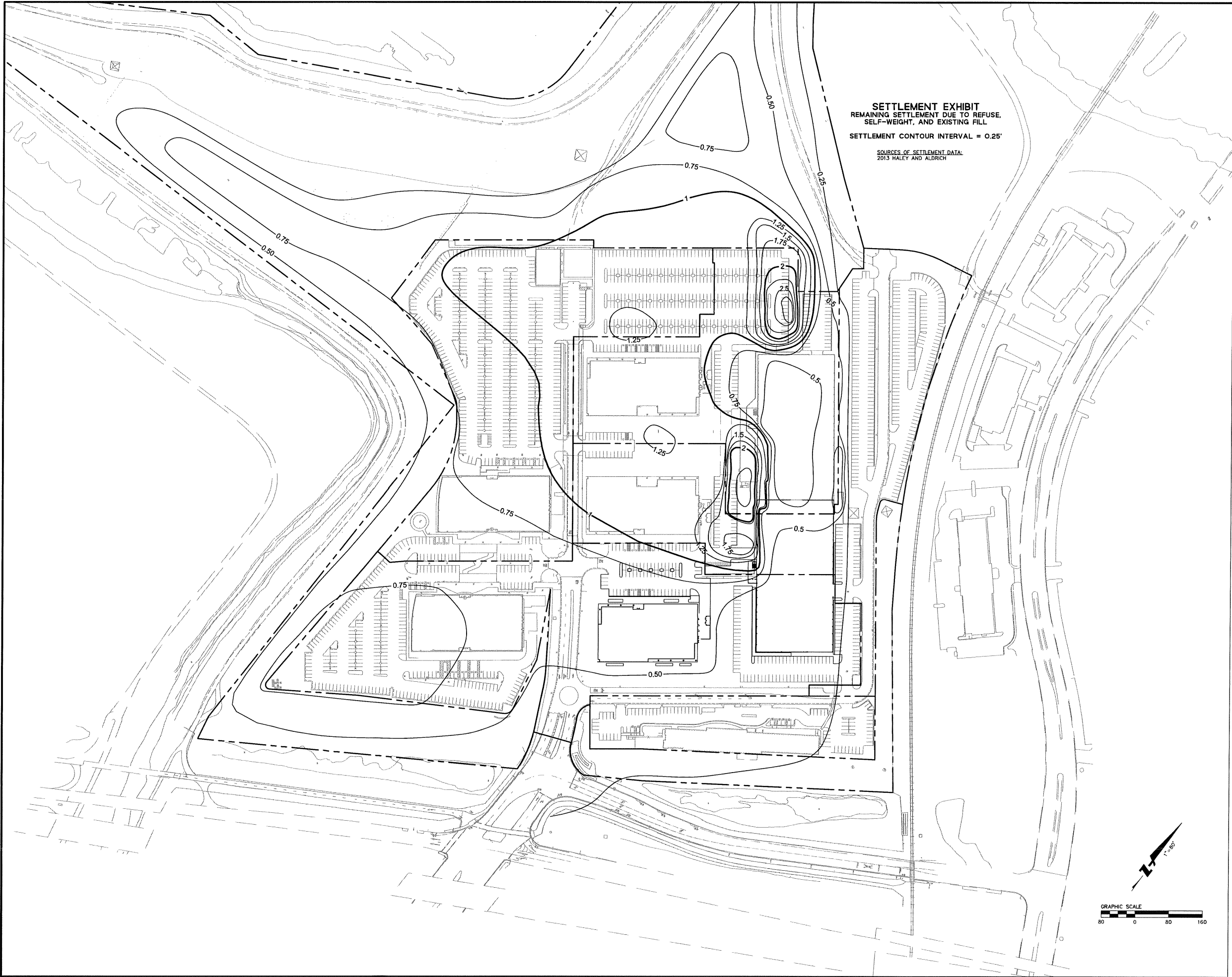


AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

SETTLEMENT EXHIBIT

PROJECT: 2015-001  
SCALE: 1"=80'  
DRAWN BY: HALJ  
DATE: 04/06/15

SHEET NUMBER  
**4.04**



SETTLEMENT EXHIBIT  
REMAINING SETTLEMENT DUE TO REFUSE,  
SELF-WEIGHT, AND EXISTING FILL  
SETTLEMENT CONTOUR INTERVAL = 0.25'  
SOURCES OF SETTLEMENT DATA:  
2013 HALEY AND ALDRICH

STEELWAVE

4000 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650.571.2200

## CONSULTANTS

ARCHITECT

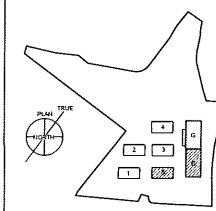
**LARGE architecture**  
1551 SOUTH ROBERTSON BOULEVARD, 2ND FLOOR  
LOS ANGELES, CA 90035  
TEL: 310 553 1959

CIVIL ENGINEERING

**BKF ENGINEERS**  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94104  
TEL: 415 920 7900

LANDSCAPE ARCHITECT

**CARDUCCI AND ASSOCIATES**  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415 674 0900

[illegible]

**AMERICA CENTER**  
AMERICA CENTER DRIVE  
SAN JOSE, CA

**STORMWATER PLAN**

PROJECT  
2015-001  
SCALE  
1"=80'  
DRAWN BY  
HALJ  
DATE  
04/06/15

SHEET NUMBER

## 5.01

① LARGÉ architecture

## OWNER

STEELWAVE  
4000 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650 511 2200

## CONSULTANTS

## ARCHITECT

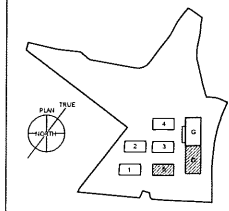
LARGE architecture  
1551 SOUTH ROBERTSON BOULEVARD, 210 FLOOR  
LOS ANGELES, CA 90035  
TEL: 310 553 1999

## CIVIL ENGINEERING

BEIF ENGINEERS  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415 392 7300

## LANDSCAPE ARCHITECT

CARDUCHI AND ASSOCIATES  
505 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415 674 0900



NO.	ISSUE	DATE
1	PD ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/24/12
3	PD ADJUSTMENT	02/13/13
4	PD ADJUSTMENT	11/08/13
5	PD AMENDMENT	04/06/15
6	PD AMENDMENT	09/11/15
7	PD AMENDMENT RESUBMITTAL	07/23/16
8	PD AMENDMENT RESUBMITTAL	04/08/16
9	PD AMENDMENT RESUBMITTAL	04/18/17

STEELWAVE

AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

STORMWATER  
CONTROL PLAN

PROJECT

2015-001

SCALE

1"=80'

DRAWN BY

HALJ

DATE

04/18/17

© LARGE ARCHITECTURE

SHEET NUMBER

5.02

TREATMENT MEASURES	
DMA	TCM
AREA 1	STORMFILTER 1
AREA 2	STORMFILTER 2
AREA 3	STORMFILTER 3
AREA 4	STORMFILTER 4
AREA 5	STORMFILTER 5
AREA 6a	STORMFILTER 6
AREA 6b	STORMFILTER 6
AREA 7	STORMFILTER 7
B1	BIORETENTION AREA B1
B2	BIORETENTION AREA B2
B3	BIORETENTION AREA B3
B4	BIORETENTION AREA B4
B5	BIORETENTION AREA B5
B6	BIORETENTION AREA B6
B7	BIORETENTION AREA B7
B8	BIORETENTION AREA B8
B9	BIORETENTION AREA B9
STREET	FILTERRA LAWS C-2

Table 1 - StormFilter Sizing Chart								
StormFilter	1	2	3	4	5	6	7	Total/Avg
Total Area (SF)	195,541	209,453	249,696	73,504	71,841	214,510	71,040	1,085,575
Total Area (acre)	4.49	4.81	5.73	1.69	1.65	4.92	1.63	24.92
Pervious Area (SF)	37,128	31,649	62,149	6,422	27,297	47,616	28,593	238,564
Pervious Area (acre)	0.85	0.73	1.43	0.15	0.63	1.09	0.61	5.49
Impervious Area (SF)	158,415	177,804	187,537	67,082	44,544	166,892	44,347	846,821
Impervious Area (acre)	3.64	4.08	4.31	1.54	1.02	3.83	1.02	19.44
% Impervious	81.01%	84.89%	75.11%	81.26%	62.00%	77.80%	62.43%	77.95%
Weighted C Factor	0.67	0.69	0.63	0.74	0.53	0.64	0.54	0.65
Intensity (in/hr)	0.20	0.20	0.20	0.20	0.20	0.20	0.20	0.20
Flow (cfs)	0.60	0.67	0.72	0.25	0.18	0.63	0.18	2.22
Flow (gpm)	269	300	322	112	79	265	79	1,445
# of Cartridges	18	20	22	8	6	19	6	99
Structure Size	8'x16'	8'x16'	8'x16'	8'x16'	8'x16'	8'x16'	8'x12'	-

Table 2 - Filterra Sizing Chart																
Filtterra	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	Total/Avg
Total Area (SF)	6,320	7,735	4,330	7,590	13,766	23,302	6,269	33,125	23,436	5,126	5,016	15,081	8,165	10,475	1,290	171,061
Total Area (acre)	0.15	0.18	0.10	0.17	0.32	0.53	0.14	0.76	0.54	0.12	0.12	0.35	0.19	0.24	0.03	3.93
Pervious Area (SF)	1,325	2,881	166	2,058	3,894	12,302	2,024	18,275	8,108	1,191	956	6,477	2,347	3,028	50	65,922
Pervious Area (acre)	0.03	0.06	0.00	0.05	0.09	0.28	0.05	0.42	0.21	0.03	0.02	0.15	0.05	0.07	0.00	1.51
Impervious Area (SF)	4,995	5,054	4,224	5,537	9,772	10,999	4,275	14,850	14,328	3,935	4,060	8,604	5,819	7,448	1,239	105,139
Impervious Area (acre)	0.11	0.12	0.10	0.13	0.22	0.25	0.10	0.34	0.33	0.09	0.09	0.20	0.13	0.17	0.03	2.41
% Impervious	79.03%	65.34%	97.55%	72.80%	70.99%	47.20%	67.97%	44.83%	61.14%	76.77%	80.95%	57.05%	71.26%	68.10%	61.46%	77.95%
Weighted C Factor	0.65	0.56	0.78	0.61	0.60	0.43	0.58	0.41	0.53	0.64	0.67	0.50	0.60	0.60	0.77	0.53
Intensity (in/hr)	0.20	0.20	0.20	0.20	0.20	0.20	0.20	0.20	0.20	0.20	0.20	0.20	0.20	0.20	0.20	0.20
Flow (cfs)	0.02	0.02	0.02	0.02	0.04	0.05	0.02	0.06	0.09	0.02	0.02	0.03	0.02	0.03	0.00	0.42
Flow (gpm)	9	9	7	10	17	21	7	28	25	7	7	16	10	13	2	187
Structure Size	4'x6.5'	4'x6.5'	4'x6.5'	4'x6.5'	4'x6.5'	4'x6.5'	6'x10'	6'x6'	4'x6.5'	4'x6.5'	6'x8'	4'x6.5'	4'x6.5'	4'x6.5'	4'x6.5'	-

Table 3 - Biotreatment Planter Sizing Chart										
Biotreatment Planter	B1	B2	B3	B4	B5	B6	B7	B8	B9	Total/Avg
Total Area (SF)	6,456	6,456	9,217	6,478	6,478	4,320	5,1293	9,810	7,524	108,022
Pervious Area (SF)	368	368	793	360	360	261	2,341	501	376	5,788
Impervious Area (SF)	6,088	6,088	8,424	6,088	6,088	4,059	48,642	9,309	7,148	102,234
Equal Imp. Area (SF)	6,125	6,125	8,503	6,127	6,127	4,085	49,176	9,359	7,186	102,813
4% Method	0.04	0.04	0.04	0.04	0.04	0.04	0.04	0.04	0.04	0.04
Required TreatArea (SF)	245	245	340	245	245	163	1,587	374	287	4,113
Provided Area (SF)	245	245	351	245	245	165	1,580	384	288	4,150
Excess Provided (SF)	0	0	11	1	1	2	13	10	1	37

NOTE:  
EQUIVALENT IMPERVIOUS AREA = IMPERVIOUS AREA + (PERVIOUS AREA \* RUNOFF FACTOR)  
RUNOFF FACTORS PER SANTA CLARA VALLEY URBAN RUNOFF C.3 HANDBOOK TABLE 5-4

## STORMWATER CONTROL PLAN NOTES

- GROUNDWATER RANGES FROM APPROXIMATELY ELEVATION 6 TO -3.
- THE PROJECT SITE IS ON AN EXISTING CLOSED LANDFILL FOR AREAS WITH SLOPES LESS THAN 3:1, THERE IS APPROXIMATELY 5 FEET OF ENGINEERED FILL ABOVE THE CLAY CAP. FOR AREAS WITH SLOPES STEEPER THAN 3:1, THE COVER IS REDUCED TO 2 FEET ABOVE THE CLAY CAP.
- THE PROPERTY IS SHOWN ON FEMA FLOOD INSURANCE RATE MAP COMMUNITY-PANEL NUMBER 060349 0007 G, DATED AUGUST 17, 1998 AND IS DESIGNATED AS ZONE A1 (EL 9). THE MINIMUM PROPOSED GRADE ON THE SITE IS AT APPROXIMATELY ELEVATION 16, WELL ABOVE THE FLOODING ELEVATION.
- SEE SHEET 4 FOR PROPOSED GRADES.
- AREA 6 IS COMPRISED OF AREAS 6a AND 6b.
- SOURCE CONTROL MEASURES INCLUDE: BENEFICIAL LANDSCAPING, USE OF WATER EFFICIENT IRRIGATION SYSTEMS, MAINTENANCE, AND STORM DRAIN LABELING.

## LEGEND

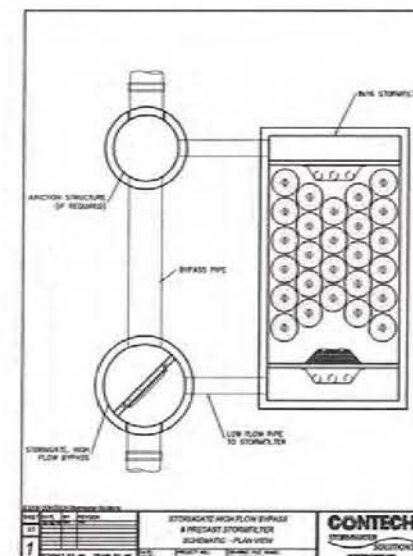
- AREA TREATED BY STORMFILTER (1)
- AREA TREATED BY STORMFILTER (2)
- AREA TREATED BY STORMFILTER (3)
- AREA TREATED BY STORMFILTER (4)
- AREA TREATED BY STORMFILTER (5)
- AREA TREATED BY STORMFILTER (6)
- AREA TREATED BY STORMFILTER (7)
- AREA TREATED BY BIORETENTION PLANTERS
- AREA TREATED BY FILTERRA UNITS

— SURFACE FLOW ARROW

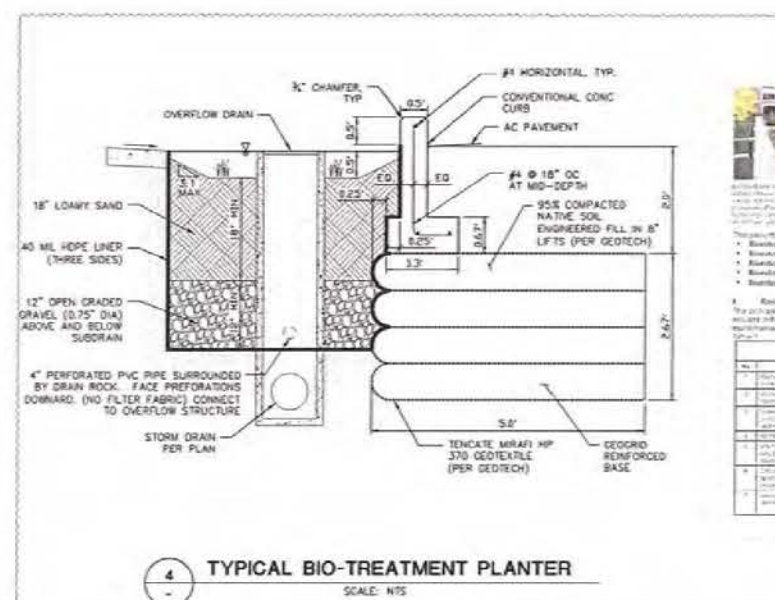
NOTE: SEE TABLE 1-3 FOR BMP SIZING AND DRAINAGE AREA INFORMATION.

PERVIOUS AND IMPERVIOUS SURFACES COMPARISON TABLE			
a. Project Phase Number (N/A, 1, 2, 3, etc.):	3	b. Total Site (acres):	53.00
c. Total Site Existing Impervious Surfaces (square feet):	1,057,898	d. Total Area of Site Disturbed (acres):	2.48 Acres (108,022 SF)
e. Impervious Surfaces	Existing Condition of Site Area Disturbed (square feet)	Replaced	New
Roof Areas	0	0	97,850
Parking	41,653	2,573	0
Sidewalks, Patios, Driveways, etc.	245	245	1,565
Streets (Public)	0	0	0
Streets (Private)	0	0	0
Total Impervious Surfaces:	e.1: 41,898	e.2: 2,819	e.3: 99,415
f. Pervious Surfaces			
Landscaped Areas	66,124	5,788	0
Pervious Paving	0	0	0
Other Pervious Surfaces (green roof, etc.)	0	0	0
Total Pervious Surfaces:	f.1: 66,124	f.2: 5,788	f.3: 0
g. Total Proposed Replaced + New Impervious Surfaces (e.2 + e.3):			102,234
h. Total Proposed Replaced + New pervious Surfaces (f.2 + f.3):			5,788
i. Percent of Replacement of Impervious Area in redevelopment projects (e.2 / e.3 x 100):			0%

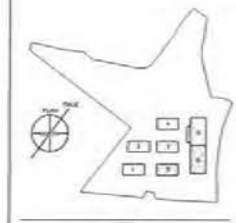
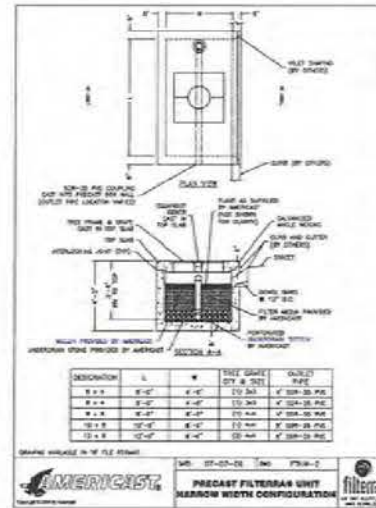




3 STORMFILTER HIGH FLOW BYPASS



4 TYPICAL BIO-TREATMENT PLANTER

[illegible]

6 STORMFILTER MAINTENANCE  
SCALE: NTS

OWNER

STEELWAVE

4300 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650.971.2500

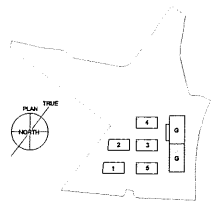
CONSULTANTS

CIVIL ENGINEERING

BAF ENGINEERS  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.930.7900

LANDSCAPE ARCHITECT

CARDUCCI AND ASSOCIATES  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415.874.9900



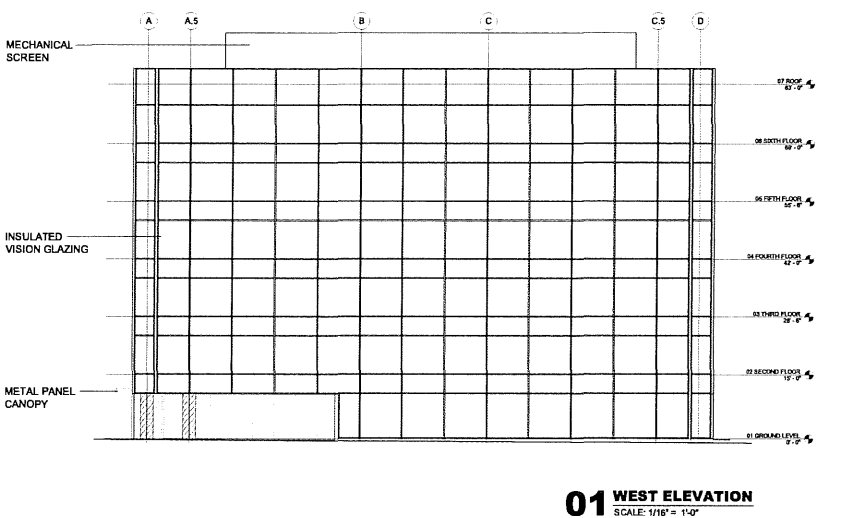
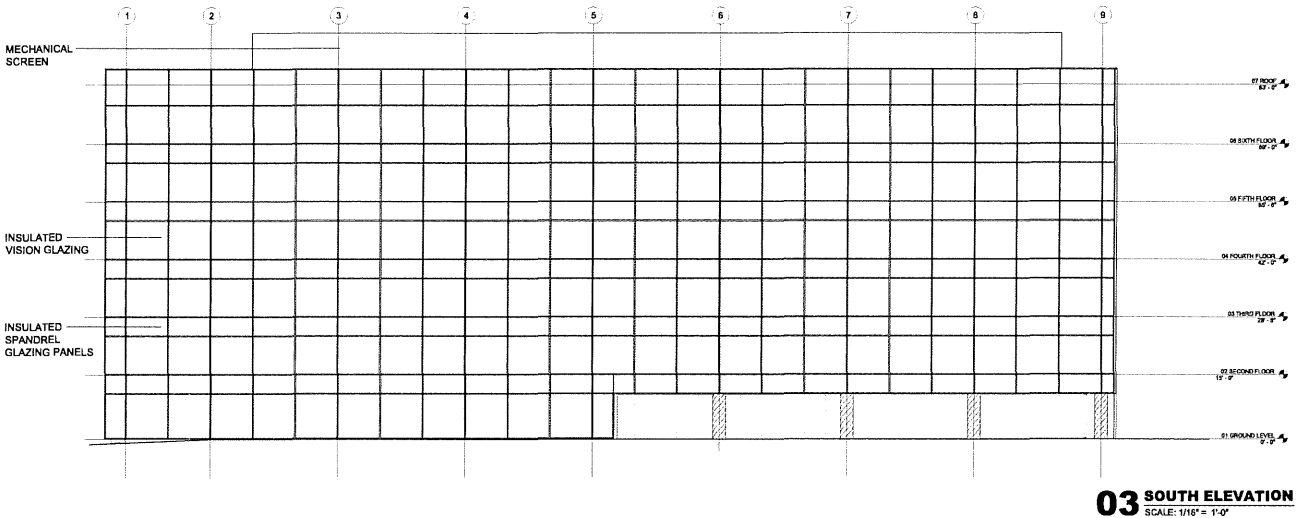
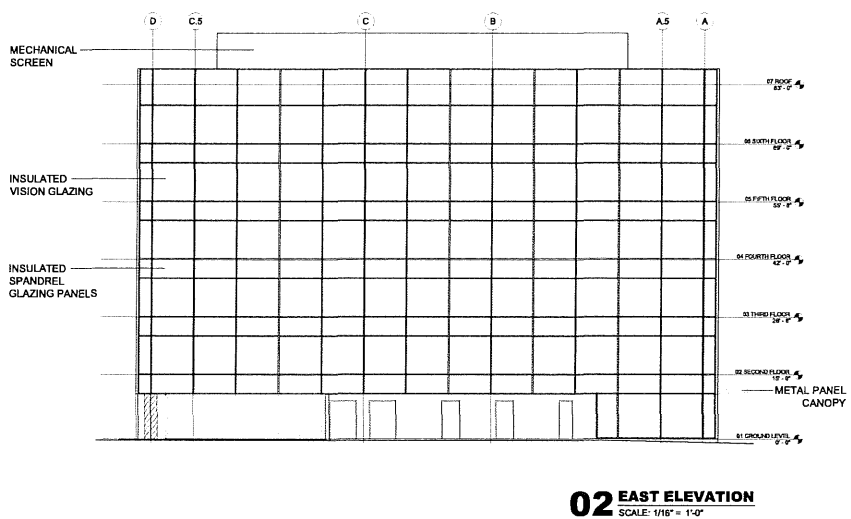
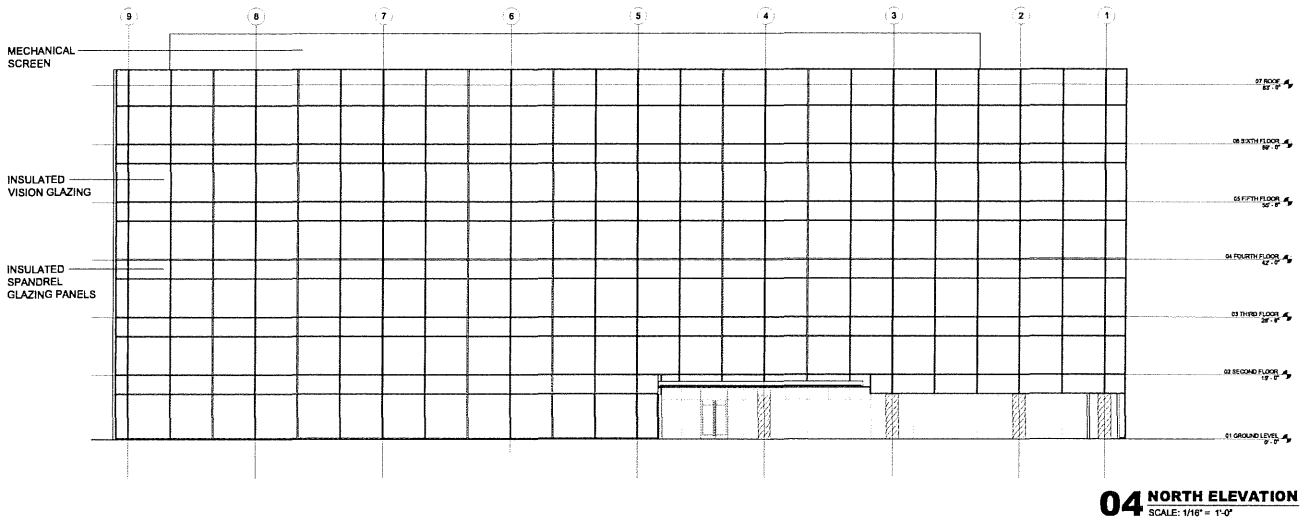
NO.	ISSUE	DATE
	PD ADJUSTMENT	10/01/12
	CITY RESPONSE	10/24/12
	PD ADJUSTMENT	02/13/13
	PD ADJUSTMENT	11/08/13
	PD AMENDMENT	04/06/15
	PD AMENDMENT	09/11/15
	PD AMENDMENT RESUBMITTAL	01/29/16



AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

BUILDING 5  
ELEVATIONS

PROJECT 2015-001	SHEET NUMBER
SCALE 1/16" = 1'-0"	7A
DRAWN BY -	
DATE 09/11/15	





1001 SOUTH ROBERTSON BLVD. 2ND FLOOR  
LOS ANGELES, CALIFORNIA 90025  
T: 213.552.1990  
F: 213.552.1990

## OWNER

STEELWAVE  
400 EAST THIRD AVENUE, SUITE 300  
FOSTER CITY, CA 94404  
TEL: 650.571.2000

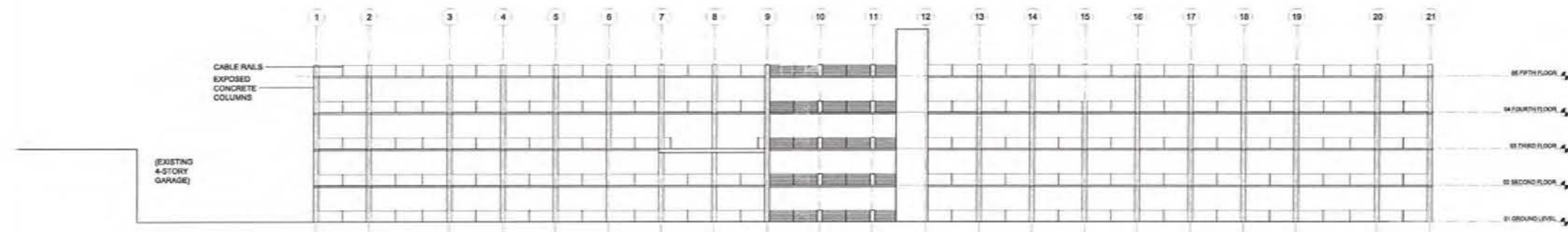
## CONSULTANTS

### CIVIL ENGINEERING

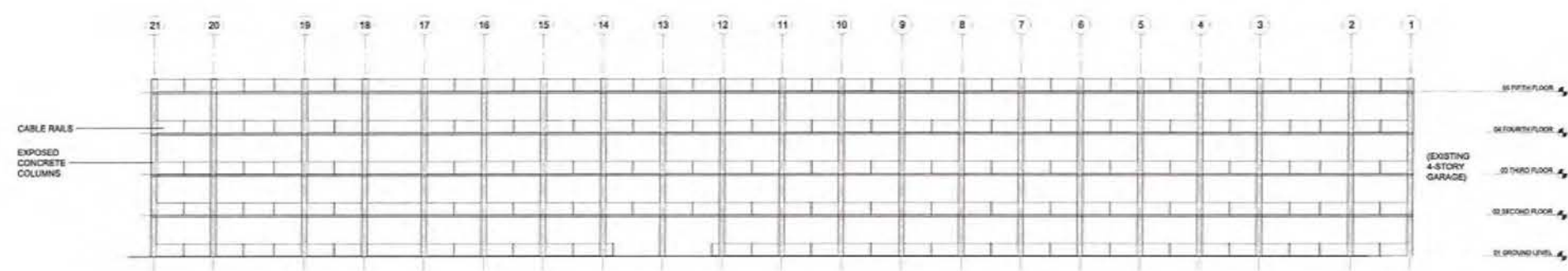
800 ENGINEERS  
100 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.398.7300

### LANDSCAPE ARCHITECT

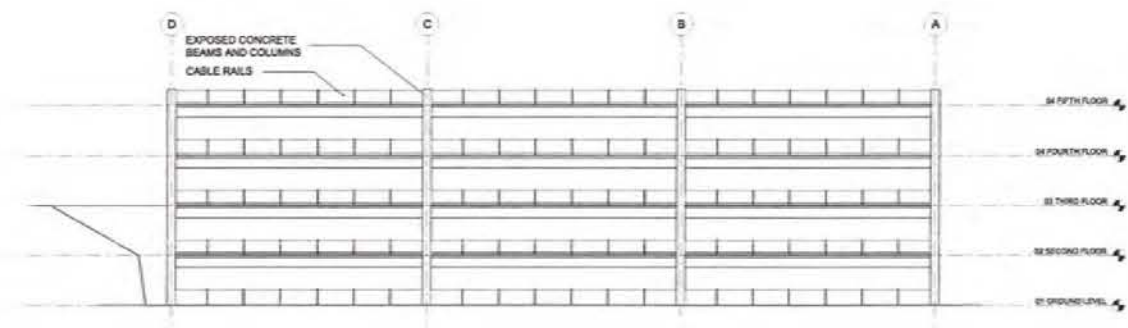
GARDNER AND ASSOCIATES  
200 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94105  
TEL: 415.674.8800



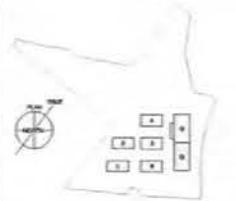
**03 WEST ELEVATION**  
SCALE: 1/8" = 1'-0"



**02 EAST ELEVATION**  
SCALE: 1/8" = 1'-0"



**01 SOUTH ELEVATION**  
SCALE: 1/8" = 1'-0"



NO.	ISSUE	DATE
1.	PO ADJUSTMENT	10/01/12
2.	CITY RESPONSE	10/04/12
3.	PO ADJUSTMENT	10/13/12
4.	PO ADJUSTMENT	11/08/12
5.	PO AMENDMENT	04/09/13
6.	PO AMENDMENT	08/11/13
7.	PO AMENDMENT RESUBMITTAL	01/09/15



**AMERICA CENTER**  
AMERICA CENTER DRIVE  
SAN JOSE, CA

**GARAGE  
EXPANSION  
ELEVATIONS**

PROJECT  
2015-001  
SCALE  
1/8" = 1'-0"  
DRAWN BY  
-  
DATE  
09/11/15  
© LRG ARCHITECTS

SHEET NUMBER  
**7B**

1561 SOUTH ROBERTSON BLVD, 2ND FLOOR  
LOS ANGELES, CALIFORNIA 90035  
T: 310.553.1959  
F: 310.552.1950

**OWNER**

## STEELWAVE

4000 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650.571.2200

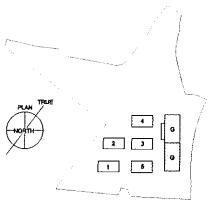
## CONSULTANTS

**CIVIL ENGINEERING**

**BKF ENGINEERS**  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.930.7900

LANDSCAPE ARCHITECT

**CARDUCCI AND ASSOCIATES**  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415.874.0990



NO.	ISSUE	DATE
	PD ADJUSTMENT	10/01/12
A	CITY RESPONSE	10/24/12
A	PD ADJUSTMENT	02/13/13
A	PD ADJUSTMENT	11/08/13
A	PD AMENDMENT	04/06/15
	PD AMENDMENT	09/11/15
B	PD AMENDMENT RESUBMITTAL	01/29/16



**AMERICA CENTER**  
AMERICA CENTER DRIVE  
SAN JOSE, CA

## BUILDING 5 PLANS

PROJECT  
2015-001

SCALE

DRAWN BY

DATE \_\_\_\_\_

09/11/15

09/17/13

**9A**

## FLOOR PLAN AREA SUMMARY - BUILDING 5

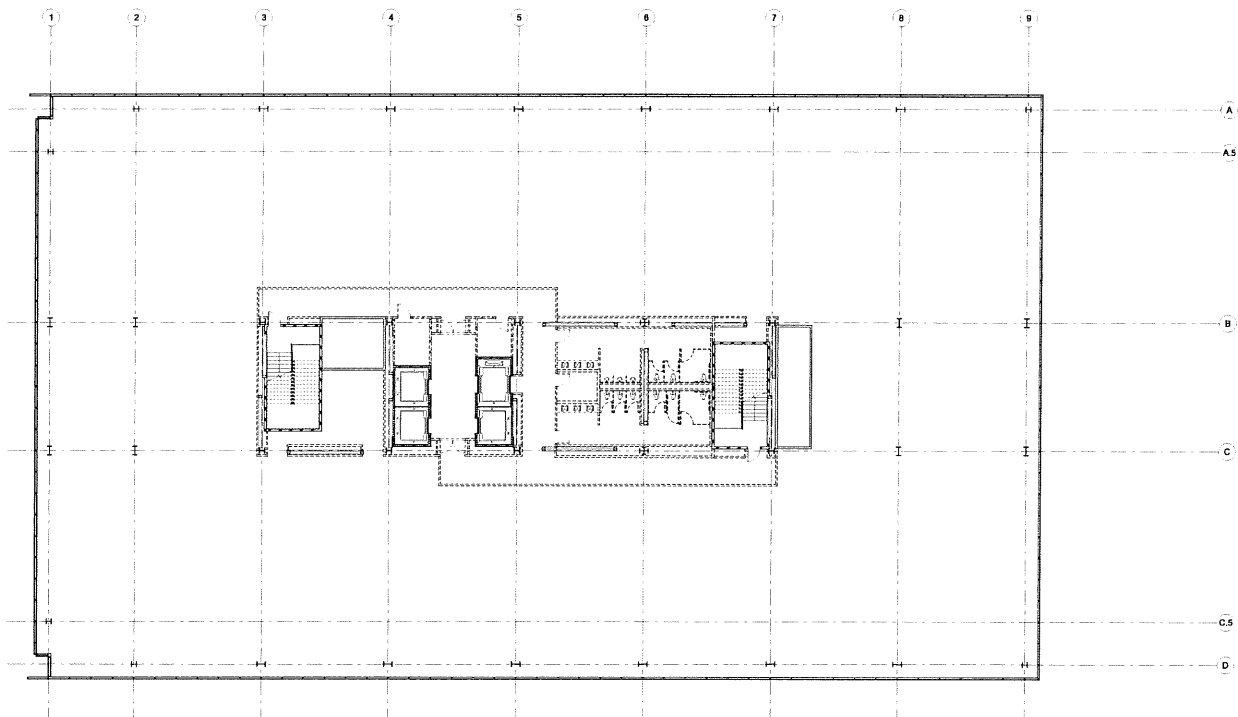
**BUILDING AREA SUMMARY:**

GROUND FLOOR:	30,185 (GROSS SF)	25,657 SF (NET SF)
TYPICAL FLOOR (2-6):	32,433 (GROSS SF)	27,568 SF (NET SF)
TOTAL AREA:	192,350 (GROSS SF)	163,498 SF (NET SF)

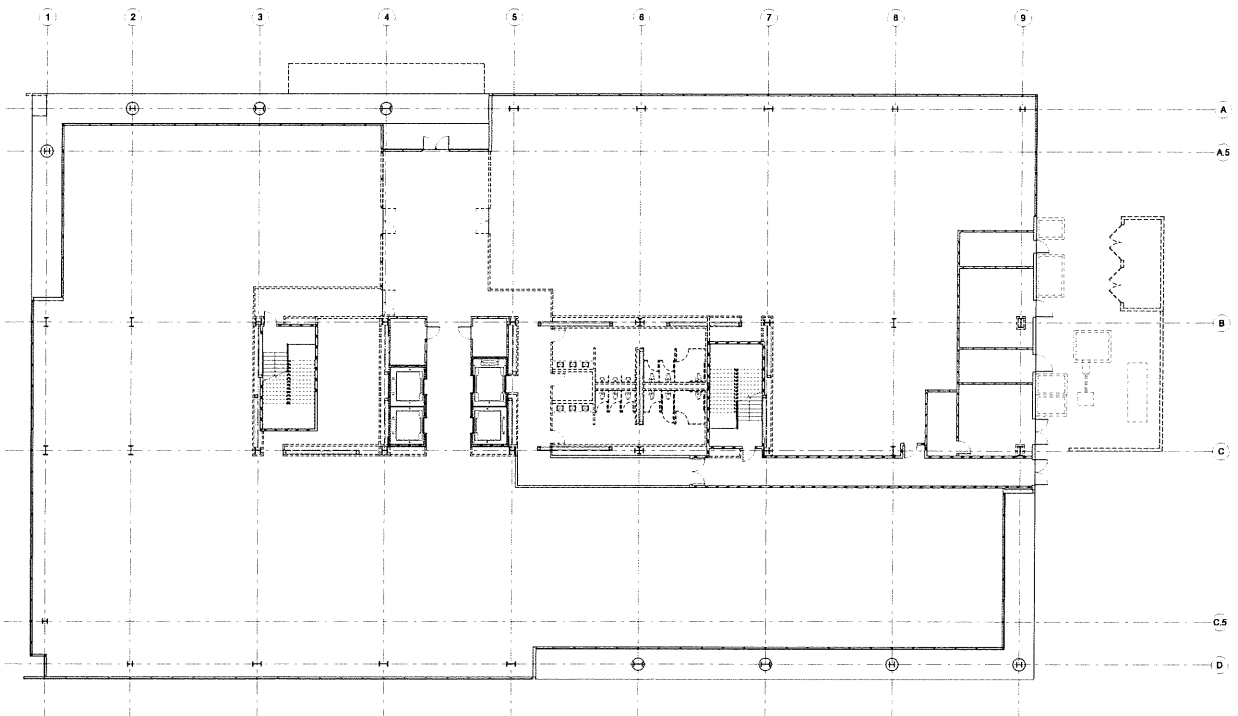
**NOTE**

**NOTE**

TYPE OF CONSTRUCTION FOR BUILDING 5 : TYPE II-A



## 02 TYPICAL FLOOR PLAN



## 01 GROUND FLOOR PLAN

SCALE: 1/16" = 1'-0"

## OWNER

STEELWAVE  
4000 EAST THIRD AVENUE, SUITE 500  
POMONA, CA 91764  
TEL: 951.271.2200

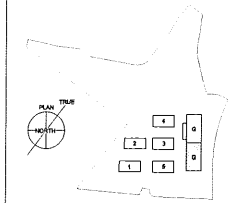
## CONSULTANTS

### CIVIL ENGINEERING

BRP ENGINEERS  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.826.7800

### LANDSCAPE ARCHITECT

CARRUCCI AND ASSOCIATES  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415.874.9990



NO.	ISSUE	DATE
1	PD ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/24/12
3	PD ADJUSTMENT	02/13/13
4	PD ADJUSTMENT	11/08/13
5	PD AMENDMENT	04/06/15
6	PD AMENDMENT	09/11/15
7	PD AMENDMENT RESUBMITTAL	01/28/16

STEELWAVE

AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

GARAGE  
EXPANSION PLANS

PROJECT  
2015-001  
SCALE  
3/64" = 1'-0"  
DRAWN BY  
-  
DATE  
09/11/15  
SHEET NUMBER  
9B

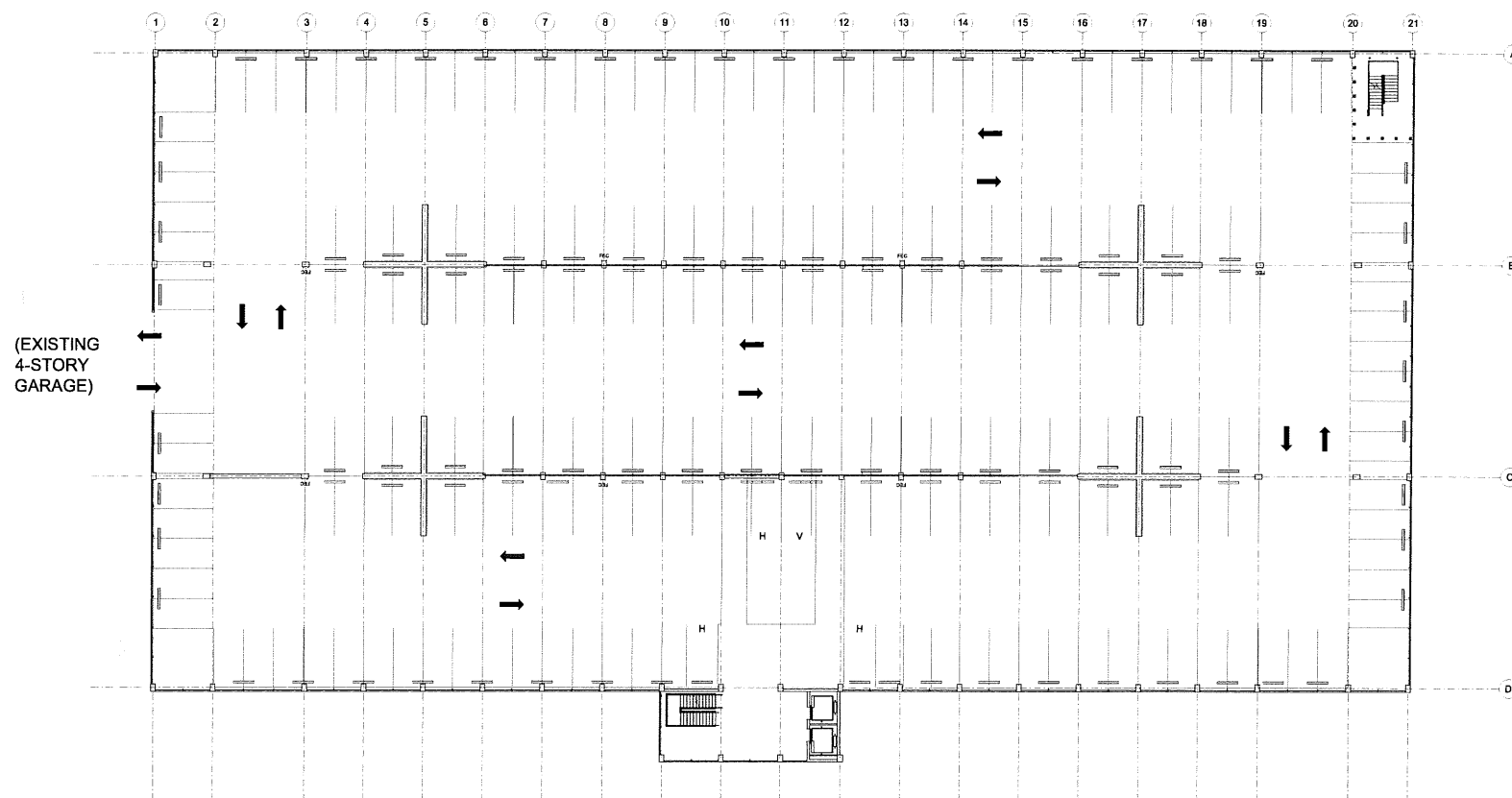
## FLOOR PLAN AREA SUMMARY - GARAGE

### BUILDING AREA SUMMARY:

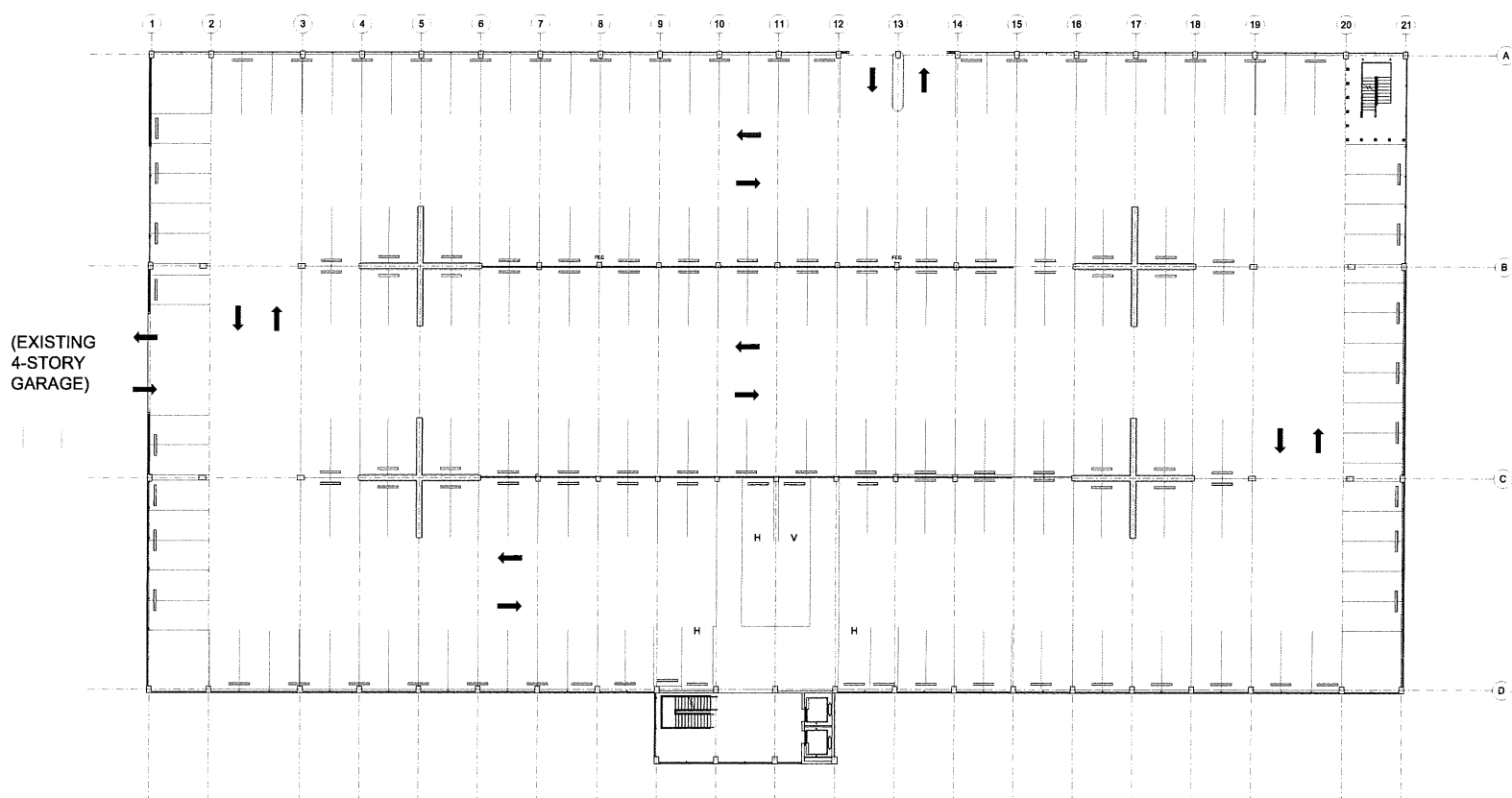
GROUND FLOOR:	66,430 (GROSS SF)	56,466 SF (NET SF)
TYPICAL FLOOR (2-5):	66,430 (GROSS SF)	56,466 SF (NET SF)
TOTAL AREA:	332,150 (GROSS SF)	282,328 SF (NET SF)

### NOTE:

TYPE OF CONSTRUCTION FOR GARAGE : TYPE I-A



**02 SECOND FLOOR PLAN**  
SCALE: 3/64" = 1'-0"



**01 GROUND FLOOR PLAN**  
SCALE: 3/64" = 1'-0"



1501 SOUTH ROBERTSON BLVD. 2ND FLOOR  
LOS ANGELES, CALIFORNIA 90035  
T: 310.552.1850  
F: 310.552.1850

## OWNER

STEELWAVE  
4500 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650.571.7200

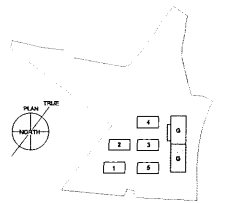
## CONSULTANTS

### CIVIL ENGINEERING

BAF ENGINEERS  
100 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.530.7900

### LANDSCAPE ARCHITECT

CARBONICO AND ASSOCIATES  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415.674.0990



NO.	ISSUE	DATE
1	PD ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/24/12
3	PD ADJUSTMENT	02/13/13
4	PD ADJUSTMENT	11/08/13
5	PD AMENDMENT	04/06/15
6	PD AMENDMENT	09/11/15
7	PD AMENDMENT RESUBMITTAL	01/28/16

STEELWAVE

AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

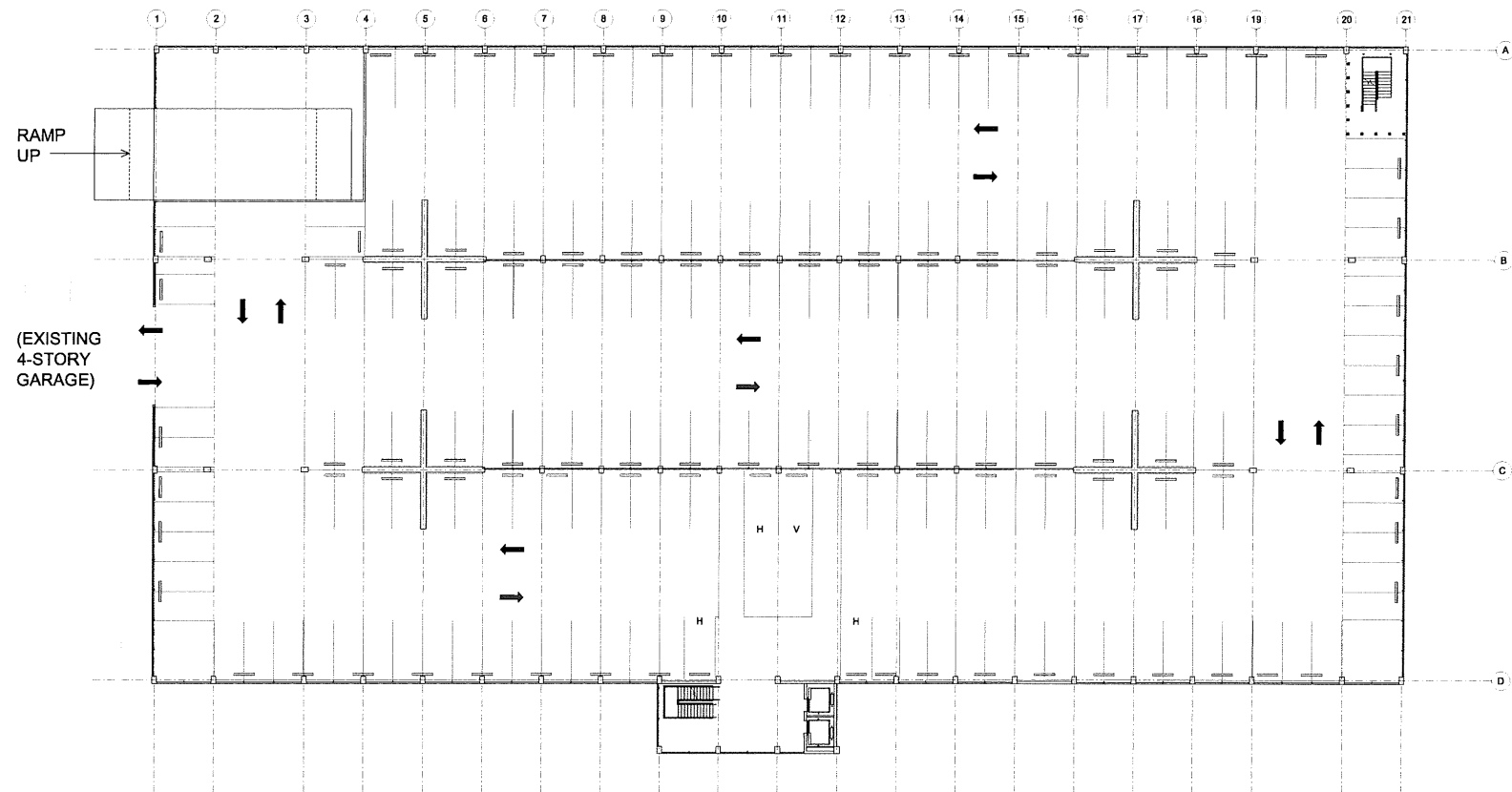
GARAGE  
EXPANSION PLANS

PROJECT  
2015-001  
SCALE  
3/8" = 1'-0"  
DRAWN BY  
-  
DATE  
09/11/15

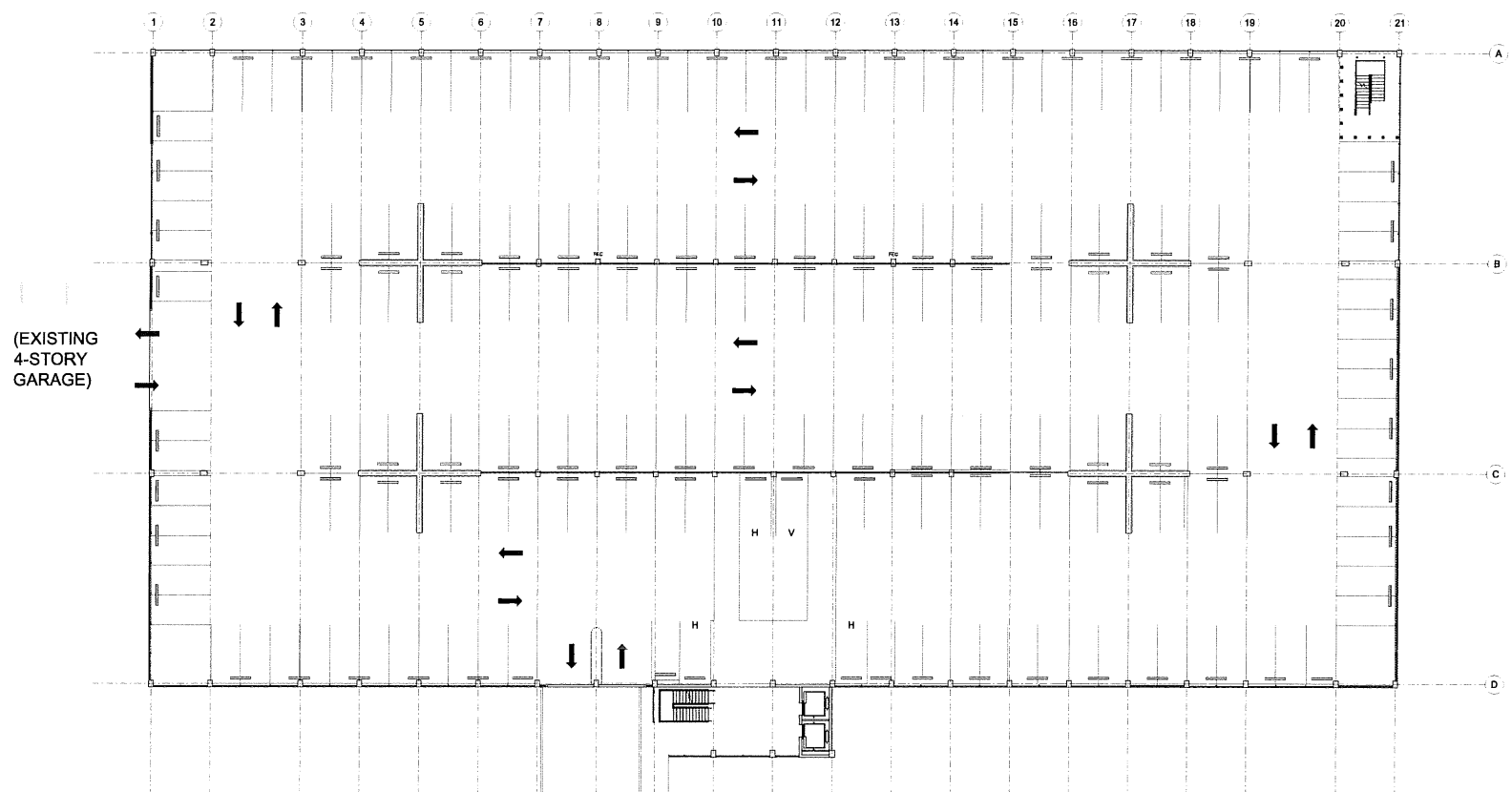
SHEET NUMBER

9C

© LARG ARCHITECT



02 FOURTH FLOOR PLAN  
SCALE: 3/8" = 1'-0"



01 THIRD FLOOR PLAN  
SCALE: 3/8" = 1'-0"

1551 SOUTH ROBERTSON BLVD. 2ND FLOOR  
LOS ANGELES, CALIFORNIA 90035  
T: 310.553.1850  
F: 310.552.1850

OWNER

STEELWAVE  
4000 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650.571.3200

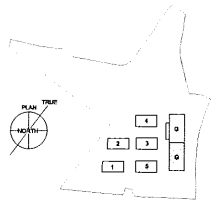
CONSULTANTS

CIVIL ENGINEERING

BNF ENGINEERS  
100 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.630.7900

LANDSCAPE ARCHITECT

GABRIELLI AND ASSOCIATES  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415.674.0900



NO.	ISSUE	DATE
1	PD ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/24/12
3	PD ADJUSTMENT	02/13/13
4	PD ADJUSTMENT	11/08/13
5	PD AMENDMENT	04/06/15
6	PD AMENDMENT	09/11/15
7	PD AMENDMENT RESUBMITTAL	01/28/16

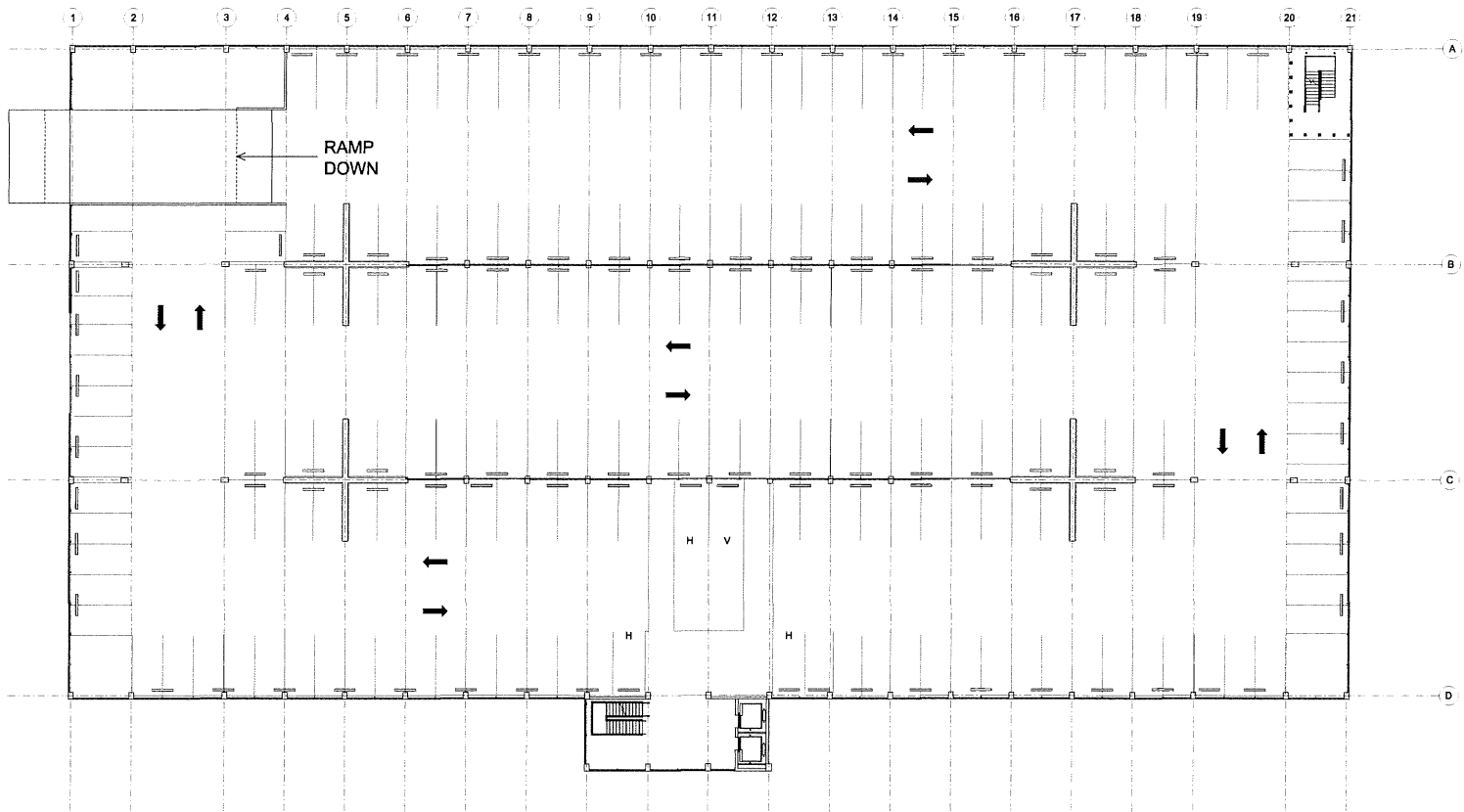


AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

GARAGE  
EXPANSION PLANS

PROJECT 2015-001	SHEET NUMBER <b>9D</b>
SCALE 3/8" = 1'-0"	
DRAWN BY -	
DATE 09/11/15	

© LRG ARCHITECTURE



01 FIFTH FLOOR PLAN  
SCALE: 3/8" = 1'-0"

POST DATE: 09/09/15 4:57 PM

LEGEND

SYMBOL	DESCRIPTION
---	PROPERTY LINE
LOW LOW LOW LOW	LANDSCAPE LIMIT OF WORK
	(E) CONDITIONS, SEE TOPOGRAPHIC SURVEY

MATERIALS LEGEND

SYMBOL	DESCRIPTION
	12"x12"x4" STEP STONE VEHICULAR PAVERS (7/10.04)
	12"x24"x2" STEP STONE PEDESTRIAN PAVERS ON GRADE (7/10.04)
	PEDESTRIAN PAVES MATCH EXISTING PEDESTRIAN PAVES FROM PHASE 1 (7/10.04) SIM.
	MEXICAN PEBBLE BAND ALONG EDGE OF BUILDING, WIDTH VARIES, SET TOP OF PEBBLE BAND 1" BELOW THE ADJACENT FINISH FLOOR ELEVATION OF BUILDING S, TYP. (2/10.04)
	TRUNCATED DOME PAVES (5/10.05)
	PLANTING AREA, SEE PLANTING PLAN
	STEEL HEADER (3/10.04)
	STAIR CASE WITH HANDRAIL (2/10.05)
	LANDSCAPE PLANTER CURB / WALL (3/10.05) (5/10.04) (4/10.04)
	4" DIA. PERFORATED PIPE IN PLANTING AREA, SEE SHEET L2.01 TO L2.04 FOR LOCATIONS, SEE CIVIL FOR STORM DRAIN CONNECTION. (3/10.05)
	CLEAN OUT IN PLANTING AREA, SEE SHEET L2.01 TO L2.04 FOR LOCATIONS. SEE CIVIL DRAWING FOR STORM DRAIN CONNECTION. (3/10.05)
	PLANTER - TOURNESEUL WILSHIRE COLLECTION - #03D30 PROVIDE PLANTER BASE AND STABILIZED ANCHOR, INSTALL PER MANUFACTURER DRAINAGE PER MANUFACTURER RECOMMENDATION AND SPECIFICATION. SUBMIT COLOR SAMPLE FOR SELECTION BY OWNER. (2/10.05)
	TREE, SEE PLANTING PLAN

MATERIALS NOTES

- SEE LAYOUT PLAN FOR LOCATION OF WALL AND PLANTER CURB EXPANSION JOINT, PERFORATED PIPES AND CLEANOUTS.
- PEDESTRIAN AND VEHICULAR PAVERS THAT ARE NOT EDGED BY CONCRETE WALL, CONCRETE BAND, OR CONCRETE PAVING, SHALL RECEIVE METAL HEADER EDGE, TYPICAL.
- SEE SHEET L1.05 FOR PAVING MATERIAL, CONCRETE RETAINING WALL & CONCRETE CURB LOCATIONS.
- SEE CIVIL DRAWING FOR CURBS, GUTTERS, CURB RAMPS ROADWAY PAVING, TRAFFIC SIGNAGE PARKING STRIPING, UTILITIES, ADA PARKING, STORM DRAIN CONNECTION.
- SEE ARCHITECT DRAWINGS FOR PATH OF TRAVEL.
- SET ALL UTILITIES BOXES IN PAVING FLUSH WITH FINISH GRADE.
- SEE ELECTRICAL DRAWINGS FOR LIGHT FIXTURE, OUTLETS AND LOCATIONS.

PATH OF TRAVEL NOTES

- BARRIER FREE ACCESS ROUTE WITH FOLLOWING CHARACTERISTICS:
  - LESS THAN 1:20 MAXIMUM SLOPE WITHOUT RAMP
  - LESS THAN 1:50 MAXIMUM CROSS SLOPE
  - NO ABRUPT CHANGES IN LEVEL
  - 48" MIN CLEAR IN WIDTH
  - NON-SLIP SURFACE
  - ACCESSIBLE PATH SHALL HAVE NO ABRUPT VERTICAL CHANGES EXCEEDING 1/2" AT 1:2 MAXIMUM SLOPE, EXCEPT THAT LEVEL CHANGES DO NOT EXCEED 1/4".
  - MAXIMUM SLOPE OF RAMPS IS 1:12CONTRACTOR TO VERIFY THAT ALL BARRIERS IN THE PATH OF TRAVEL HAVE BEEN REMOVED OR WILL BE REMOVED IN THIS PROJECT, & THAT PATH OF TRAVEL COMPLIES WITH 2013 CALIFORNIA BUILDING CODE CHAPTER 11B
- CONSTRUCT ALL PAVED AREAS TO DRAIN
- OPENING AND GATES IN PATH OF TRAVEL MUST COMPLY WITH CALIFORNIA BUILDING CODE EXIT DOOR REQUIREMENTS.
- DOOR AND GATE OPENING FORCE TO BE 5 POUNDS MAXIMUM PER CALIFORNIA BUILDING CODE DOOR AND GATE OPENING FORCE.

INDEX

Sheet No.	Drawing No.	Sheet Title
1	10.01	MATERIALS LEGEND
2	10.02	MATERIALS PLAN - SOUTH
3	10.03	MATERIALS PLAN - NORTH
4	10.04	CONSTRUCTION DETAILS
5	10.05	CONSTRUCTION DETAILS
6	10.06	CONSTRUCTION DETAILS
7	10.07	PLANTING LEGEND AND NOTES
8	10.08	PLANTING PLAN - SOUTH
9	10.09	PLANTING PLAN - NORTH
10	10.10	PLANTING DETAILS
11	10.11	IRRIGATION LEGEND & NOTES
12	10.12	IRRIGATION PLAN - SOUTH
13	10.13	IRRIGATION PLAN - NORTH
14	10.14	IRRIGATION DETAILS
15	10.15	IRRIGATION DETAILS
16	10.16	IRRIGATION DETAILS

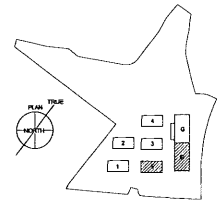
CARDUCCI & ASSOCIATES, INC.  
LANDSCAPE ARCHITECTURE PLANNING  
IRRIGATION DESIGN URBAN DESIGN  
CARDUCCI  
LANDSCAPE 355 BEACH STREET, 4TH FLOOR  
ARCHITECTS SAN FRANCISCO, CALIFORNIA 94133  
PHONE (415) 674-0990 FAX (415) 674-0999  
© 2013 CARDUCCI & ASSOCIATES, INC.

OWNER  
STEELWAVE  
4000 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650.571.2200

CONSULTANTS  
ARCHITECT  
LARGE ARCHITECTS  
1801 SOUTH ROBERTSON BOULEVARD, 2ND FLOOR  
LOS ANGELES, CA 90035  
TEL: 310.563.1599

CIVIL ENGINEERING  
RKF ENGINEERS  
850 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.250.7500

LANDSCAPE ARCHITECT  
CARDUCCI AND ASSOCIATES  
355 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415.674.0990



NO.	ISSUE	DATE
1	PD ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/24/12
3	PD ADJUSTMENT	02/13/13
4	PD ADJUSTMENT	11/08/13
5	PD AMENDMENT	04/09/15
6	PD AMENDMENT	09/11/15



AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

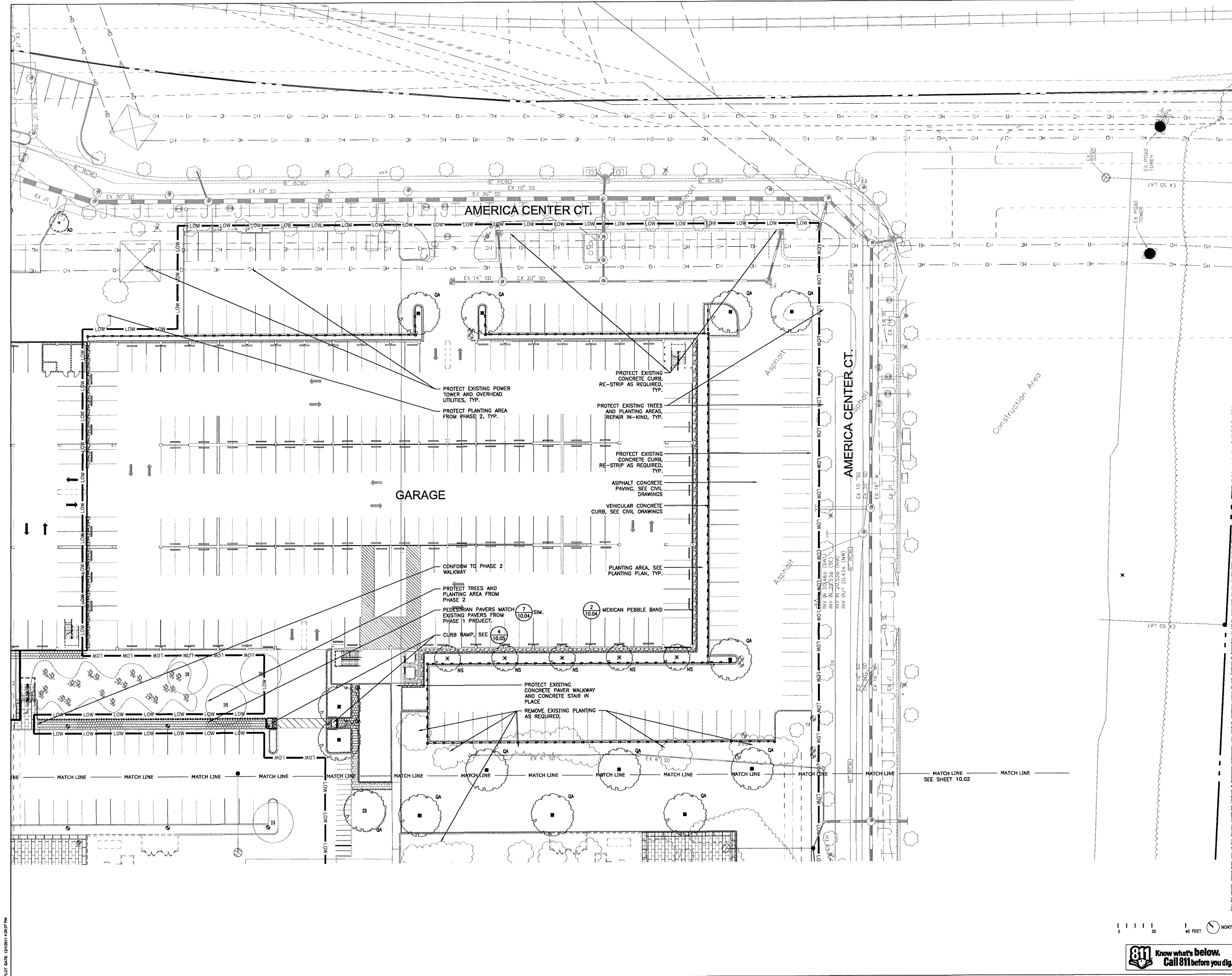
MATERIALS  
LEGEND



PROJECT  
2015-001  
SCALE  
DRAWN BY  
DATE  
09/11/15

SHEET NUMBER  
10.01





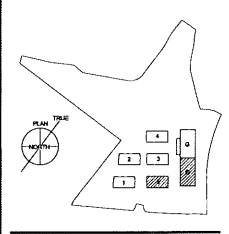
CARDUCCI & ASSOCIATES, INC.  
LANDSCAPE ARCHITECTURE PLANNING  
IRRIGATION DESIGN URBAN DESIGN  
4440 LUCY CT  
SAN FRANCISCO, CA 94114  
PHONE (415) 674-9990 FAX (415) 674-9999  
© 2015 CARDUCCI & ASSOCIATES, INC.

**OWNER**  
**STEELWAVE**  
4000 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650.571.2200

**CONSULTANTS**  
**ARCHITECT**  
LARGE ARCHITECTURE  
1501 SOUTH ROBERTSON BOULEVARD, 2ND FLOOR  
LOS ANGELES, CA 90035  
TEL: 213.553.1959

**CIVIL ENGINEERING**  
BKT ENGINEERS  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.393.7900

**LANDSCAPE ARCHITECT**  
CARDUCCI AND ASSOCIATES  
4440 LUCY STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94114  
TEL: 415.674.9990



NO.	ISSUE	DATE
1	PD ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/24/12
3	PD ADJUSTMENT	02/13/13
4	PD ADJUSTMENT	11/06/13
5	PD AMENDMENT	04/06/15
6	PD AMENDMENT	09/11/15

STEELWAVE

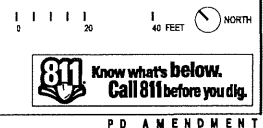
AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

MATERIALS PLAN  
- NORTH

PROJECT  
2015-001  
SCALE  
DRAWN BY  
DATE  
09/11/15

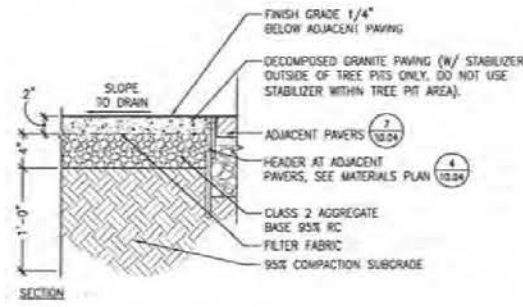
SHEET NUMBER  
**10.03**

PD AMENDMENT

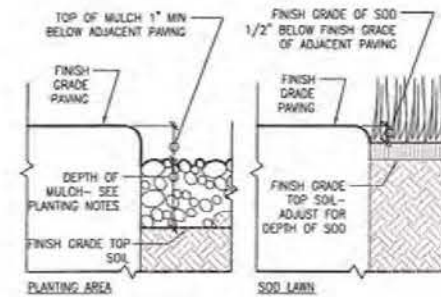
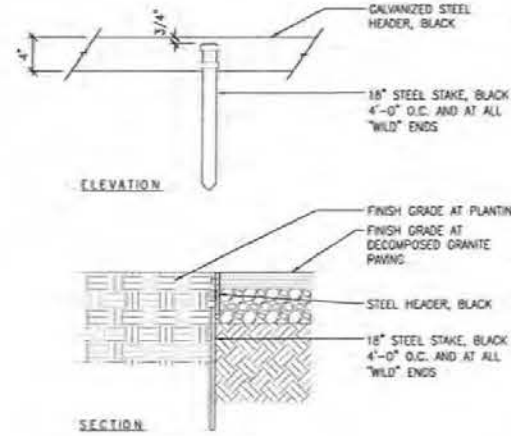


PLOT DATE: 10/26/15 1:45:27 PM





- NOTES:
1. PROVIDE MOCKUP FOR APPROVAL.
  2. PROVIDE 3/8\"
  3. PROVIDE 1/8\"

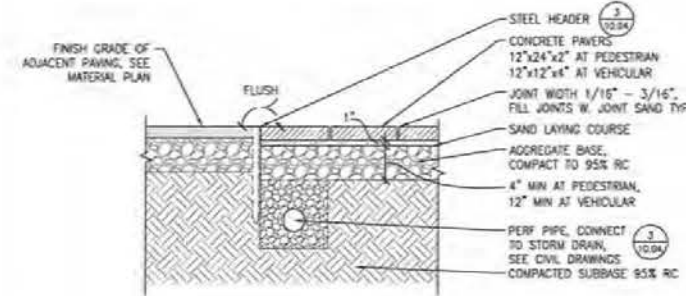


- NOTE:
1. FINISH ELEVATION IS MEASURED AFTER SOIL PREPARATION AND FIRING OF SOIL BY WETTING FROM IRRIGATION SYSTEM TO MINIMIZE FUTURE SETTLING.
  2. IN A CONDITION WHERE THE PAVES NEEDS TO BE CUT, USE NO LESS THAN HALF OF THE PAVE (LENGTH AND WIDTH) ADJUST PAVES TO ACHIEVE THE RESULT AS REQUIRED.

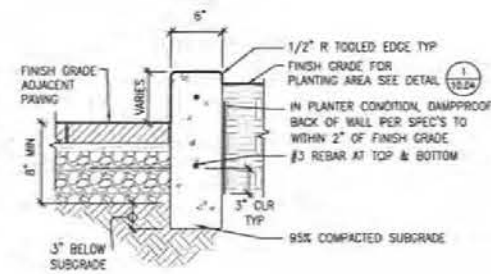
## 6 DECOMPOSED GRANITE PAVING

## 3 STEEL HEADER

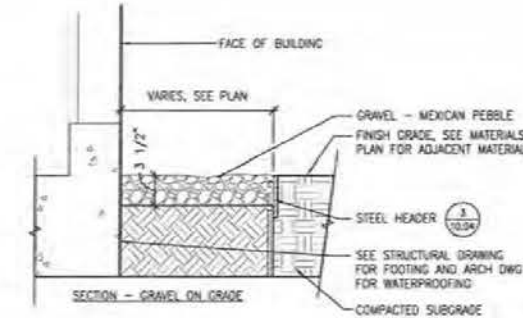
## 1 GRADE AT EDGE OF PAVING



- NOTES:
1. PROVIDE MOCKUP ON SITE FOR REVIEW & ACCEPTANCE
  2. INSTALL PAVES FLUSH TYP

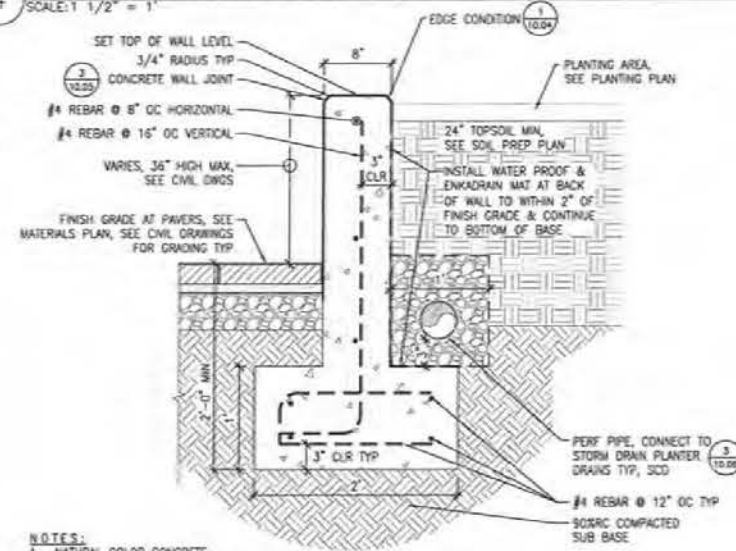


- NOTES:
1. MEDIUM SAND BLAST ALL EXPOSED SURFACES
  2. PROVIDE EXPANSION JOINTS AT 20' OC, SCORE JOINTS MAX OF 5' OC
  3. COLOR FOR ALL EXPOSED CONCRETE: NATURAL CONCRETE COLOR W/ LAMP BLACK COLOR
  4. SEE CIVIL DRAWINGS FOR VEHICULAR CONCRETE CURB

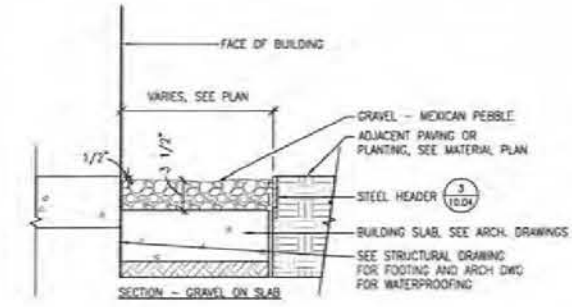


## 7 PAVERS

## 4 LANDSCAPE CONCRETE CURB



- NOTES:
1. NATURAL COLOR CONCRETE.
  2. 2X4 BOARD FORM LEVEL HORIZONTAL AND LIGHT SANDBLAST CONCRETE ON ALL EXPOSED SURFACES.
  3. EXPANSION JOINTS, 20'-0\"
  4. PROVIDE WATER PROOFING AT BACK OF WALL WHEN RETAINING SOIL.
  5. PROVIDE SHOP DRAWING & MOCK-UP ON SITE FOR APPROVAL.



## 5 CONCRETE RETAINING WALL

## 2 GRAVEL EDGE

CARDUCCI & ASSOCIATES, INC.  
LANDSCAPE ARCHITECTURE, PLANNING  
300 EAST THIRD AVENUE, SUITE 200  
SAN JOSE, CA 95131  
TEL: 408.281.2200  
FAX: 408.281.2201

OWNER  
AMERICA CENTER  
300 EAST THIRD AVENUE, SUITE 200  
SAN JOSE, CA 95131  
TEL: 408.281.2200

CONSULTANTS  
ARCHITECT  
LARGE BUILDINGS  
300 EAST THIRD AVENUE, SUITE 200  
SAN JOSE, CA 95131  
TEL: 408.281.2200

CIVIL ENGINEERING  
RAY ENGINEERS  
700 CALIFORNIA STREET  
SAN FRANCISCO, CA 94101  
TEL: 415.398.1000

LANDSCAPE ARCHITECT  
CARDUCCI & ASSOCIATES  
300 EAST THIRD AVENUE, SUITE 200  
SAN JOSE, CA 95131  
TEL: 408.281.2200

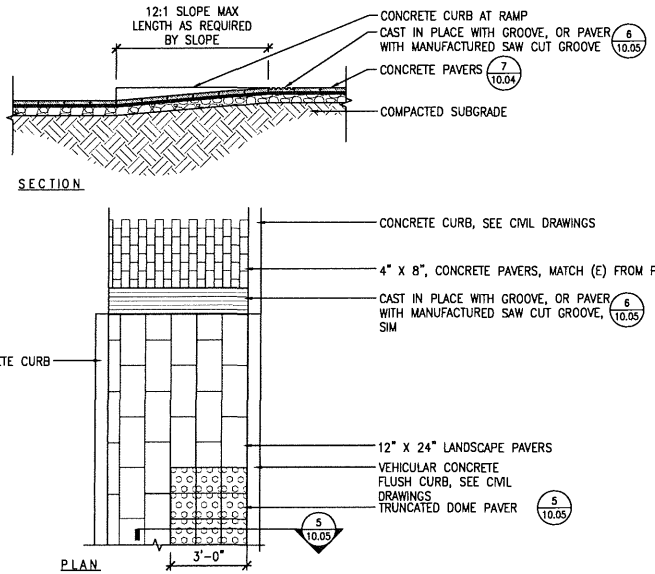
NO. 10.04  
ISSUE DATE  
10/01/15

STEELWAVE

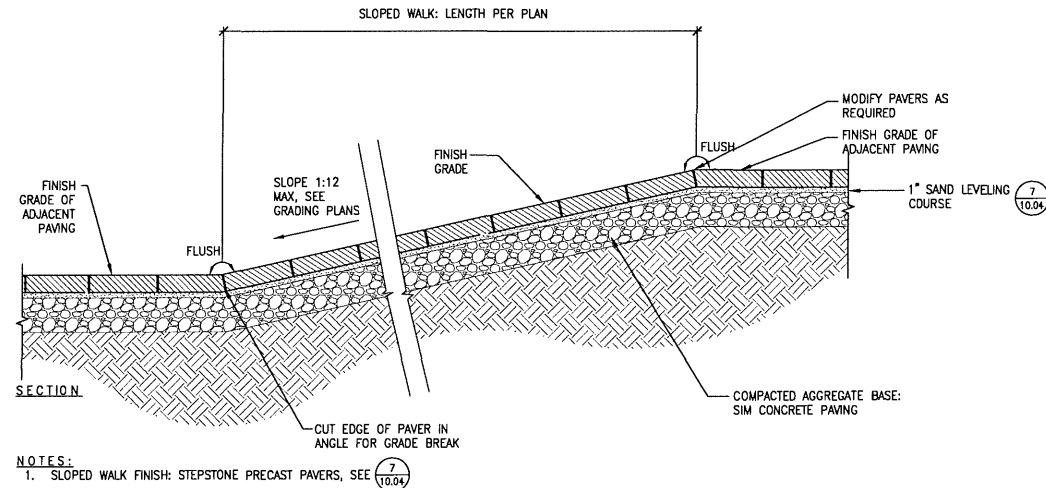
AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

CONSTRUCTION  
DETAILS

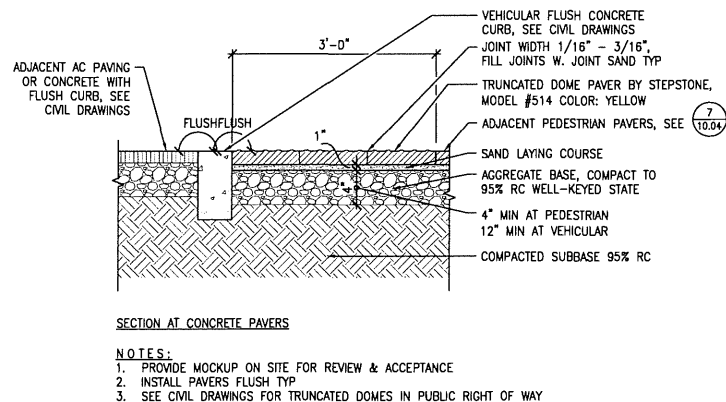
PROJECT  
2015-001  
SCALE  
1\"/>



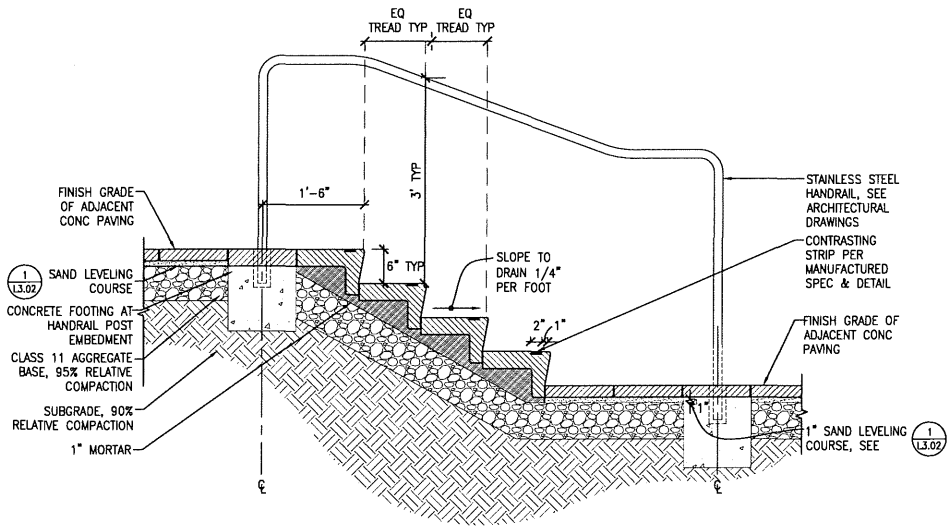
4 CURB RAMP  
SCALE: 3/8" = 1'



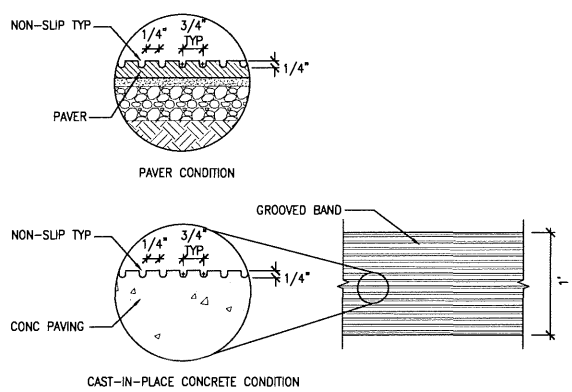
1 SLOPE WALK ADJACENT TO BUILDING  
SCALE: 1" = 1'



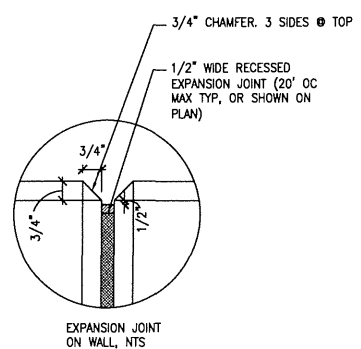
5 TRUNCATED DOMES - PAVEMENT  
SCALE: 1" = 1'



2 PAVEMENT STAIR CASE  
SCALE: 1" = 1'



6 GROOVED CONC BAND  
SCALE: 1 1/2" = 1'



3 CONCRETE WALL JOINTS  
SCALE: 1 1/2" = 1'

CARDUCCI & ASSOCIATES, INC.  
LANDSCAPE ARCHITECTURE PLANNING  
IRRIGATION DESIGN URBAN DESIGN  
ARCHITECT  
215 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CALIFORNIA 94133  
PHONE (415) 874-0994 FAX (415) 874-0999  
© 2013 CARDUCCI & ASSOCIATES, INC.

OWNER

STEELWAVE  
4000 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650-571-5200

CONSULTANTS

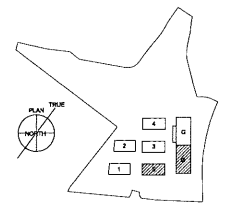
ARCHITECT  
LARGE ARCHITECTURE  
1881 SOUTH HOUGHTON BOULEVARD, 2ND FLOOR  
LOS ANGELES, CA 90035  
TEL: 310-553-1969

CIVIL ENGINEERING

BEF ENGINEERS  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415-336-7900

LANDSCAPE ARCHITECT

CARDUCCI AND ASSOCIATES  
665 REACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415-874-0990



NO.	ISSUE	DATE
1	PD ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/24/12
3	PD ADJUSTMENT	02/13/13
4	PD ADJUSTMENT	11/08/13
5	PD AMENDMENT	04/06/15
6	PD AMENDMENT	09/11/15

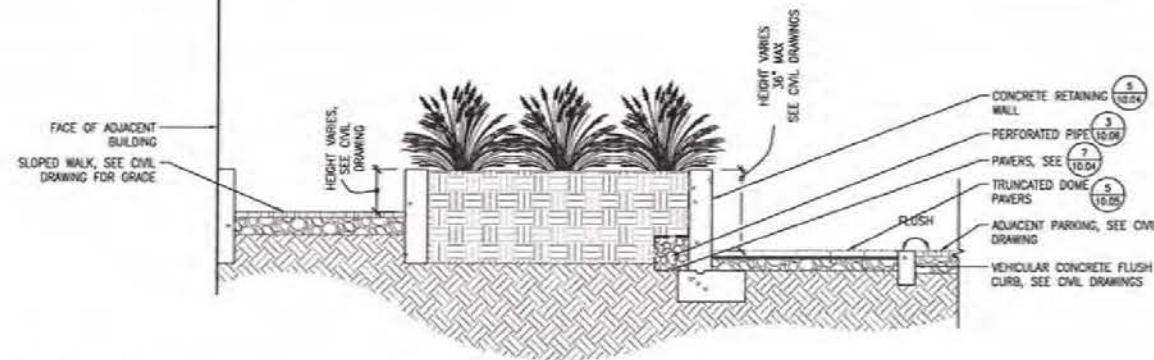
STEELWAVE

AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

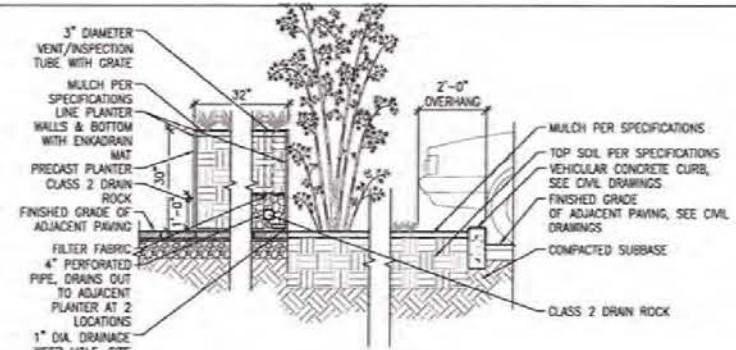
CONSTRUCTION  
DETAILS

PROJECT  
2015-001  
SCALE  
DRAWN BY  
DATE  
09/11/15  
SHEET NUMBER  
10.05

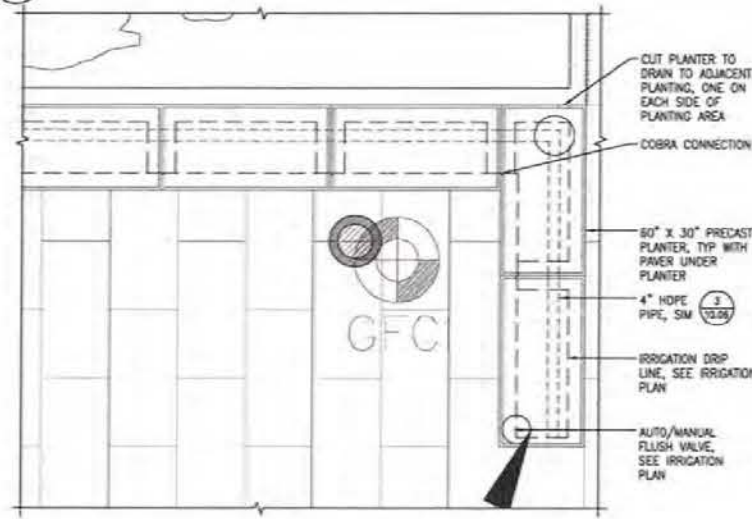




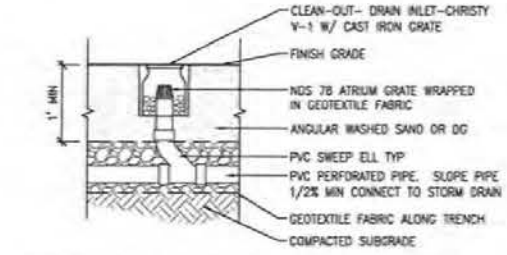
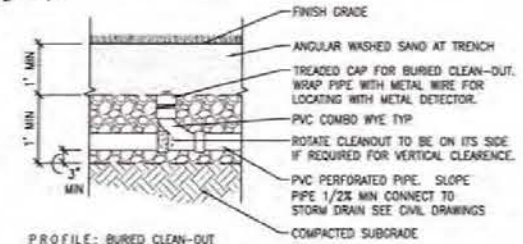
4 SECTION BLDG 5  
SCALE: 1/2" = 1'



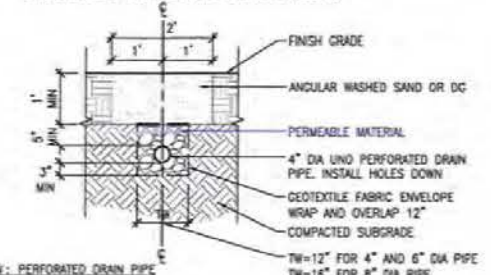
1 SECTION 1  
SCALE: 1/2" = 1'



2 PLANTER UTILITY DIAGRAM  
SCALE: 1/2" = 1'



PROFILE: PERFORATED DRAIN PIPE AND CLEAN-OUT



3 SECTION: PERFORATED DRAIN PIPE  
SCALE: 3/4" = 1'

OWNER

STEELWAVE  
4800 EAST THIRD AVENUE, SUITE 200  
FOSTER CITY, CA 94404  
TEL: (650) 371-1200

CONSULTANTS

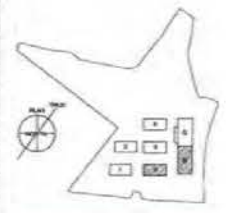
ARCHITECT  
LARGE ARCHITECTURE  
1000 NORTH HARBORVIEW BOULEVARD, 2ND FLOOR  
LOS ANGELES, CA 90015  
TEL: (213) 551-1000

CIVIL ENGINEERING

BAV ENGINEERING  
80 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: (415) 392-7800

LANDSCAPE ARCHITECT

CARBONETTI & ASSOCIATES  
300 BAY STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94111  
TEL: (415) 394-1000



NO.	REVISION	DATE
1	PD ADJUSTMENT	11/05/12
2	PD ADJUSTMENT	11/05/12
3	PD ADJUSTMENT	11/05/12
4	PD ADJUSTMENT	11/05/12
5	PD ADJUSTMENT	11/05/12
6	PD ADJUSTMENT	11/05/12
7	PD ADJUSTMENT	11/05/12
8	PD ADJUSTMENT	11/05/12
9	PD ADJUSTMENT	11/05/12
10	PD ADJUSTMENT	11/05/12

STEELWAVE

AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

CONSTRUCTION  
DETAILS



## PLANTING LEGEND

### TREES

Count	KEY	BOTANICAL NAME	COMMON NAME	SIZE	TRUNK	SPACING	WATER USE (WUCOLS)
5	AS	ACACIA STENOPHYLLA	SHOESTRING ACACIA	24" BOX	SINGLE	VL	VL
13	CO	CERCIS OCCIDENTALIS	WESTERN REDBUD	24" BOX	MULTI-TRUNK	VL	VL
5	NS	NYSSA SYLMATICA	TUPELO	24" BOX	STANDARD	M	M
17	QA	QUERCUS AGRIFOLIA	COAST LIVE OAK	24" BOX	STANDARD	VL	VL

### SHRUBS AND GROUNDCOVERS

Count	KEY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	WATER USE (WUCOLS)	NOTES
12	AM	ARCTOSTAPHYLOS DENSIFLORA	HOWARD MCMINN	5 GAL	4' OC	L	---
455	CT	CHONDROPETALUM TECTORUM	CAPE RUSH	5 GAL	3'-6"	L	---
70	J	JUNCUS PATENS 'ELK BLUE'	CALIFORNIA GRAY RUSH	1 GAL	2'-6" OC	L	---
83	L	LEYMUS ARENARIUS 'GLAUCUS'	BLUE LYMES GRASS	5 GAL	3' OC	L	---
138	LC	LEYMUS CONDENSATUS 'CANYON PRINCE'	LYME GRASS	1 GAL	3' OC	L	---
12	RC	RHAMNUS CALIFORNICA 'Mound San Bruno'	COFFEEBERRY	1 GAL	5' OC	L	---
12	RV	RIBES VIBURNIFOLIUM	CATALPA PERFUME	5 GAL	4' OC	L	---
8,165 FT <sup>2</sup>		HELIOTROPION SEMPERVIRENS	BLUE CAT GRASS	1 GAL	2' O.C.	L	---

### VINES

Count	KEY	BOTANICAL NAME	COMMON NAME	ORIGIN	SPACING	WATER USE (WUCOLS)	SIZE	NOTES
5	F	FICUS PUMILA	CREeping FIG	CHINA, JAPAN, AUSTRALIA	AS SHOWN	M	5 GAL	---

### BAMBOO

Count	KEY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	WATER USE (WUCOLS)	NOTES
121	B3	BAMBUSA OLDHAMII	GIANT TIMBER BAMBOO	15 GAL	15'	L	---
65	FR	FARGESIA ROBUSTA	ROBUSTA BAMBOO	15 GAL	5' OC	VL	15'-40' FLOWER STALK AFTER 10 YRS OR MORE

### GROUNDCOVERS

	NATIVE BENT GRASS SOD
	MULCH AREA

### PLANTING NOTES

1. PLANT MATERIAL QUANTITIES ARE SHOWN FOR INFORMATION ONLY. CONTRACTOR SHALL VERIFY ALL QUANTITIES PRIOR TO ORDER.
2. SUBSTITUTIONS OF PLANT MATERIAL, SPECIES AND/OR VARIETIES ARE NOT PERMITTED.
3. SUBMIT PLANT LIST SHOWING QUANTITIES AND AVAILABILITY OF PLANT MATERIAL WITHIN 30 DAYS OF NOTICE TO PROCEED.
4. ALL PLANT MATERIAL SHALL CONFORM TO THE ANSI Z601 "STANDARD FOR NURSERY STOCK".
5. AMEND SOILS PER SOIL TEST RECOMMENDATIONS. SEE SPECIFICATIONS.
6. APPLY A THREE INCH (3") LAYER OF MULCH IN ALL NON-TURF AND NON-GRASS/PLUG AREAS AND TREE/SHRUB BASINS. KEEP BARK SIX INCHES (6") AWAY FROM TREE CROWNS.
7. PLACE PLANTS IN PROPOSED LOCATIONS FOR REVIEW AND ACCEPTANCE BY THE PROJECT MANAGER PRIOR TO ACTUAL PLANTING. NOTIFY THE PROJECT MANAGER A MINIMUM OF 48 HOURS PRIOR.
8. ALL LANDSCAPED AREAS SHALL BE MAINTAINED IN A HEALTHY AND WEED-FREE CONDITION. DEAD PLANT MATERIAL SHALL BE REPLACED IMMEDIATELY. ALL TREES SHALL BE MAINTAINED AND PRUNED IN ACCORDANCE TO THE INTERNATIONAL SOCIETY OF ARBORICULTURE (ISA).
9. INSTALL PLANT MATERIAL PER SPECIFICATIONS.
10. NOTIFY PROJECT MANAGER 48 HOURS PRIOR TO PLANT DELIVERY. ALL PLANTS TO BE REVIEWED AND ACCEPTED PRIOR TO PLANTING.
11. ALL PLANTS LISTED ABOVE TO BE INSTALLED BY CONTRACTOR.
12. POT HOLE ALL TREE PLANTING LOCATIONS TO IDENTIFY CONFLICTS UNDERGROUND UTILITIES BEFORE TREE PLANTING.
13. WHERE POSSIBLE LOCATE TREES AT LEAST 5' FROM EDGE OF WALKS, PAVING, AND UNDERGROUND UTILITIES. ADJUST LOCATION IN FIELD AS REQUIRED AND AS APPROVED BY LANDSCAPE ARCHITECT.
14. SEE CIVIL DRAWINGS, ELECTRICAL DRAWINGS AND LANDSCAPE DRAWINGS FOR APPROXIMATE LOCATION OF UTILITIES.
15. SLOPE PLANTING AREAS TO DRAIN.

### LEGEND

SYMBOL	DESCRIPTION
	PROPERTY LINE
	LANDSCAPE LIMIT OF WORK
	(E) CONDITIONS, SEE TOPOGRAPHIC SURVEY

#### OWNER

STEELWAVE  
4000 EAST THIRD AVENUE, SUITE 100  
FOSTER CITY, CA 94404  
TEL: 650.571.2200

#### CONSULTANTS

##### ARCHITECT

LARSEN ARCHITECTS  
1501 SOUTH ROBERTSON BOULEVARD, 2ND FLOOR  
LOS ANGELES, CA 90006  
TEL: 310.551.9888

##### CIVIL ENGINEERING

REY ENGINEERS  
500 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.398.7300

##### LANDSCAPE ARCHITECT

GARDUCY & ASSOCIATES  
355 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94111  
TEL: 415.441.4141



NO.	ISSUE	DATE
1	PO ADJUSTMENT	05/01/12
2	CITY RESPONSE	02/24/12
3	PO ADJUSTMENT	02/19/13
4	PO ADJUSTMENT	11/06/13
5	PO ADJUSTMENT	04/06/15
6	PO ADJUSTMENT	05/11/15



**AMERICA CENTER**  
AMERICA CENTER DRIVE  
SAN JOSE, CA

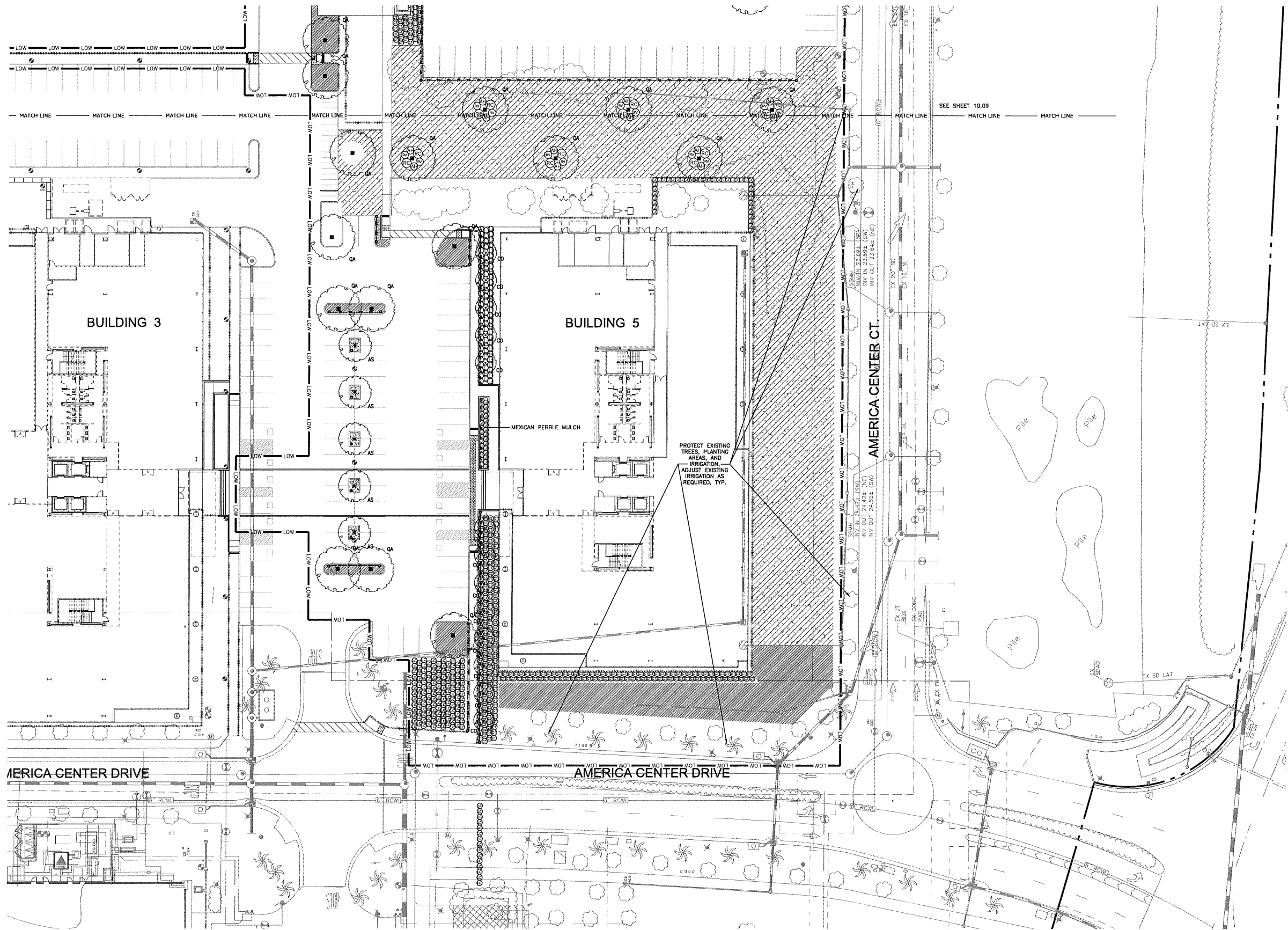
### PLANTING LEGEND AND NOTES

PROJECT 2015-001	SHEET NUMBER 10.07
SCALE -	DATE 05/11/15
DRAWN BY -	DATE 05/11/15

TITLE 23 MODEL WATER EFFICIENT LANDSCAPE ORDINANCE COMPLIANCE  
I HAVE COMPLIED WITH THE CRITERIA OF THE ORDINANCE AND APPLIED THEM ACCORDINGLY FOR THE EFFICIENT USE OF WATER IN THE LANDSCAPE DESIGN PLAN.



PD AMENDMENT



CARDUCCI & ASSOCIATES, INC.  
 LANDSCAPE ARCHITECTURE PLANNING  
 IRRIGATION DESIGN URBAN DESIGN  
 555 BEACH STREET, 4TH FLOOR  
 SAN FRANCISCO, CALIFORNIA 94133  
 PHONE (415) 474-0990 FAX (415) 474-0999  
 © 2015 CARDUCCI & ASSOCIATES, INC.

OWNER

STEELWAVE  
 4000 EAST THIRD AVENUE, SUITE 500  
 FOSTER CITY, CA 94404  
 TEL: 650 571 2006

CONSULTANTS

ARCHITECT

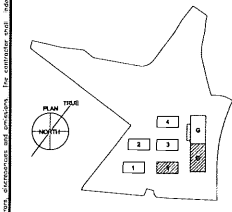
LARGE architecture  
 1501 SOUTH ROBERTSON BOULEVARD, 2ND FLOOR  
 LOS ANGELES, CA 90035  
 TEL: 310 253 1869

CIVIL ENGINEERING

RIF ENGINEERS  
 150 CALIFORNIA STREET  
 SAN FRANCISCO, CA 94111  
 TEL: 415 936 7900

LANDSCAPE ARCHITECT

CARDUCCI AND ASSOCIATES  
 555 BEACH STREET, 4TH FLOOR  
 SAN FRANCISCO, CA 94133  
 TEL: 415 474 0990



NO.	ISSUE	DATE
1	PD ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/24/12
3	PD ADJUSTMENT	02/13/13
4	PD ADJUSTMENT	11/08/13
5	PD AMENDMENT	04/08/15
6	PD AMENDMENT	09/11/15

STEELWAVE

AMERICA CENTER  
 AMERICA CENTER DRIVE  
 SAN JOSE, CA

PLANTING PLAN-  
 SOUTH

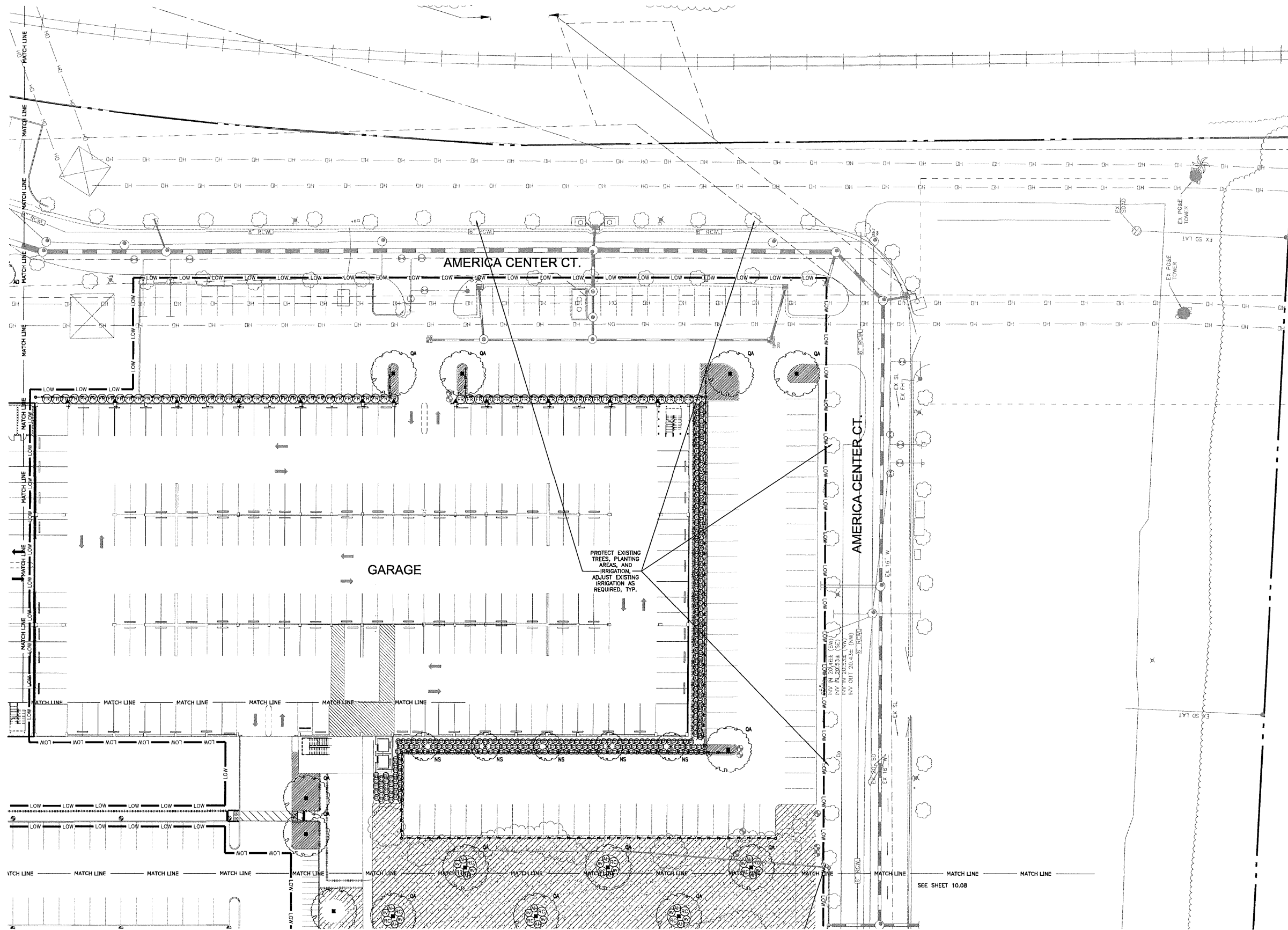
PROJECT  
 2015-001  
 SCALE  
 1"=40'  
 SHEET NUMBER  
 10.08  
 DRAWN BY  
 DATE  
 09/11/15

TITLE 23 MODEL WATER EFFICIENT LANDSCAPE ORDINANCE COMPLIANCE:  
 I HAVE COMPLIED WITH THE CRITERIA OF THE ORDINANCE AND APPLIED THEM ACCORDINGLY FOR THE EFFICIENT USE OF WATER IN THE  
 LANDSCAPE DESIGN PLAN.

811 Know what's below.  
 Call 811 before you dig.

PD AMENDMENT

PLANT DATE: 10/01/15 10:07 PM



CARDUCCI & ASSOCIATES, INC.  
LANDSCAPE ARCHITECTURE PLANNING  
IRIGATION DESIGN URBAN DESIGN  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CALIFORNIA 94133  
PHONE (415) 474-0990 FAX (415) 474-0999  
© 2015 CARDUCCI & ASSOCIATES, INC.

OWNER

STEELWAVE  
4000 EAST THIRD AVENUE, SUITE 300  
FOSTER CITY, CA 94404  
TEL: 650 871 2200

CONSULTANTS

ARCHITECT

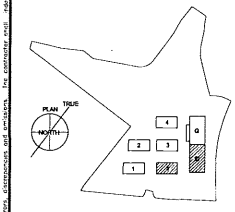
LARGE architecture  
1551 SOUTH ROBERTSON BOULEVARD, 2ND FLOOR  
LOS ANGELES, CA 90035  
TEL: 310 353 1909

CIVIL ENGINEERING

RFI ENGINEERS  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415 926 7900

LANDSCAPE ARCHITECT

CARDUCCI AND ASSOCIATES  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415 474 0990



NO.	ISSUE	DATE
1	PD ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/24/12
3	PD ADJUSTMENT	10/13/13
4	PD ADJUSTMENT	11/08/13
5	PD AMENDMENT	04/08/15
6	PD AMENDMENT	09/11/15



AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

PLANTING  
PLAN-NORTH

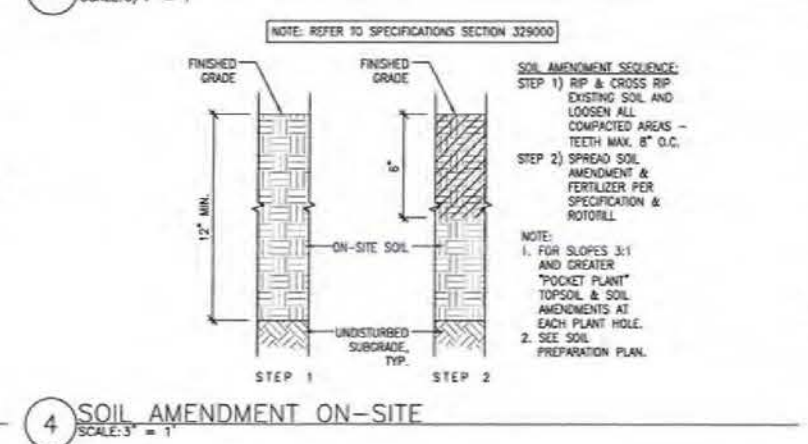
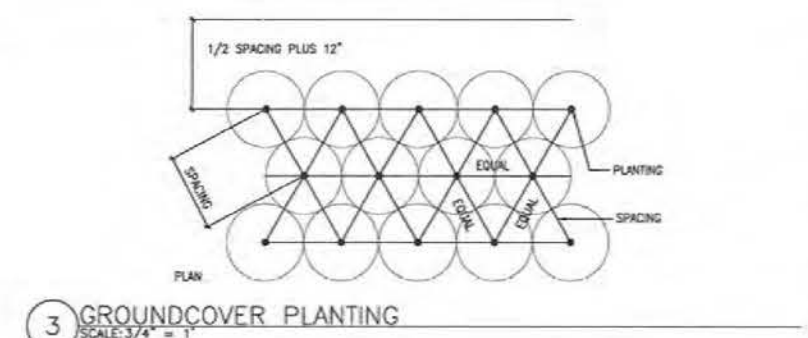
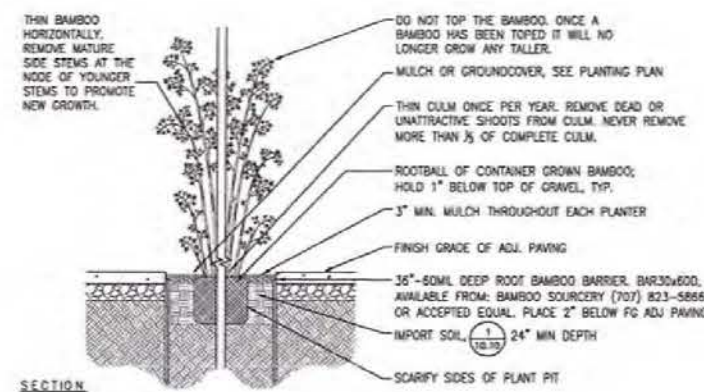
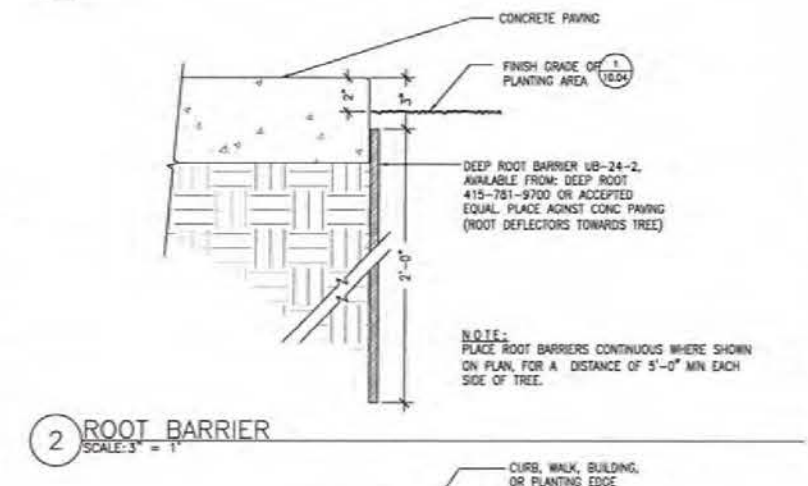
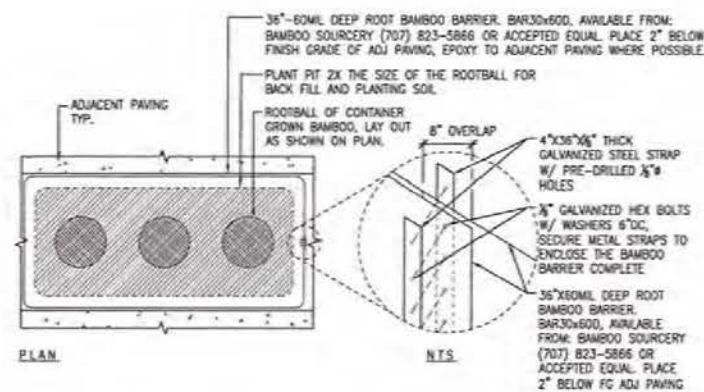
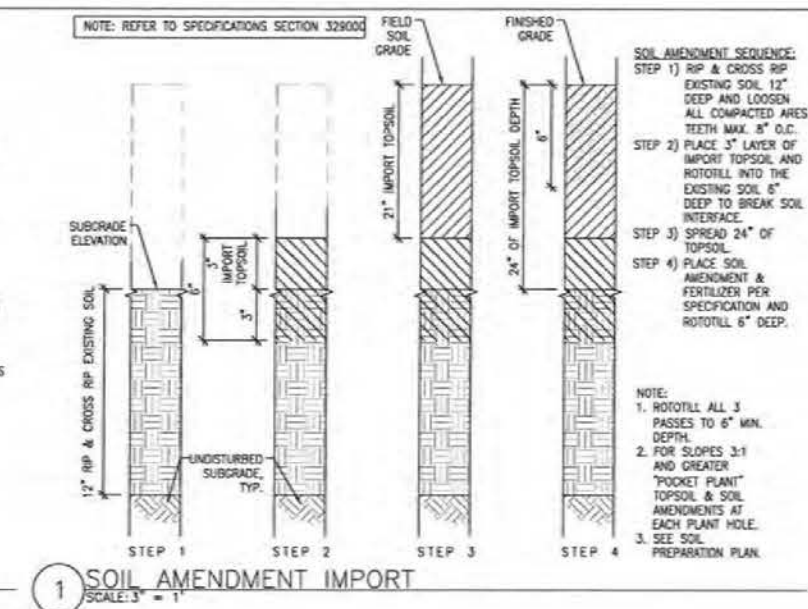
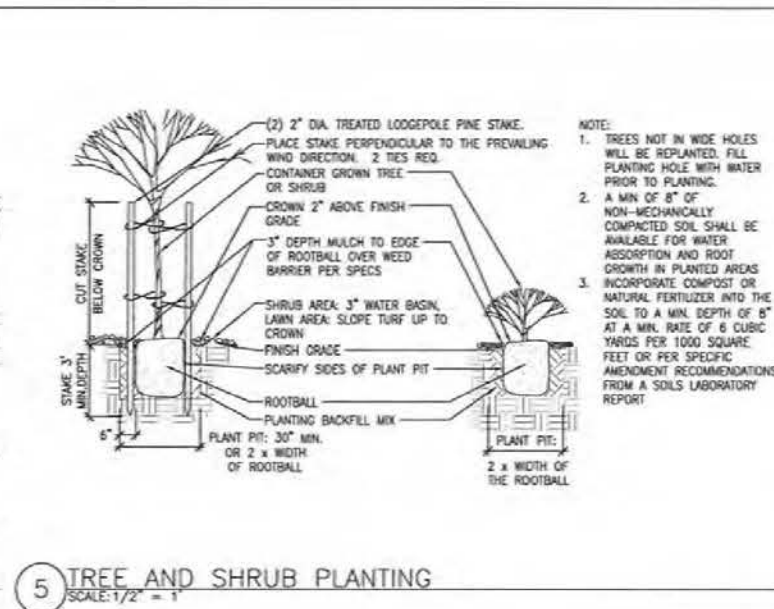
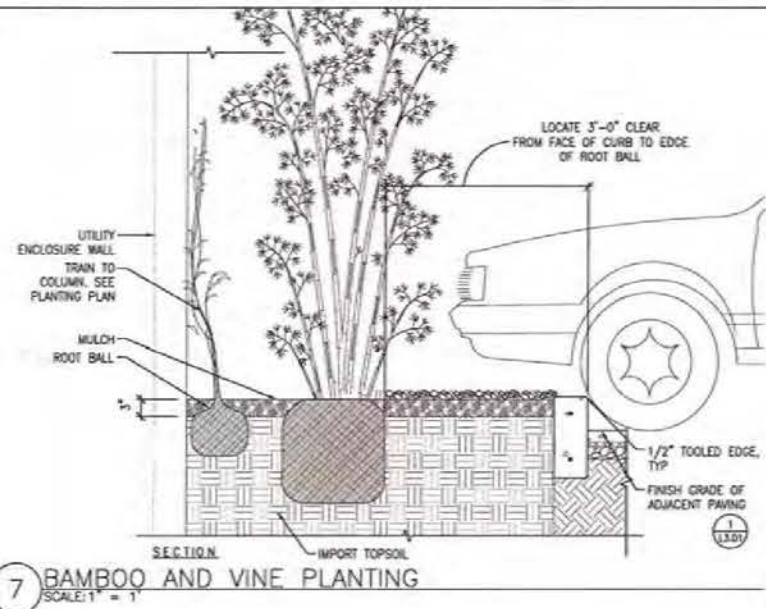
PROJECT  
2015-001  
SCALE  
DRAWN BY  
DATE  
09/11/15  
SHEET NUMBER  
10.09  
PD AMENDMENT

TITLE 23 MODEL WATER EFFICIENT LANDSCAPE ORDINANCE COMPLIANCE:  
I HAVE COMPLIED WITH THE CRITERIA OF THE ORDINANCE AND APPLIED THEM ACCORDINGLY FOR THE EFFICIENT USE OF WATER IN THE  
LANDSCAPE DESIGN PLAN.



PLANTING PLAN-NORTH





NO.	ISSUE	DATE
1	PO ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/01/12
3	PO ADJUSTMENT	05/13/13
4	PO ADJUSTMENT	11/28/13
5	PO AMENDMENT	04/05/15
6	PO AMENDMENT	09/11/15

STEELWAVE

AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

PLANTING DETAILS



## DRIP IRRIGATION LEGEND

1 10/15	02K-700-1-LF (FLOW BETWEEN 0.1 TO 7.9 GPM) 02K-700-1-MF (FLOW BETWEEN 8.0 TO 20 GPM)	TORO	700 ULTRAFLOW IN-LINE DRIP ZONE VALVE KITS WITH Y-FILTER, REMOTE CONTROL VALVE, ISOLATION BALL VALVE & MED/LOW FLOW PRESSURE REGULATOR & EFFLUENT VALVE ID TAG
4 10/15	AS DETAIL	TORO	PVC TO DRIPPERLINE CONNECTION
1A 10/15	1/2" 1/2" 1/2"		VALVE CALLOUT W/ DRIP VALVE INFO, DRIP LINE MODEL, FLOW RATE & LINE SPACING
2 10/15	ROP-218 ROP-418 ROP-212	TORO	DRIP LINE MODEL (GPM AND Emitter Spacing) DRIP LINE SPACING (MAX)
3 10/15	AS DETAIL	TORO	DRIPPERLINE TO DRIPPERLINE CONNECTION, USE TORO LOC-EEZ FITTINGS COMPATIBLE WITH DRIPPERLINE PIPE. USE THIS CONNECTION AT TIGHT BENDS AND AS INDICATED ON DETAIL DIAGRAM
5 10/15	YO-500-34	TORO	AIR VACUUM RELIEF VALVE AND BOX PER DETAIL. CROSS-CONNECT ALL DRIPPERLINES TO AVOID WITH BLANK TUBING
7 10/15	FOH-N-FPT	TORO	AUTOMATIC FLUSH VALVE ASSEMBLY AND BOX PER DETAIL
8 10/15	T-DL-MP9 & BALL VALVE	TORO	MANUAL FLUSH VALVE ASSEMBLY AND AUTO-FLUSH VALVE IN BOX PER DETAIL
10-F-01		METAFM	POP-UP FLAG INDICATOR, SET FLUSH WITH FINISH GRADE. INSTALL PER MANUFACTURER'S SPECIFICATIONS. LOCATE AWAY FROM PATHWAYS, AT LOCATIONS DIRECTED BY THE SITE MAINTENANCE PERSONNEL
10/15	PVC CLASS 200 SCH 40 UNDER PAVING	PURPLE PIPE TYP	PURPLE NON-PRESSURE LATERAL IRRIGATION PIPE WITH SOLVENT WELD SCHEDULE 40 FITTINGS. 3/4" MIN PIPE SIZE. INSTALL AS COLLECTOR LATERAL FOR ALL WATER THAT HAS PASSED THROUGH DRIPPERLINES
8 10/15	8702 4P-PRK-COM-E 78-200-ADU-PC2.00	TORO BLACK	ADJUSTABLE FLOW FULL-CIRCLE FLOOD BUBBLER ON 4" POP-UP WITH PRESSURE REGULATOR AND X-FLOW. UP TO 2.0 GPM. SET FLOW AT 0.25 GPM, TYP
9 10/15	8702 4P-PRK-COM-E 58-4-180-PC2	TORO WHITE	180" 4 STREAM BUBBLER WITH 2" RADIUS ON 4" POP-UP WITH PRESSURE REG. AND X-FLOW
10/15	PROS-06-PR540-CV-R-MP1000 PROS-06-PR540-CV-R-MP1000 PROS-06-PR540-CV-R-MP1000 PROS-06-PR540-CV-R-MP2000 PROS-06-PR540-CV-R-MP2000 PROS-06-PR540-CV-R-MP2000	HUNTER OLIVE HUNTER MAROON HUNTER LT BLUE HUNTER RED HUNTER GREEN HUNTER BLACK	FULL CIRCLE STREAM SPRAY, 6" POP-UP, CHECK VALVE, WITH PRESSURE REG. ADJUSTABLE 90"-210" ARC STREAM SPRAY, 6" POP-UP, WITH PRESSURE REG. ADJUSTABLE 210"-270" ARC STREAM SPRAY, 6" POP-UP, WITH PRESSURE REG. FULL CIRCLE STREAM SPRAY, 6" POP-UP, WITH PRESSURE REG. ADJUSTABLE 210"-270" STREAM SPRAY, 6" POP-UP, WITH PRESSURE REG. ADJUSTABLE 90"-210" ARC STREAM SPRAY, 6" POP-UP, WITH PRESSURE REG.
10/15	PROS-06-PR540-CV-R-MP3000-90 PROS-06-PR540-CV-R-MP3000-90 PROS-06-PR540-CV-R-MP3000-90 PROS-06-PR540-CV-R-MP3000-90	HUNTER BLUE HUNTER TURQ HUNTER ORANGE	ADJUSTABLE 90"-210" ARC STREAM SPRAY, 6" POP-UP, WITH PRESSURE REG. CORNER STREAM SPRAY, 6" POP-UP, WITH PRESSURE REG. ADJUSTABLE 90"-210" ARC STREAM SPRAY, 6" POP-UP, WITH PRESSURE REG.

### RECYCLED WATER IRRIGATION NOTES:

- PRIOR TO RECEIVING RECYCLED WATER, THE SITE MUST BE PERMITTED BY SBWR. A PERMIT WILL BE GRANTED AFTER:
  - INSPECTION BY SBWR HAS BEEN COMPLETED SHOWING CONFORMANCE WITH SBWR RULES AND REGULATIONS;
  - A FINAL ON-SITE INSPECTION HAS BEEN CONDUCTED TO CONFIRM THAT ALL REQUIREMENTS HAVE BEEN MET;
  - SITE HAS PASSED REQUIRED CROSS-CONNECTION TEST PERFORMED BY A CERTIFIED ANWA CROSS-CONNECTION SPECIALIST (IF NO POTABLE WATER LINES CROSS THE SITE, THEN REQUIRED CROSS-CONNECTION TEST IS WAIVED).
- THE OWNER'S OR TENANT'S REPRESENTATIVE MUST ALSO COMPLETE A SITE SUPERVISOR TRAINING CLASS OFFERED BY SBWR IN ORDER TO RECEIVE A PERMANENT PERMIT. IN THE INTERIM BETWEEN CONNECTION AND TRAINING, THE TENANT OR OWNER WILL RECEIVE A TEMPORARY RECYCLED WATER PERMIT.
- CONTACT SBWR AT (408) 945-3042 FOR FURTHER INFORMATION.
- ALL WORK SHALL CONFORM TO EXISTING REGULATIONS INCLUDING BUT NOT LIMITED TO:
  - SOUTH BAY WATER RECYCLING RULES AND REGULATIONS
  - DEPARTMENT OF HEALTH SERVICES REGULATIONS
- CHANGES MADE TO THE APPROVED IRRIGATION PLANS SHALL BE SUBMITTED TO SBWR FOR REVIEW AND APPROVAL AT LEAST 2 WEEKS PRIOR TO START OF CONSTRUCTION.
- AT LEAST TWO DAYS PRIOR TO START OF CONSTRUCTION, CONTRACTOR AND SBWR INSPECTOR SHALL HOLD A PRE-CONSTRUCTION MEETING. TO SCHEDULE MEETING, CONTACT SBWR AT (408) 945-3000.
- NOTIFY SBWR INSPECTOR A MINIMUM OF AT LEAST 24 HOURS BEFORE WORK BEGINS. SBWR INSPECTOR MUST INSPECT AND/OR VERIFY:
  - PRESENCE OF PROPER BACKFLOW PREVENTION AT ALL POTABLE POINTS OF CONNECTION;
  - NEW UNDERGROUND PIPING (LABELING, CLEARANCES, BURIAL DEPTH, SLEEVING);
  - INSTALLATION OF SIGNS, TAGS, AND CONTROLLER DETAILS;
  - REQUIRED TEMPORARY CONNECTION TO POTABLE WATER SERVICE; IN MOST CASES, THE SITE'S IRRIGATION SYSTEM MUST BE CONNECTED TO A TEMPORARY SOURCE OF POTABLE WATER IN ORDER TO CONDUCT REQUIRED CROSS-CONNECTION TEST;
  - SITE PASSED REQUIRED CROSS-CONNECTION TEST PERFORMED BY A CERTIFIED ANWA CROSS-CONNECTION SPECIALIST (IF APPLICABLE)
  - NEW WATER INSTALLATION - PRIOR TO RECEIVING RECYCLED WATER, SBWR INSPECTOR MUST INSPECT THE DISCONNECTION OF THE SITE'S IRRIGATION SYSTEM FROM THE TEMPORARY POTABLE WATER SUPPLY, AND THEN INSPECT THE CONNECTION OF THE SYSTEM TO THE RECYCLED WATER METER.
- NO CROSS-CONNECTION BETWEEN THE POTABLE AND RECYCLED WATER SYSTEMS ARE PERMITTED.
- ALL ON-SITE BURIED RECYCLED WATER PIPING SHALL BE IDENTIFIED BY ONE OF THE FOLLOWING METHODS:
  - USING PURPLE-COLORED PVC PIPE WITH CONTINUOUS WORDING: "CAUTION - RECYCLED WATER" PRINTED ON OPPOSITE SIDES OF THE PIPE; PIPE SHALL BE LAID WITH WORDING FACING UPWARDS.
  - WARNING TAPE WITH A MINIMUM WIDTH OF 3 INCHES READING: "CAUTION - RECYCLED WATER" (IN BLACK OR WHITE LETTERING ON PURPLE BACKGROUND) SHALL RUN CONTINUOUSLY ON TOP OF PIPING AND SHALL BE ATTACHED TO PIPING WITH PLASTIC TAPE BANNED AROUND THE WARNING TAPE AND THE PIPE EVERY 5 FEET ON CENTER.
- PVC PIPE: CONSTANT PRESSURE MAINLINE PIPING 1 1/2 INCHES AND SMALLER SHALL BE SCHEDULE 40; CONSTANT PRESSURE MAINLINE PIPING 2 INCHES AND LARGER SHALL BE CLASS 315; INTERMITTENT PRESSURE LATERAL PIPING SHALL BE CLASS 200 OR SCHEDULE 40. COPPER PIPE SHALL BE TYPE "K".
- ALL ON-SITE RECYCLED WATER PIPING SHALL BE BURIED TO A MINIMUM DEPTH FROM FINISH GRADE TO TOP OF PIPE (MINIMUM COVER) OF:
  - PRESSURIZED LINES 3 INCHES AND LARGER - 24 INCHES
  - PRESSURIZED LINES 3 INCHES AND SMALLER - 18 INCHES
  - INTERMITTENT PRESSURE LINES - 12 INCHES
- ALL RECYCLED WATER PIPING OTHER THAN PVC PIPING WITH SOLVENT WELDED JOINTS SHALL BE PROTECTED AGAINST MOVEMENT WITH THRUST BLOCKS OR RESTRAINED JOINTS OR OTHER APPROVED METHOD PER SBWR DETAILS.
- MAINTAIN A 10' - FOOT HORIZONTAL SEPARATION BETWEEN BURIED PRESSURIZED RECYCLED WATER IRRIGATION PIPING AND BURIED POTABLE WATER PIPING UNLESS OTHERWISE NOTED. AT PIPE CROSSINGS, BURIED PRESSURIZED RECYCLED WATER IRRIGATION PIPING MUST BE 12 INCHES BELOW POTABLE WATER LINES. PRESSURIZED RECYCLED WATER PIPELINES ARE ALLOWED OVER POTABLE WATER PIPELINES WITH A MINIMUM OF 12 INCHES VERTICAL SEPARATION IF A FULL STANDARD PIPE LENGTH IS CENTERED OVER THE CROSSING, OR THE RECYCLED WATER PIPELINE IS INSTALLED IN A PIPE SLEEVE WHICH EXTENDS A MINIMUM OF 10 FEET ON EITHER SIDE OF THE POTABLE WATER PIPING. INTERMITTENTLY PRESSURIZED IRRIGATION LATERALS MAY BE LOCATED A MINIMUM OF 12 INCHES ABOVE POTABLE WATER PIPELINES WITHOUT SLEEVING.
- ALL RECYCLED WATER SYSTEM REMOTE CONTROLS, VALVES, ISOLATION VALVES, QUICK COUPLER VALVES, STRAINERS, AND PRESSURE REGULATING VALVES SHALL BE INSTALLED BELOW GRADE IN VALVE BOXES. GREEN, BLACK, OR PURPLE COLORED BOXES AND LIDS ARE ACCEPTABLE. VALVE BOXES SHALL HAVE A WARNING LABEL OR NAMEPLATE PERMANENTLY MOLDED INTO OR ATTACHED ONTO THE LID WITH NUTS, SCREWS, OR BOLTS. WARNING LABELS SHALL BE PER SBWR STANDARD DETAILS.
- RECYCLED WATER QUICK-COUPLED VALVE SHALL HAVE A PURPLE COVER AND BE IDENTIFIED PER SBWR STANDARD DETAILS.
- NO HOSE BIBBS ARE ALLOWED ON THE RECYCLED WATER IRRIGATION SYSTEM. ANY EXTERIOR HOSE BIBBS SERVICED WITH POTABLE WATER MUST BE LABELED PER SBWR STANDARD DETAILS.
- ALL RECYCLED WATER METERS, DEVICES, AND VALVES - EG. ISOLATION VALVES, IRRIGATION CONTROLLERS, REMOTE CONTROL VALVES, PRESSURE REGULATING VALVES, QUICK COUPLER VALVES, ETC. SHALL BE TAGGED PER SBWR STANDARD DETAILS.
- LABEL ALL POTABLE WATER METERS AND ABOVE GROUND POTABLE WATER PIPES/ DEVICES (BACKFLOW PREVENTERS, HOSE BIBBS, ETC) WITH TAGS OR LABELS READING, "POTABLE WATER" IN BLACK LETTERS ON BLUE BACKGROUND, PER SBWR DETAILS.
- ALL RECYCLED WATER IRRIGATION SYSTEMS SHALL HAVE THE FOLLOWING:
  - A WYE STRAINER (WITH A 20 MESH OR FINEER SCREEN) INSTALLED AS CLOSE AS PRACTICABLE TO THE RECYCLED WATER METER BOX.
  - A PRESSURE-REGULATING VALVE INSTALLED IMMEDIATELY DOWNSTREAM OF THE STRAINER (UNLESS OTHERWISE DIRECTED BY SBWR)
  - THESE COMPONENTS SHALL BE INSTALLED WITH ISOLATION VALVES TO FACILITATE MAINTENANCE.
- RECYCLED WATER ADVISORY SIGNS CONFORMING TO THE DETAILS AND SPECIFICATIONS ON THE SBWR - APPROVED IRRIGATION PLANS SHALL BE POSTED PER LOCATIONS SHOWN ON THOSE IRRIGATION PLANS.
- INSTALLATION OF DIRECT INJECTION SYSTEMS ON THE RECYCLED WATER IRRIGATION SYSTEM IS NOT PERMITTED.
- NO DRINKING FOUNTAINS OR EATING AREAS ARE ALLOWED IN THE APPROVED RECYCLED WATER USE AREA UNLESS ADEQUATELY PROTECTED FROM OVERSPRAY.
- ALL RECYCLED WATER METERS WILL BE SET BY THE LOCAL WATER UTILITY AFTER:
  - THE SITE'S OWNER, DEVELOPER, OR CONTRACTOR HAS APPLIED FOR RECYCLED WATER SERVICE WITH THE LOCAL WATER UTILITY, THE WATER SERVICE AGREEMENT HAS BEEN APPROVED (IF APPLICABLE), AND ALL APPLICABLE FEES HAVE BEEN PAID.
  - THE WATER UTILITY HAS RECEIVED AUTHORIZATION FROM SBWR TO SET RECYCLED WATER METERS. SBWR INSPECTOR MUST INSPECT THE DISCONNECTION OF THE SITE'S IRRIGATION SYSTEM FROM THE TEMPORARY POTABLE WATER SUPPLY, AND THEN INSPECT THE CONNECTION OF THE SYSTEM TO THE RECYCLED WATER METER.
- NO OVERSPRAY OR RUNOFF OF RECYCLED WATER IS ALLOWED ON ANY NON-APPROVED USE AREA. PONDING OF RECYCLED WATER DUE TO IRRIGATION IS NOT ALLOWED IN ANY AREA. UPON RECEIVING RECYCLED WATER, THE ON-SITE RECYCLED WATER IRRIGATION SYSTEM MUST PASS A COVERAGE TEST CONDUCTED BY SBWR INSPECTOR.
- CONTRACTOR SHALL SUBMIT AS-BUILT IRRIGATION PLANS TO SBWR WITHIN 90 DAYS OF SITE RECEIVING RECYCLED WATER.

## IRRIGATION LEGEND

SYMBOL	MODEL NUMBER	MFR	DESCRIPTION & NOTES
10/15	POINT OF CONNECTION		AT POINT OF CONNECTION, STATIC WATER PRESSURE IS 45 PSI; MAXIMUM FLOW IS 50 GPM
10/15	WEATHERTRAK ET PRO 3 SMART STATION MANAGER WTHP03-C-48-SBM	HYDROPOINT	48 STATION STAINLESS STEEL TOP-ENTRY PEDESTAL MOUNT ET IRRIGATION CONTROLLER WITH REMOTE READY AND FLOW SENSOR COMPATIBLE (TWO SENSOR INPUT). 5 YEAR WARRANTY. COORDINATE INSTALLATION WITH HYDROPOINT REPRESENTATIVE. VERIFY ET-EVERYWHERE SERVICE RECEPTION AT POINT OF INSTALLATION.
10/15	(1) BACKFLOW PREVENTER		PROTECT (1) REDUCED PRESSURE BACKFLOW PREVENTER TO REMAIN. CONTRACTOR TO VERIFY EXISTENCE & CONDITION. REPLACE AS REQUIRED.
10/15	P22050-28-X-X SERIES RCV (TPV100 FOR FLOWS UNDER 1 GPM)	TORO	REMOTE CONTROL ANGLE VALVE ASSEMBLY W/ OPTIONAL SPIKE GUARD AND BOX. SIZE PER PLAN. TUNE VALVE FOR FLOWS FROM 1-5 GPM. INSTALL PER DETAIL.
10/15	LT-1111-1 LINE SIZE TYP (2" AND SMALLER)	KBI	LO-TORQUE™ SCH 80 PVC THREADED BALL VALVE SHUT-OFF OR ISOLATION VALVE, FOR LINES 2" AND SMALLER
10/15	#1-619-RW-SOH LINE SIZE TYP (2-1/2" AND LARGER)	NIBCO	FLANGED, NON-RISING STEM RESILIENT WEDGE IRON BODY GATE VALVE. SHUT-OFF ISOLATION VALVE, FOR LINES 2 1/2" AND LARGER
10/15	100-2SLVLC LK - PROVIDE (2) TOTAL 100-MHS - PROVIDE (3) TOTAL 100-SLK - PROVIDE (3) TOTAL	TORO	2-PIECE, 1" SINGLE-LUG RECYCLED WATER QUICK COUPLER VALVE W/ LAVENDER LOCKING COVER KEY FOR LOCKING COVER 1" NPT x 1" MHT HOSE SWIVEL SINGLE LUG KEY, 1" TOP PIPE THREAD OUTLET W/INTERNAL 3/4" NPT THREAD
10/15	CARSON #1419 CHRISTY #1-8 (PAVING)	OLD CASTLE	FULL BOX
10/15	(2) MASTER VALVE		PROTECT (2) MASTER VALVE TO REMAIN. CONNECT TO NEW CONTROLLER. CONTRACTOR TO VERIFY EXISTENCE & CONDITION. REPLACE AS REQUIRED.
10/15	(1) FLOW SENSOR		PROTECT (1) FLOW SENSOR TO REMAIN. CONNECT TO NEW CONTROLLER. CONTRACTOR TO VERIFY EXISTENCE & CONDITION. REPLACE AS REQUIRED.
10/15	PVC CLASS 200 SCH 40 UNDER PAVING		NON-PRESSURE LATERAL IRRIGATION PIPE WITH SOLVENT WELD SCHEDULE 40 FITTINGS. 3/4" MIN. PIPE SIZE.
10/15	PVC CLASS 315 SCHEDULE 40 CLASS 200		MAIN PRESSURE IRRIGATION PIPE, SEE SPECS
10/15			SIZE MATERIAL 1"-1 1/2" SCH 40 W/SOLVENT WELD SCH 40 FITTINGS 2"-3" CLASS 315 W SOLVENT WELD SCH 40 FITTINGS 4"- CLASS 200 RUBBER GASKETED (RING-TITE) PIPE W/ RUBBER GASKETED DEEP BELL FITTINGS
10/15	PVC CLASS 315		SLEEVE. SIZE AS REQUIRED
10/15	PVC SCHEDULE 40 ELECTRICAL CONDUIT		ELECTRICAL CONDUIT. SIZE AS REQUIRED. INSTALL PARALLEL TO MAIN LINE UNDER ALL PAVING
10/15	FS		ELECTRICAL CONDUIT. SIZE AS REQUIRED. INSTALL PARALLEL TO MAIN LINE UNDER ALL PAVING.
10/15	EXL		EXISTING IRRIGATION MAINLINE TO BUILDING 5 PLANTING AREA.
10/15	1" 1 1/2" 2" 2 1/2" 3" 4"		LATERAL SIZING
10/15	1 1/2" 2" 2 1/2" 3" 4"		MAINLINE SIZING
10/15	1" 1 1/2" 2" 2 1/2" 3" 4"		STATION NUMBER
10/15	1" 1 1/2" 2" 2 1/2" 3" 4"		VALVE SIZE
10/15	1" 1 1/2" 2" 2 1/2" 3" 4"		GALLONS PER MINUTE

### VALVE BOX SPECIFICATIONS:

- PLASTIC IRRIGATION VALVE BOXES SHALL BE PURPLE COLOR. USE CONCRETE BOXES IN DG & PAVING AREAS AS SHOWN IN THE IRRIGATION DETAILS.
- IRRIGATION VALVE LOCATIONS ARE SHOWN DIAGRAMMATICALLY ON THE IRRIGATION PLANS. SEE PLANTING PLAN FOR INSTALLATION LOCATIONS, AS WELL AS VALVE BOX LAYOUT DETAIL.

### SPRAY AND GENERAL IRRIGATION NOTES:

- INSTALL MAIN LINES, LATERAL LINES AND EQUIPMENT IN PLANTING AREAS AND NOT UNDER PAVEMENT EXCEPT WHERE NECESSARY BETWEEN PLANTERS SEPARATED BY PAVEMENT.
- INSTALL ALL HEADS ADJACENT TO BUILDINGS AT LEAST 18" AWAY FROM THE BUILDING FACE.
- THE CONTRACTOR SHALL VERIFY THAT THERE IS NO OVERSPRAY ONTO BUILDINGS, WALLS, COLUMNS OR FENCES. ADJUST OR RENEGOTIATE AS REQUIRED.
- INSTALL SLEEVES FOR ALL LATERALS UNDER CONCRETE UNLESS SPECIFICALLY NOTED. NO SLEEVES ARE REQUIRED FOR LATERAL LINES UNDER ASPHALTIC CONCRETE, UNLESS OTHERWISE SHOWN ON PLANS.
- IRRIGATION VALVE LOCATIONS ARE SHOWN DIAGRAMMATICALLY ON THE IRRIGATION PLANS. SEE PLANTING PLAN AND VALVE BOX LAYOUT DETAIL FOR INSTALLATION LOCATIONS.

### SUBSURFACE DRIP IRRIGATION NOTES:

- PROVIDE TEMPORARY SUPPLEMENTAL OVERHEAD IRRIGATION TO ESTABLISH PLANTS.
- HOLD DRIPPERLINES 1/4" OFF FACE OF BUILDING, 6" OFF PAVING @ SHRUBS, 2" OFF PAVING @ SOO, TURF & GROUND COVER TYP.
- SPACE DRIPPERLINES EQUALLY WITHIN EACH ZONE UNLESS OTHERWISE NOTED (12" OR 18" O.C. MAX PER LEGEND)
- FOR SLOPES GREATER THAN 10:1, MODIFY DRIPPERLINE SPACING ON THE BOTTOM 1/3 OF THE SLOPE TO BE 25% GREATER AND OMIT THE DRIPPERLINE AT THE BOTTOM OF SLOPE.
- DRIP SYSTEMS WITH 12" & 18" EMITTER SPACING HAVE DIFFERENT PRECIPITATION RATES. INDEPENDENTLY TEST AND SET RUN TIMES FOR THESE TWO CONDITIONS.
- INSTALL WYVES AT HIGH POINTS OF EACH PLANTING AREA. ARROWS ARE SHOWN TO INDICATE INTENT - LOCATE AS REQUIRED BASED ON ACTUAL GRADES OF THE SITE. LOCATE (1) ARROW PER 500' OF DRIPPERLINE TUBING, TYP. SEE DETAIL.

### IRRIGATION SYSTEM MAINTENANCE

- SYSTEM OBSERVATION: THE CONTRACTOR SHALL VISUALLY CHECK ALL SYSTEMS FOR PROPER OPERATION ON A WEEKLY BASIS AND MAKE ALL NECESSARY REPAIRS. ALL EQUIPMENT SHALL BE ADJUSTED AS NECESSARY FOR PROPER COVERAGE AND FUNCTION.
- ESTABLISHMENT IRRIGATION SCHEDULING: FOR THE FIRST 60-DAYS OF PLANTING ESTABLISHMENT AND MAINTENANCE, PROGRAM CONTROLLER IN A PLANTING ESTABLISHMENT IRRIGATION MODE. SCHEDULE IRRIGATION TO ESTABLISH PLANTINGS PER THE SPECIFICATIONS.
- CLIMATOLOGICALLY CONTROLLED IRRIGATION SCHEDULING: FOR THE LAST 80-DAYS OF PLANTING ESTABLISHMENT AND MAINTENANCE, PROGRAM CONTROLLER TO RUN AUTOMATICALLY, SITE-SPECIFIC CLIMATOLOGICALLY ENVIROMENTATION (ET) DATA OPTIMIZE AUTOMATIC SCHEDULE AS REQUIRED FOR PLANT MATERIALS, SUN ASPECT, SOLAR REFLECTANCE & HEAT ISLANDS, LOCALITY, SLOPE, AND THE INSTALLED IRRIGATION SYSTEM. CHECK SYSTEM FOR PROPER FUNCTION AND ADJUST AUTOMATIC SCHEDULE WEEKLY TO PROVIDE OPTIMAL CONDITIONS FOR SUSTAINING HEALTHY PLANTING.
- MAINTENANCE STAFF TRAINING: PERFORM A FULL INSTRUCTION SESSION IN THE PRESENCE OF THE DESIGNATED MAINTENANCE PERSONNEL DEMONSTRATING THE IRRIGATION CONTROLLER SYSTEM, PROGRAM ADJUSTMENT AND OVER-RIDES, SYSTEM TESTING, TROUBLE-SHOOTS, ETC. INCLUDE INSTRUCTIONS ON HOW TO TURN OFF SYSTEM IN CASE OF EMERGENCY.
- REPAIRS: DURING THE PLANT ESTABLISHMENT & MAINTENANCE PERIOD, ALL REPAIRS MADE TO THE IRRIGATION SYSTEM SHALL BE AT THE CONTRACTOR'S EXPENSE. ALL REPAIRS SHALL BE MADE WITHIN TWENTY-FOUR (24) HOURS.
- SYSTEM CERTIFICATION: OBTAIN A LETTER OF CERTIFICATION FROM CONTROLLER MANUFACTURER'S REPRESENTATIVE CERTIFYING THAT THE CONTROLLER IS PROPERLY PROGRAMMED AND IN WORKING ORDER BEFORE THE END OF THE MAINTENANCE PERIOD.

## IRRIGATION LEGEND

SPRAY	USE PURPLE PIPE AND PURPLE EQUIPMENT FOR FUTURE RECYCLED WATER CONNECTION
SANDBOX STREAM SPRAY	
DRIP	

## LEGEND

SYMBOL	DESCRIPTION
---	PROPERTY LINE
---	LANDSCAPE LIMIT OF WORK
(S) CONDITIONS, SEE TOPOGRAPHIC SURVEY	

### OWNER

AMC EAST THIRD AVENUE SUITE 300  
COSTA MESA, CA 92626  
TEL: 949.471.2300

### CONSULTANTS

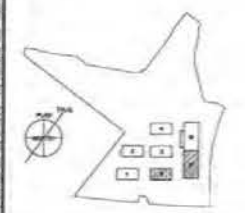
ARCHITECT  
LARRY WILSON  
1000 SOUTH ROSELAND AVENUE, SUITE 200  
LOS ANGELES, CA 90007  
TEL: 213.681.7000

### CIVIL ENGINEERING

WFP ENGINEERS  
100 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.774.7000

### LANDSCAPE ARCHITECT

CARDUCK AND ASSOCIATES  
1000 EAST THIRD AVENUE, SUITE 300  
COSTA MESA, CA 92626  
TEL: 949.471.2300



NO.	REVISION	DATE
1	PC ADJUSTMENT	10/01/15
2	CITY RESPONSE	10/02/15
3	PC ADJUSTMENT	10/13/15
4	PC ADJUSTMENT	11/05/15
5	PC ADJUSTMENT	11/05/15
6	PC ADJUSTMENT	11/11/15



AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

## IRRIGATION LEGEND & NOTES

I HAVE COMPLIED WITH THE CRITERIA OF THE ORDINANCE AND APPLIED THEM ACCORDINGLY FOR THE EFFICIENT USE OF WATER IN THE



OWNER

STEELWAVE  
800 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650.971.2200

CONSULTANTS

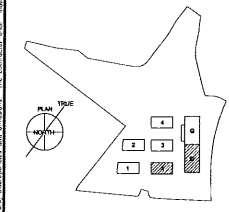
ARCHITECT  
LARGE architecture  
1501 SOUTH ROBERTSON BOULEVARD, 2ND FLOOR  
LOS ANGELES, CA 90035  
TEL: 310.553.1569

CIVIL ENGINEERING

BEF ENGINEERS  
100 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.850.7900

LANDSCAPE ARCHITECT

CARDUCCI AND ASSOCIATES  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415.424.0900



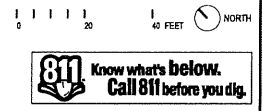
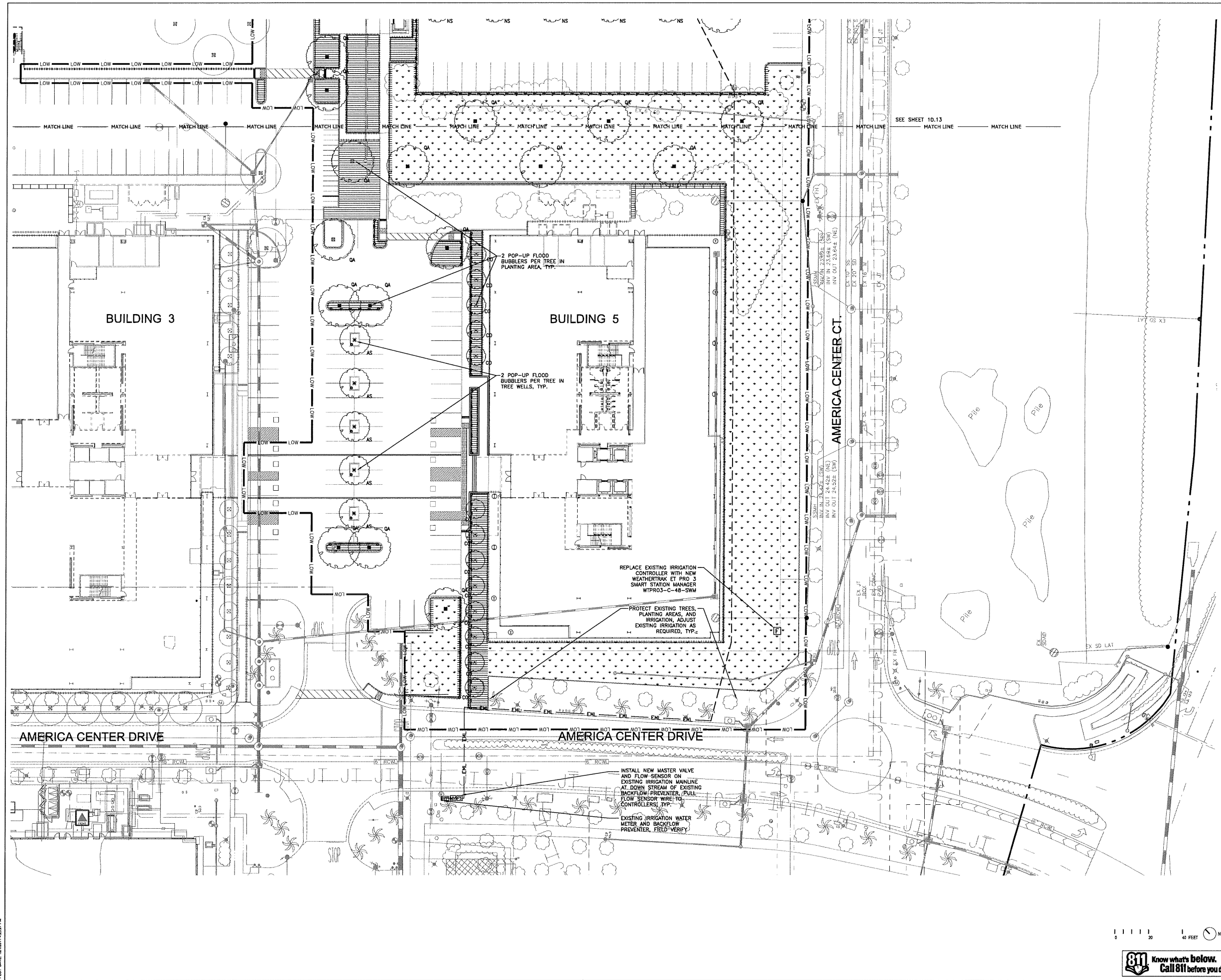
NO.	ISSUE	DATE
1	PD ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/24/12
3	PD ADJUSTMENT	02/13/13
4	PD ADJUSTMENT	11/08/13
5	PD AMENDMENT	04/05/15
6	PD AMENDMENT	09/11/15



AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

IRRIGATION  
PLAN- SOUTH

PROJECT 2015-001	SHEET NUMBER
SCALE	10.12
DRAWN BY	
DATE	09/11/15
PD AMENDMENT	



PD AMENDMENT

NOT DATE: 12/08/14 4:26:27 PM

OWNER

STEELWAVE  
4000 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650.971.2200

CONSULTANTS

ARCHITECT

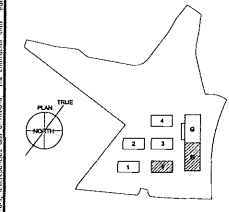
LARGE ARCHITECTURE  
1551 SOUTH ROBERTSON BOULEVARD, 2ND FLOOR  
LOS ANGELES, CA 90035  
TEL: 310.553.1959

CIVIL ENGINEERING

BOF ENGINEERS  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.390.7900

LANDSCAPE ARCHITECT

CARDUCCI AND ASSOCIATES  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415.474.0900



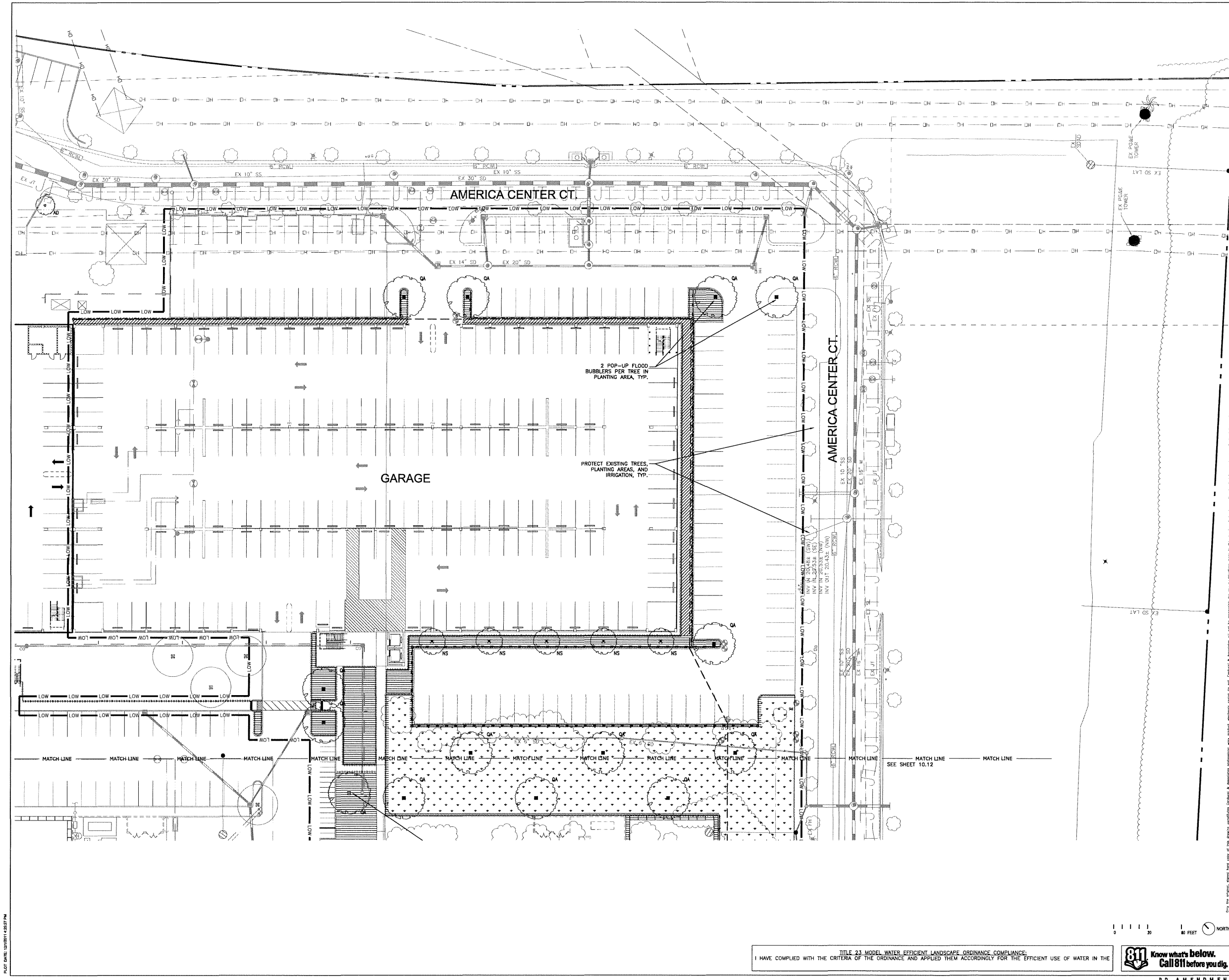
NO.	ISSUE	DATE
1	PD ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/24/12
3	PD ADJUSTMENT	03/13/13
4	PD ADJUSTMENT	11/08/13
5	PD AMENDMENT	04/08/15
6	PD AMENDMENT	09/11/15

STEELWAVE

AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

IRRIGATION PLAN  
- NORTH

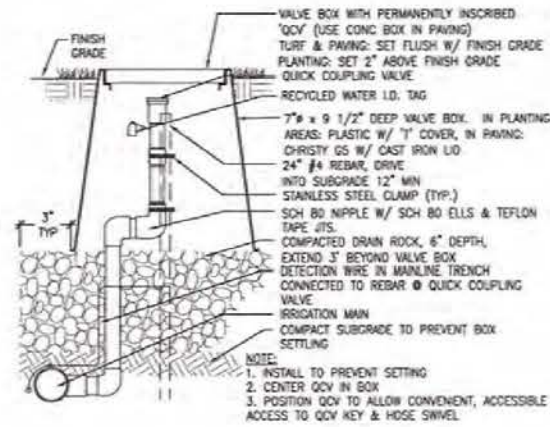
PROJECT  
2015-001  
SCALE  
DRAWN BY  
DATE  
09/11/15  
SHEET NUMBER  
10.13  
PD AMENDMENT



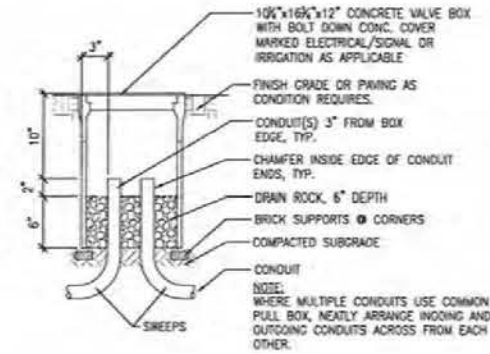
TITLE 23 MODEL WATER EFFICIENT LANDSCAPE ORDINANCE COMPLIANCE:  
I HAVE COMPLIED WITH THE CRITERIA OF THE ORDINANCE AND APPLIED THEM ACCORDINGLY FOR THE EFFICIENT USE OF WATER IN THE

811 Know what's below.  
Call 811 before you dig.

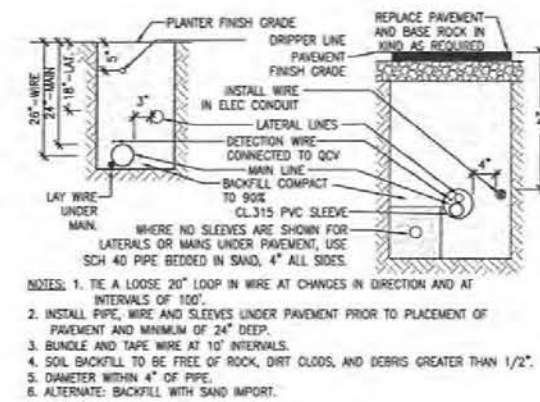




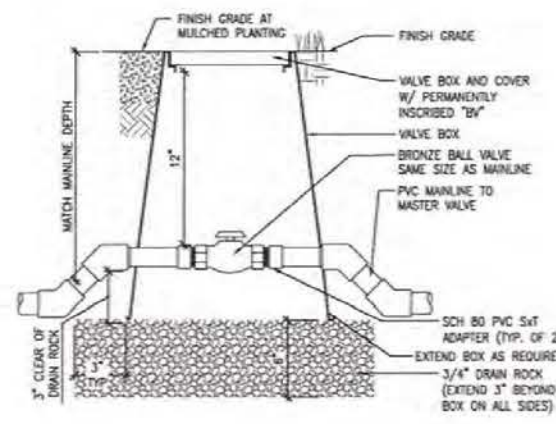
7 QUICK COUPLER VALVE  
SCALE: 3" = 1'



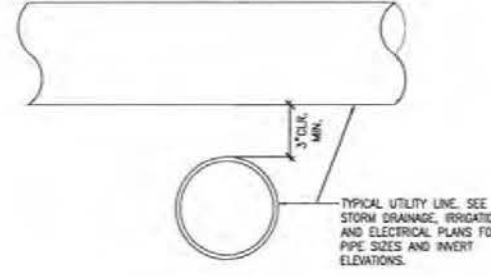
4 PULLBOX  
SCALE: 1 1/2" = 1'



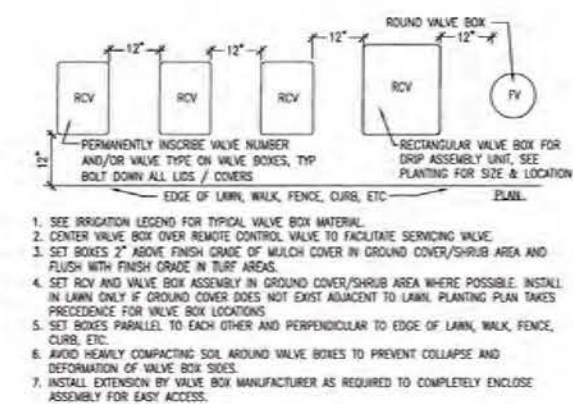
1 TRENCH  
SCALE: 1" = 1'



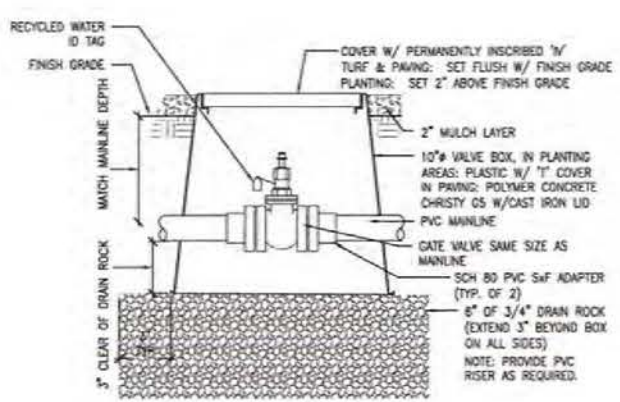
8 BALL VALVE 2 INCH & SMALLER  
SCALE: 3" = 1'



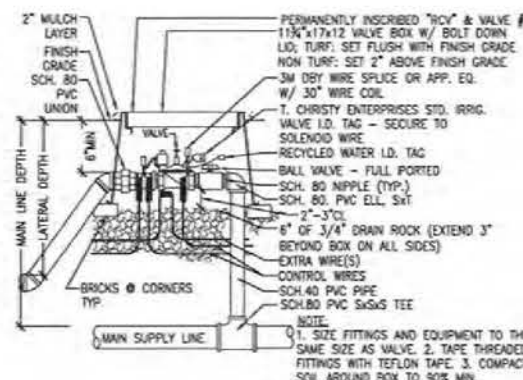
5 TYPICAL UTILITY CROSSING  
SCALE: 3" = 1'



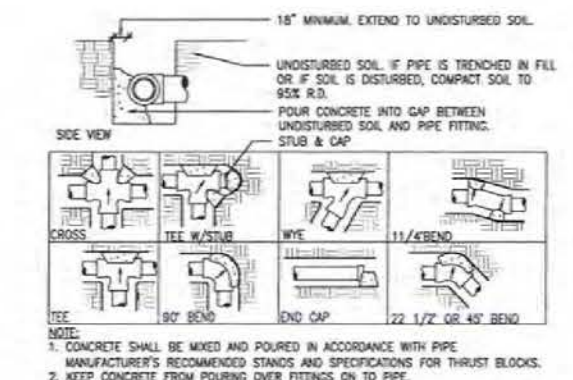
2 VALVE BOX LAYOUT  
SCALE: 3/4" = 1'



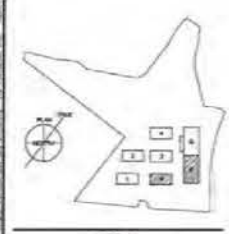
9 BALL VALVE - 2.5 IN AND LARGER  
SCALE: 3" = 1'



6 REMOTE CONTROL VALVE  
SCALE: 1 1/2" = 1'

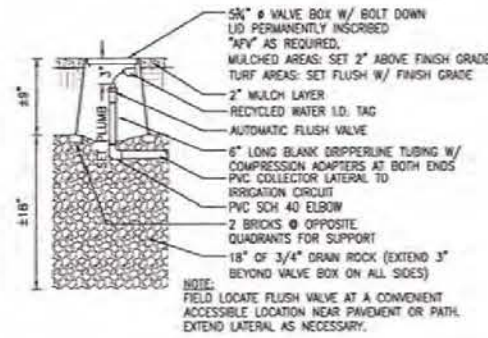


3 THRUST BLOCK  
SCALE: 3" = 1'

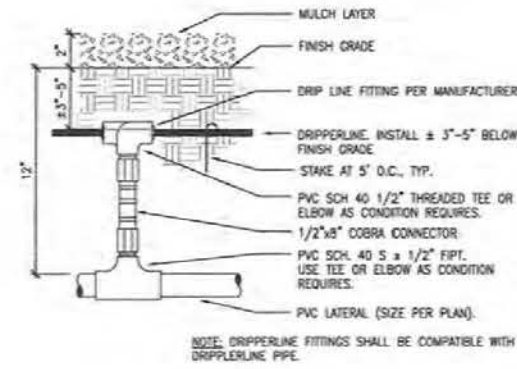


NO.	ISSUE	DATE
1	PO ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/04/12
3	PO ADJUSTMENT	10/19/12
4	PO ADJUSTMENT	11/09/12
5	PO AMENDMENT	04/09/13
6	PO AMENDMENT	08/11/13

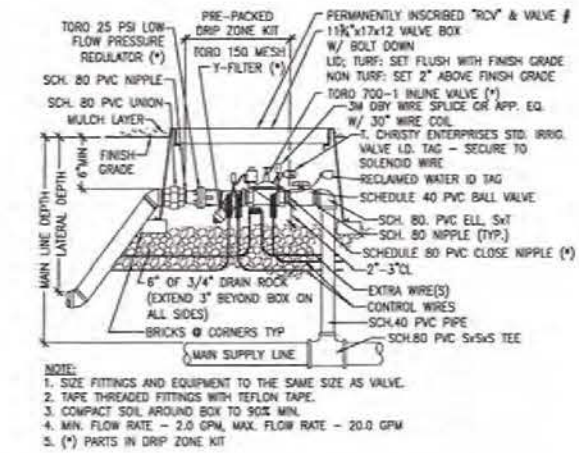




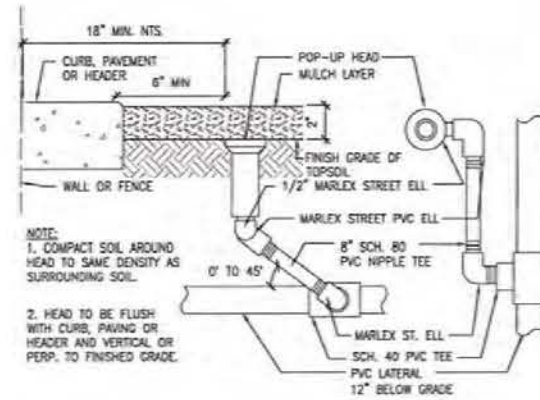
7 AUTOMATIC FLUSH VALVE  
SCALE: 1 1/2" = 1'



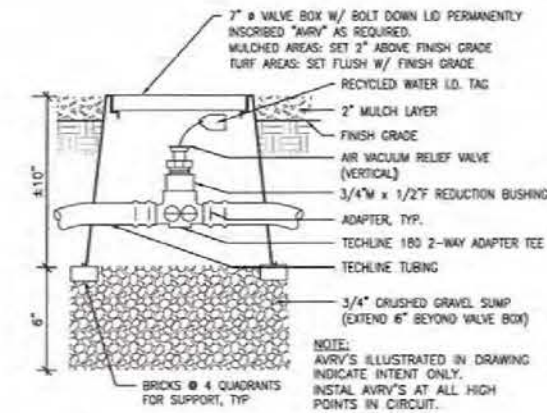
4 PVC TO DRIP  
SCALE: 3\"/>



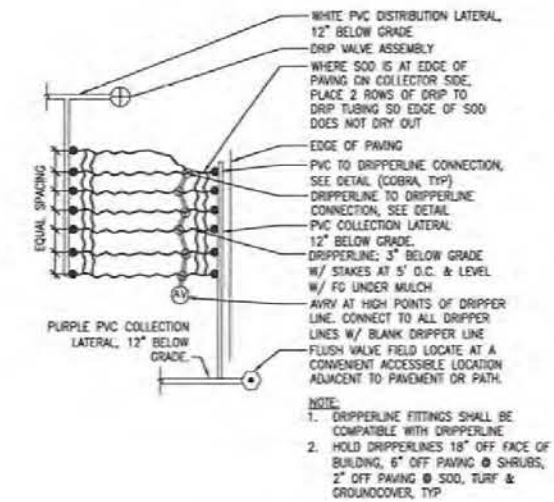
1 DRIP ZONE KIT  
SCALE: 1 1/2" = 1'



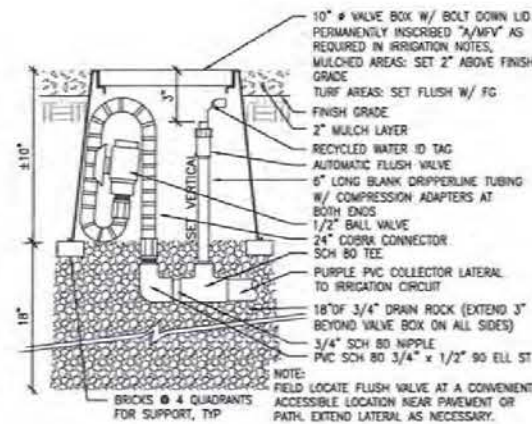
8 POP-UP BUBBLER  
SCALE: 3\"/>



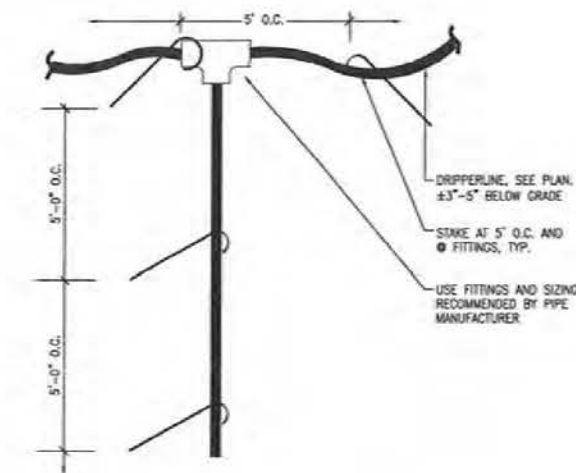
5 AIR VACUUM RELIEF VALVE  
SCALE: 3\"/>



2 DRIP LAYOUT  
SCALE: 3/4\"/>



6 AUTO-MANUAL FLUSH VALVE  
SCALE: 3\"/>



3 DRIP TO DRIP  
SCALE: 1/2\"/>

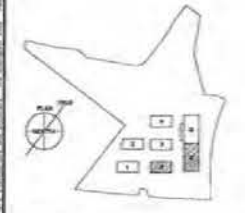
**CARDUCCI & ASSOCIATES, INC.**  
LANDSCAPE ARCHITECTURE PLANNING  
IRRIGATION DESIGN URBAN DESIGN  
1111 11TH STREET, 4TH FLOOR  
SAN FRANCISCO, CALIFORNIA 94103  
415.398.1111 FAX 415.398.1112  
WWW.CARDUCCI.COM

**OWNER**  
STEELWAVE  
4000 WEST HAVEN AVENUE, SUITE 300  
FOSTER CITY, CA 94404  
TEL 650.871.3000

**CONSULTANTS**  
**ARCHITECT**  
LABRE Architecture  
1501 BOUTY AVENUE, SUITE 200, 2ND FLOOR  
LOS ANGELES, CA 90006  
TEL 310.853.1500

**CIVIL ENGINEERING**  
RFP ENGINEERS  
20 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL 415.655.7500

**LANDSCAPE ARCHITECT**  
CARDUCCI & ASSOCIATES  
1111 11TH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94103  
TEL 415.398.1111



LAIR-01

NO.	ISSUE	DATE
1	PD ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/04/12
3	PD ADJUSTMENT	10/15/12
4	PD ADJUSTMENT	11/06/12
5	PD AMENDMENT	04/05/15
6	PD AMENDMENT	08/11/15

**STEELWAVE**

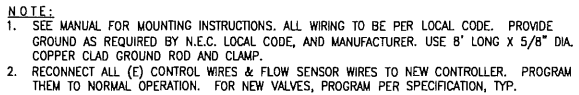
**AMERICA CENTER**  
AMERICA CENTER DRIVE  
SAN JOSE, CA

**IRRIGATION DETAILS**

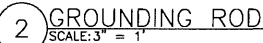
PROJECT  
2015-001  
SCALE  
DRAWN BY  
DATE  
05/11/15

SHEET NUMBER  
**10.15**

PD AMENDMENT

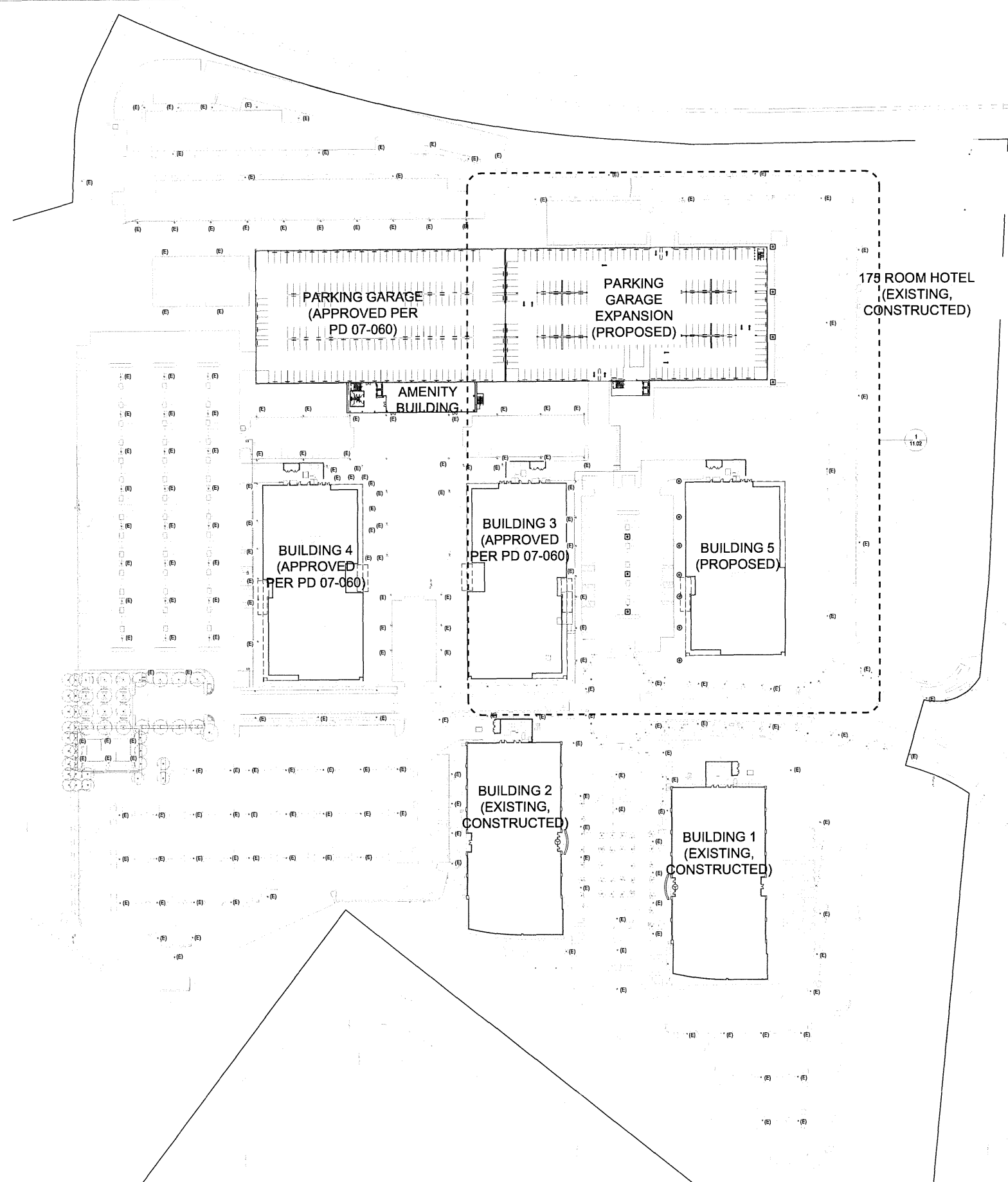


1

[illegible]

AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

## IRRIGATION DETAILS



LIGHTING FIXTURE SYMBOLS	
SYMBOL	DESCRIPTION
	PEDESTRIAN POLE MOUNTED LIGHT FIXTURE ON POLE TOP
	SITE POLE MOUNTED LIGHT FIXTURE ON POLE TOP
	SITE POLE MOUNTED LIGHT FIXTURE WITH ARM
	SITE LOW VOLTAGE LED STRAND LIGHTING
	SITE FLOOD LIGHT FIXTURE
	SITE BOLLARD LIGHT FIXTURE

**[LRG]**

Architecture | Interiors | Planning

1501 SOUTH ROBERTSON BLVD., 2ND FLOOR  
LOS ANGELES, CALIFORNIA 90035  
T: 310.552.1859  
F: 310.552.1950

**OWNER**

**STEELWAVE**  
4000 EAST THIRD AVENUE, SUITE 500  
FOOTER CITY, CA 94340  
TEL: 510.571.2200

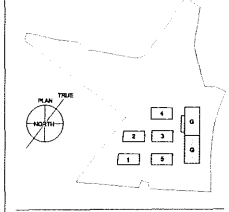
**CONSULTANTS**

**CIVIL ENGINEERING**

**BNF ENGINEERS**  
120 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.398.7900

**LANDSCAPE ARCHITECT**

**CARDUCCI AND ASSOCIATES**  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415.874.9500



NO.	ISSUE	DATE
	PD ADJUSTMENT	10/01/12
1	CITY RESPONSE	10/24/12
2	PD ADJUSTMENT	02/13/13
3	PD ADJUSTMENT	11/09/13
4	PD AMENDMENT	04/09/15
5	PD AMENDMENT	09/11/15
6	PD AMENDMENT RESUBMITTAL	01/29/16



**AMERICA CENTER**  
AMERICA CENTER DRIVE  
SAN JOSE, CA

**OVERALL ELECTRICAL SITE PLAN**

PROJECT: 2015-001  
SCALE: As Indicated  
DRAWN BY:  
DATE: 09/11/15  
SHEET NUMBER: 11.01

PLOT DATE: 10/10/11 1:22:27 PM

OWNER

STEELWAVE  
4100 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650.571.2200

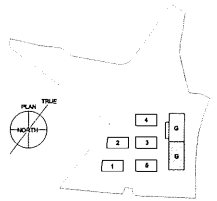
CONSULTANTS

CIVIL ENGINEERING

BKF ENGINEERS  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.530.7900

LANDSCAPE ARCHITECT

CARDUCCI AND ASSOCIATES  
555 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94133  
TEL: 415.674.0990



NO.	ISSUE	DATE
1	PD ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/24/12
3	PD ADJUSTMENT	02/13/13
4	PD ADJUSTMENT	11/08/13
5	PD AMENDMENT	04/08/15
6	PD AMENDMENT	09/11/15
7	PD AMENDMENT RESUBMITTAL	01/28/16

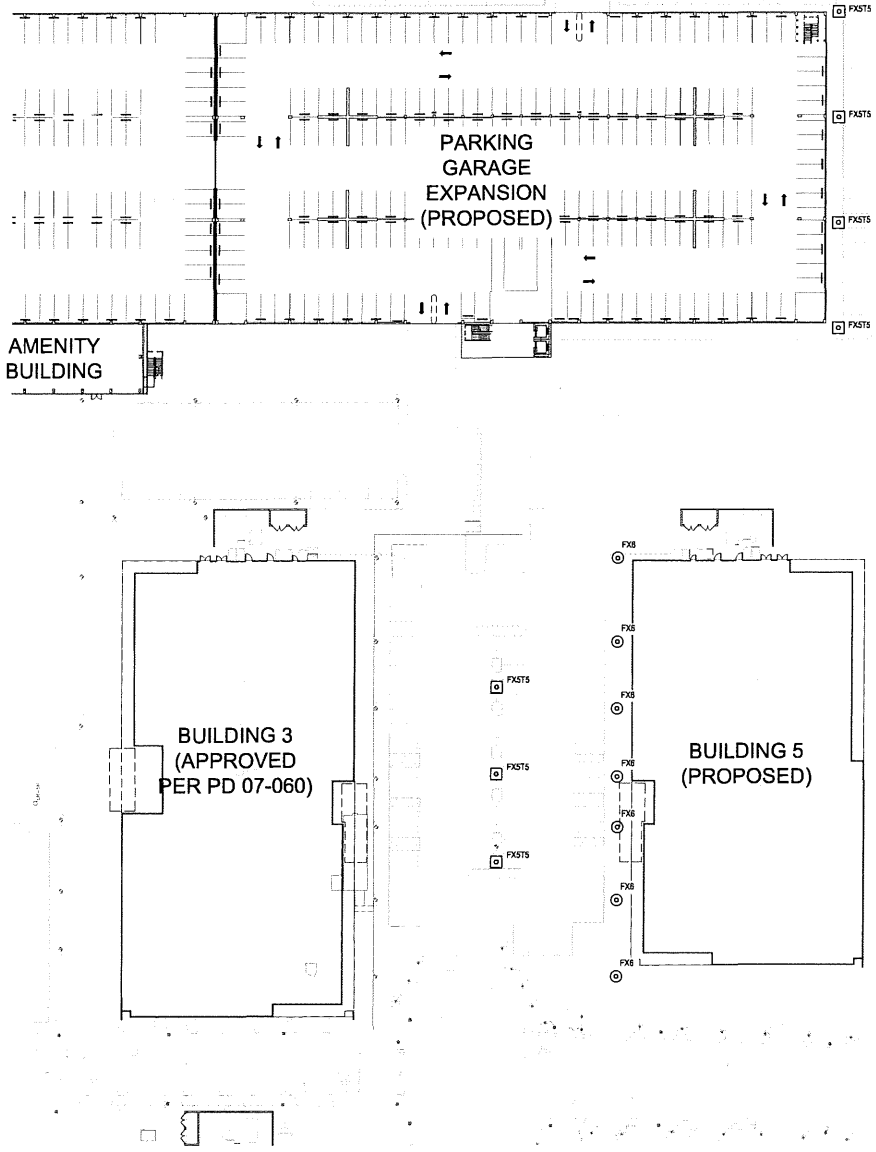


AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

ELECTRICAL SITE  
LIGHTING PLAN

LIGHTING FIXTURE SYMBOLS	
SYMBOL	DESCRIPTION
⊙	PEDESTRIAN POLE MOUNTED LIGHT FIXTURE ON POLE TOP
⊠	SITE POLE MOUNTED LIGHT FIXTURE ON POLE TOP
⊡	SITE POLE MOUNTED LIGHT FIXTURE WITH ARM
⊞	SITE LOW VOLTAGE LED STRAND LIGHTING
⌞	SITE FLOOD LIGHT FIXTURE
⊙	SITE BOLLARD LIGHT FIXTURE

LIGHTING FIXTURE SCHEDULE											
TYPE	DESCRIPTION	LAMPING			LOAD (VA)	VOLT	MOUNTING	REMARKS	MANUFACTURER	CATALOG NUMBER	
		QTY	TYPE	DET							
FX21	CUT-OFF 20" CYLINDRICAL POST TOP MOUNTED LED POLE LIGHT FIXTURE WITH TYPE B DISTRIBUTION AND 3' W/ SHAW-WALK COVER ON 30' ROUND POLE	1	LED	3000K	40W	120V	277	POLE	COORDINATE FINISH WITH ARCHITECT	DAVIDSON	CPFL-ENNA-KA-3-18LA-RW-277-180
FX21A	SAME AS TYPE FX21 EXCEPT WITH TYPE IV DISTRIBUTION	1	LED	3000K	40W	120V	277	POLE	COORDINATE FINISH WITH ARCHITECT	DAVIDSON	CPFL-ENNA-KA-3-18LA-RW-277-180
FX21B	SAME AS TYPE FX21 EXCEPT WITH TYPE V DISTRIBUTION	1	LED	3000K	40W	120V	277	POLE	COORDINATE FINISH WITH ARCHITECT	DAVIDSON	CPFL-ENNA-KA-3-18LA-RW-277-180
FX2	CUT-OFF POST TOP MOUNTED LED POLE LIGHT FIXTURE WITH TYPE V DISTRIBUTION ON 10' ROUND POLE	1	LED	3000K	10W	120V	277	POLE	COORDINATE FINISH WITH ARCHITECT	SELUX	SACL-FS-1-LED100-30-10-SP-277-02
FX4	SAME AS TYPE FX2 EXCEPT WITH AN INTEGRAL RECEPTACLE	1	LED	3000K	40W	120V	277	POLE	COORDINATE FINISH WITH ARCHITECT	SELUX	SACL-FS-1-LED100-30-10-SP-277-02-REC
FX7	PEDESTRIAN WANDER ROUND LED 7" TALL BOLLARD	1	LED	3000K	40W	120V	277	BOLLARD	COORDINATE FINISH WITH ARCHITECT	SELUX	RMFL-3-0-36-M-SP-277
FX8	LANDSCAPE LED FLOOD LIGHT FIXTURE MOUNTED ON 10' FORGED PIPE WITH INTEGRAL 277V/2000W ELECTRONIC NON-DIMMING TRANSFORMER	1	LED	3100K	40W	120V	277	GAZE	COORDINATE FINISH WITH ARCHITECT	B & K LIGHTING	FX8-LED-40W-MFL-AS-700-10-11" TRANSFORMER, JUNCTION BOX SYSTEM PFA-30-17-THRO-40-30-36
FX9	20" X 20" RECTANGULAR FILL CUT-OFF SITE POLE MOUNTED LIGHT FIXTURE WITH ARM AND TYPE IV DISTRIBUTION ON 30' ROUND POLE	1	LED	3000K	40W	120V	277	POLE	COORDINATE FINISH WITH ARCHITECT	MOORE LIGHTING	GLECH-40-LED-61-61A-180
FX10	LOW VOLTAGE LED STRAND WITH WIDE BASE LED LAMP PROTECTED BY GLASS BRACKET PROOF POLYCARBONATE GLASS	1	LED			120V	277	STRAND	COORDINATE FINISH WITH ARCHITECT	TRUJILLO	LBI-SENER



1 ELECTRICAL SITE LIGHTING PLAN  
1" = 40'-0"

## OWNER

STEELWAVE  
4000 EAST THIRD AVENUE, SUITE 500  
FOSTER CITY, CA 94404  
TEL: 650.571.2885

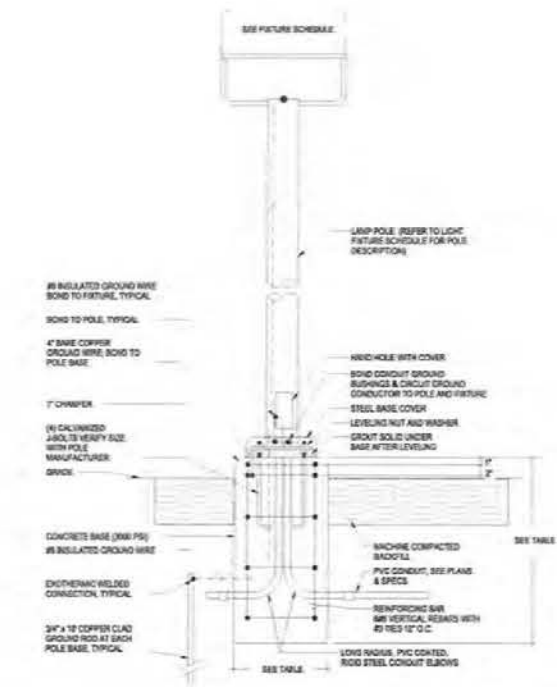
## CONSULTANTS

### CIVIL ENGINEERING

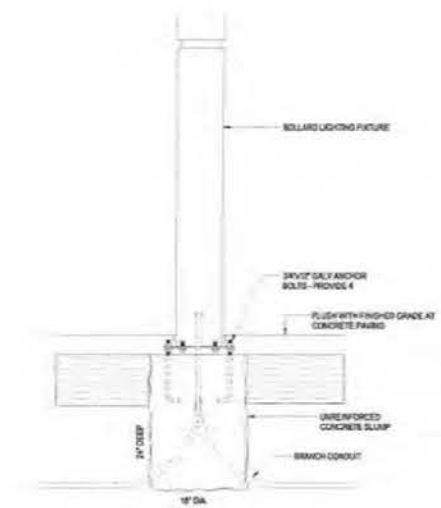
SAF ENGINEERS  
150 CALIFORNIA STREET  
SAN FRANCISCO, CA 94111  
TEL: 415.393.7800

### LANDSCAPE ARCHITECT

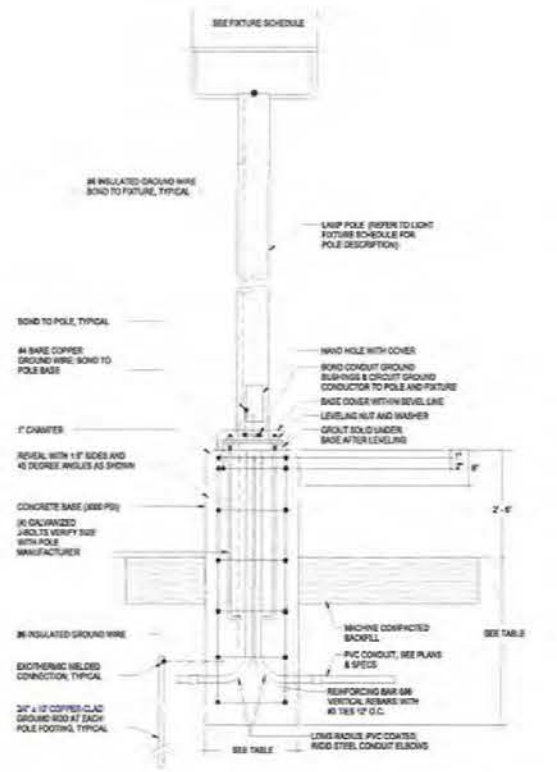
CARDUCHO AND ASSOCIATES  
880 BEACH STREET, 4TH FLOOR  
SAN FRANCISCO, CA 94109  
TEL: 415.674.0800



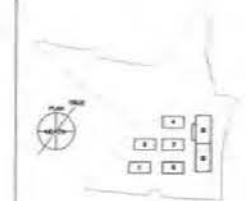
1 OUTDOOR NON-TRAFFIC AREA POLE BASE INSTALLATION  
NTS



3 LIGHTING BOLLARD BASE DETAIL  
NTS



2 OUTDOOR ABOVE GRADE OR PAVING POLE BASE INSTALLATION  
NTS



NO.	ISSUE	DATE
1	PO ADJUSTMENT	10/01/12
2	CITY RESPONSE	10/24/12
3	PO ADJUSTMENT	02/13/13
4	PO ADJUSTMENT	11/05/13
5	PO AMENDMENT	04/09/14
6	PO AMENDMENT	09/11/15
7	PO AMENDMENT RESUBMITTAL	10/15/15

STEELWAVE

AMERICA CENTER  
AMERICA CENTER DRIVE  
SAN JOSE, CA

ELECTRICAL  
DETAILS