



Memorandum

TO: HONORABLE MAYOR AND
CITY COUNCIL

FROM: Rosalynn Hughey

SUBJECT: SEE BELOW

DATE: March 14, 2018

Approved

D. D. Syl

Date

3/14/18

COUNCIL DISTRICT: 6

SUBJECT: PDC17-019. A PLANNED DEVELOPMENT REZONING FROM THE A(PD) PLANNED DEVELOPMENT TO THE R-M(PD) PLANNED DEVELOPMENT ZONING DISTRICT TO ALLOW UP TO 206 DWELLING UNITS LOCATED IN A ONE SIX-STORY MULTI-FAMILY RESIDENTIAL BUILDING AND A ONE FIVE-STORY MULTI-FAMILY RESIDENTIAL BUILDING ON A 2.30-GROSS ACRE SITE (237-253 RACE STREET & 216-280 GRAND AVENUE).

RECOMMENDATION

The Planning Commission voted 6-0-1 (Commissioner Ballard absent) to recommend that the City Council (a) adopt a resolution adopting Race Street and Grand Avenue Residential Development Project Mitigated Negative Declaration, for which an Initial Study was prepared, all in accordance with the California Environmental Quality Act, as amended (CEQA), and adopting a related Mitigation Monitoring and Reporting Program; and (b) adopt a Rezoning Ordinance as described in the attached staff report.

OUTCOME

Should the City Council adopt the Mitigated Negative Declaration and related Mitigation Monitoring and Reporting Program and Rezoning Ordinance, the applicant will be able to submit a Planned Development Permit application to allow the construction of up to 206 residential units consistent with the proposed Development Standards and the General Plan Land Use/Transportation Diagram designation of Urban Residential.

BACKGROUND

On February 28, 2018, the Planning Commission held a Public Hearing to consider the Mitigated Negative Declaration (MND) and related Mitigation Monitoring and Reporting Program (MMRP) and proposed Rezoning. The Planning Commission recommended adoption of the MND/MMRP and proposed Rezoning.

The item was heard at Public Hearing because staff had received several inquiries regarding the proposed project. During staff's presentation, staff summarized how the proposed project is consistent with the General Plan Land Use/Transportation Diagram designation of Urban Residential, and that the project will be developed in an urban form, which will provide a transition to the lower density designated along Park Avenue. Staff clarified that the project applicant, The Core Companies, is in contract with the Santa Clara Housing Authority. Upon completion of the Rezoning process, the Santa Clara Housing Authority will take over the site, and will design and build an affordable housing project with 116 multi-family units and 90 senior housing units. The project applicant, Vince Cantore with the Core Companies, provided an overview of the Core Companies projects and development of the site. Flaherty Ward, with the Santa Clara County Housing Authority (SCCHA), provided an overview of the planned affordable housing project for the subject site. She also indicated that the Santa Clara County Housing Authority is still in the conceptual design phase of the project and anticipates a future project submittal.

Public Testimony

Public testimony included five members of the public. Four members of the public spoke in support of the proposed project, noting the need for affordable and senior housing in the area. One member of the public, who is currently a tenant in one of the residential buildings on Grand Avenue, received notice of the potential to demolish her unit and inquired about her rights and the status of the demolition of the structures. Ms. Ward clarified that the Housing Authority is subject to Federal and State regulations regarding relocation assistance, and that they have set aside \$1.3 million to cover the costs for the ten family units and four commercial spaces. Relocation assistance would be handled through a third party company that will determine the actual relocation benefit, which includes moving costs, relocation assistance and rental differentials. SCCHA has been in contact with the Housing Department and will be in further contact once a formal Planned Development Permit application is submitted to the City. The applicant's representative, Erik Schoennauer, provided closing comments regarding the infeasibility of commercial space on the ground floor, the distance of the residents that signed the opposition petition, and the benefits of the proposed rezoning.

Staff and Planning Commission Discussion

Chair Pham inquired about the length of the wait list for the County's Section 8 Vouchers. Ms. Ward stated that there are currently 6,000 persons on the wait list and that they are working on other ways to streamline the process. Chair Pham also inquired what mix of Section 8 Vouchers

would be at the site. Ms. Ward noted that most likely they will not need to use Section 8 Vouchers since affordability can be maintained through tax credits and other funding sources.

Commissioner Vora inquired about the Letter of Intent (LOI) with The Core Company and when they plan to submit the Planned Development Permit. Ms. Ward confirmed that they have a LOI with the Core Companies, and the plans for development are in the conceptual phase and should be submitted in the next six to 12 months.

Commissioner Allen asked if the recent tax changes have affected the project funding. Ms. Ward noted that the existing low income tax credit programs are still in place. However, the tax rate for corporations was lowered, which in turn has resulted in lower anticipated returns on these projects. SCCHA is in a unique position, in that it has funding in place to fund the difference. Commissioner Allen also inquired about the feasibility of ground floor commercial at the site. Ms. Ward responded that providing ground floor commercial is not their specialty and they will work with staff to come up with solutions to activate the ground floor.

Chair Pham inquired if SCCHA would be using City funds. Ms. Ward clarified that currently there is no City funding for the project and they were not anticipating any need for City funding in the future.

Commissioner Allen made a motion, seconded by Commissioner Yesney, to recommend that the City Council adopt a resolution adopting the Mitigated Negative Declaration and related Mitigation Monitoring and Reporting Program and rezoning Ordinance (6-0-1, Commissioner Ballard Absent). Commissioner Allen acknowledged the need for affordable housing in the area and this will help to close that gap. Commissioner Yesney and Bit-Badel noted that although at this point ground floor commercial may not appear to be needed, as the area redevelops and intensifies, commercial space may be in demand.

ANALYSIS

A complete analysis of the issues regarding this project are contained in the attached Planning Commission Staff Report.

EVALUATION AND FOLLOW-UP

If the Rezoning is approved, the applicant will be able to move forward with submitting a Planned Development Permit application for review by various City departments.

PUBLIC OUTREACH

Staff followed Council Policy 6-30: Public Outreach Policy, in that notices for the public hearings for the project were mailed to the owners and tenants of all properties located within 1,000 feet of the project site. An electronic version of this memorandum has been available online, accessible

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from the City Council Agenda for the March 27, 2018 hearing. Staff has been available to discuss the proposal with members of the public.

Staff received one additional email in support of the project after the Planning Commission meeting ended. The email is attached to this memo for reference.

COORDINATION

Preparation of this memorandum was coordinated with the City Attorney's Office.

CEQA

An Initial Study/Environmental Assessment Determination and Compliance Findings for HUD-Assisted Projects, resulting in a CEQA Mitigated Negative Declaration (MND) and a National Environmental Protection Act (NEPA) Finding of No Significant Impact (FONSI) were prepared by the Director of Planning, Building, and Code Enforcement for the subject Planned Development Rezoning. The environmental review evaluated two development scenarios including: 1) a multi-family residential development of 206 units, and 2) an affordable housing development with 116 multi-family dwelling units and 90 senior dwelling units (total of 206 units). Additionally, the combined environmental document included the analysis of approximately 8,500 square feet of ground floor commercial and site access from Race Street and Grand Street. The environmental documents were circulated for public review from January 12, 2018 to February 1, 2018. Some of the concerns that were highlighted in the comment letters include: height, density, traffic, parking, and compatibility with the neighborhood. The environmental document was circulated separately for NEPA from January 19 to February 5, 2018. One public comment was received specifically for the EA. This comment expressed concern regarding traffic congestion, air pollution, parking, lack of services in the area, and neighborhood safety and maintenance.

The final MND/FONSI states that the proposed project will not have a significant effect on the environment if certain mitigations are incorporated into the project. The primary environmental issues addressed in the final Initial Study include potential impacts on the physical development of the site on: air quality, biological resources, cultural resources, hazards and hazardous materials, and noise. The MND/FONSI includes mitigation measures that would reduce any potentially significant project impacts to a less-than-significant level. In addition to mitigation measures, other significant environmental permit conditions would be included in the future Planned Development Permit as conditions of approval.

Compliance with NEPA is independent of CEQA. The minimum requirement under 24 CFR part 58 is that certification of the FONSI shall be executed by the Certifying Officer, as determined by the Responsible Agency [§ 58.71]. The Certifying Officer of the City of San José is the Director of Planning, Building and Code Enforcement, as approved by the City Council Resolution No. 70491. U.S. Department of Housing and Urban Development only requires the

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Certifying Officer to execute the request when the Request for Release of Funds (RROF) is made available to the applicant.

Under both CEQA and NEPA requirements, the project is found to trigger no significant impacts with the incorporation of mitigation measures and other environmental conditions. Therefore, by approving the MND, the City will determine that the project would be in compliance with the City's environmental standards and laws. No formal adoption is needed by City Council regarding the adopting of the EA/FONSI.

The entire IS/MND, EA, Response to Comments (to both CEQA and NEPA), and other related environmental documents are available on the Planning web site at:

<http://www.sanjoseca.gov/index.aspx?NID=5935>.

/s/

ROSALYNN HUGHEY, SECRETARY
Planning Commission

For questions, please contact Planning Official, Steve McHarris, at (408) 535-7819.

Attachment: Email in support of the proposed project (Dated February 28, 2017, 8:37 pm)
Planning Commission Staff Report



PLANNING COMMISSION STAFF REPORT

File No.	PDC17-019
Applicant	Race Street Investments, LLC
Location	Between Race Street and Grand Avenue 300 feet southerly of Park Avenue (237-253 Race Street & 216-280 Grand Avenue)
Existing Zoning	A(PD) Planned Development
Proposed Zoning	R-M(PD) Planned Development
Council District	6
Historic Resource	None
Annexation Date	November 30, 2009
CEQA	Initial Study/Mitigated Negative Declaration and Environmental Assessment for the Race and Grand Residential Project

APPLICATION SUMMARY:

File No. PDC16-045: Planned Development Rezoning from the A(PD) Planned Development to the R-M(PD) Planned Development Zoning District to allow up to 206 dwelling units located in one 6-story multi-family residential building and one 5-story multi-family residential building.

RECOMMENDATION:

Planning staff recommends that the Planning Commission recommend to the City Council:

1. Adopt a resolution adopting the Race and Grand Residential Project Mitigated Negative Declaration, for which an initial study was prepared, in accordance with the California Environmental Quality Act, as amended, (CEQA) and adopt a related Mitigation Monitoring and Reporting Program;
2. Consideration of an ordinance rezoning an approximately 2.3 gross acre site located between Race Street and Grand Avenue, 300 feet southerly of Park Avenue (237-253 Race Street & 216-280 Grand Avenue) from the A(PD) Planned Development Zoning District to the R-M(PD) Planned Development Zoning District.

PROJECT DATA

GENERAL PLAN CONSISTENCY	
General Plan Designation	Urban Residential <input checked="" type="checkbox"/> Consistent <input type="checkbox"/> Inconsistent
Consistent Policies	LU-1.2, LU-9.1, LU-9.2, LU-9.5, LU-9.6, LU-9.13. IP-8.5

SURROUNDING USES			
	General Plan Land Use	Zoning District	Existing Use
North	Mixed Use Neighborhood	R-1-8 Single Family CP Commercial Pedestrian	Residential
South	Urban Village	CP Commercial Pedestrian	Retail and Personal Service
East	Mixed Use Neighborhood	CP Commercial Pedestrian	Retail, Personal Service, Residential and Restaurant
West	Neighborhood Community Commercial and Urban Village	CP Commercial Pedestrian and CN Commercial Neighborhood	Commercial, Industrial and Residential

PROJECT DESCRIPTION

On May 2, 2017, a Planned Development Rezoning application (File No. PDC17-019) was filed to rezone the project site from the A(PD) Planned Development Zoning District to R-M(PD) Planned Development Zoning District to allow up to 206 units.

The site is comprised of several parcels totaling approximately 2.3 gross acres (see Figure 1). The project site is occupied with a mix of residential and commercial uses. The proposed units would be spread over two buildings one at six stories and one at five stories. Access to the site will be off of Grand Avenue. The project applicant is in discussions with an affordable housing provider to construct an affordable housing development including multi-family housing and senior housing in two different buildings. The future project would be using Federal funds to finance the development. The formal plans have not been developed, and a formal Planned Development Permit application would be submitted once the development design has been finalized.



Figure 1: Aerial of Subject Site

Previous Approvals

A rezoning of the subject site was previously approved in April 2013 (File No. PDC11-005), which allowed a mixed use development with 13,080 square feet of commercial space and up to 70 multi-family attached residential units at a net density of 30 dwelling units per acre (DU/AC). The property owner subsequently applied for a General Plan Amendment of the Land Use/Transportation Diagram (File No. GP13-005) to change the General Plan designation from Mixed Use Neighborhood to Urban Residential, which was approved by the City Council on November 19, 2013. Following the General Plan Land Use/Transportation Diagram change, the property owner sought approval of a new rezoning application (File No. PDC13-037) to allow up to 80 multi-family attached residential units and up to 12,000 square feet of commercial space, which was approved by the City Council on December 17, 2013. Neither of the PD Zonings were effectuated.

ANALYSIS

The proposed Planned Development Rezoning was analyzed with respect to conformance with: 1) the Envision 2040 General Plan; 2) the Zoning Ordinance; and 3) California Environmental Quality Act (CEQA) / National Environmental Protection Act (NEPA).

Envision San José 2040 General Plan Conformance

The Envision San José 2040 General Plan Land Use/Transportation Diagram land use designation for the subject site is Urban Residential, which allows 30-95 DU/AC and a Floor Area Ratio (FAR) of 1.0 - 4.0 (see Figure 2). This is the primary designation for new medium-density residential development and a broad range of commercial uses. This land use designation is intended for Urban Villages, growth areas or on a limited basis infill development within areas with characteristics similar to the Urban Village areas (generally developed at high-density and in proximity to transit, jobs, amenities and other services).

The proposed project is consistent with the Urban Residential General Plan land use designation, in that it provides residential uses to support the adjacent commercial uses along San Carlos Street and slightly further on The Alameda. Development Standards for the Planned Development Zoning include treatment to the ground floor along Race Street to provide active space either through the addition of ground floor commercial or through the addition of ground floor units with stoops.



Figure 2: General Plan Map of Project Site and Surroundings

The proposed Planned Development Rezoning and Planned Development Permit is consistent with the following General Plan policies:

General Land Use

- **LU-1.2** Encourage Walking. Create safe, attractive, and accessible pedestrian connections between developments and to adjacent public streets to minimize vehicular miles traveled.

Analysis: The subject site is located immediately adjacent to West San Carlos Street, which is a designated Urban Village area anticipated to be finalized in Spring 2018. Race Street has a mix of commercial and residential uses between Park Avenue and West San Carlos Street. Redevelopment of the site will enhance the pedestrian realm and place higher density residential uses in close proximity to basic services such as food sales, coffee and drug store. The close proximity of these amenities will allow future residents the choice to walk rather than drive.

High-Quality Living Environments

- **LU-9.1** Create a pedestrian-friendly environment by connecting new residential development with safe, convenient, accessible, and pleasant pedestrian facilities. Provide such connections between new development, its adjoining neighborhood, transit access points, schools, parks, and nearby commercial areas. Consistent with Transportation Policy

TR-2.11, prohibit the development of new cul-de-sacs, unless it is the only feasible means of providing access to a property or properties, or gated communities, that do not provide through- and publicly-accessible bicycle and pedestrian connections.

- **LU-9.2** Facilitate the development of complete neighborhoods by allowing appropriate commercial uses within or adjacent to residential and mixed-use neighborhoods.
- **LU-9.3** Integrate housing development with our City's transportation system, including transit, roads, and bicycle and pedestrian facilities.
- **LU-9.5** Require that new residential development be designed to protect residents from potential conflicts with adjacent land uses.
- **LU-9.6** Require residential developments to include adequate open spaces in either private or common areas to partially provide for residents' open space and recreation needs.
- **LU-9.13** Equitably distribute residential social service programs (e.g., board and care facilities) throughout the City, especially in areas with access to transit, rather than concentrating them in a few areas. Encourage the County and other social service licensing agencies to recognize and implement this policy.

Analysis: The proposed rezoning is consistent with the High-Quality Living Environments Policies by identifying a site which can accommodate increased density in an area that is adjacent to main transit lines on West San Carlos Street, in addition to being within walking distance to nearby amenities. As noted above, future redevelopment of the site will result in improvements to the pedestrian realm and enhancing the connection between the commercial corridor and the adjacent single-family neighborhood. The rezoning would allow residential development that would further buffer the single-family neighborhood from the nearby commercial uses on West San Carlos. Open space will be provided on-site through common open space and/or private balconies, consistent with the Residential Design Guidelines. Additionally, it is anticipated that the Santa Clara County Housing Authority will be developing the site to include affordable multi-family units and senior housing units.

Zoning

- **IP-8.5:** Use the Planned Development zoning process to tailor such regulations as allowed uses, site intensities and development standards to a particular site for which, because of unique circumstances, a Planned Development zoning process will better conform to Envision General Plan goals and policies than may be practical through implementation of a conventional Zoning District. These development standards and other site design issues implement the design standards set forth in the Envision General Plan and design guidelines adopted by the City Council. The second phase of this process, the Planned Development Permit, is a combined site/architectural permit and conditional use permit, which implement the approved Planned Development zoning on the property.

Analysis: The site is currently in the A(PD) Planned Development Zoning District, which allowed ground-floor commercial space and 80 attached residential units in one building. The conforming Zoning District current proposal of higher density residential uses, the available existing Zoning Districts cannot accommodate the density identified in the General Plan without the use of a Planned Development Zoning District. Although previous rezoning's included A(PD), staff is recommending that this Planned Development zoning use Residential Multi-Family (R-M), which is consistent with the General Plan Land Use/Transportation Diagram designation.

Zoning Ordinance Conformance

The site is located in the A(PD) Zoning District (see Figure 3). The current Planned Development Zoning District allows up to 10,000 square feet of commercial space and up to 80 attached dwelling units. The proposed Planned Development would exceed the allowed number of dwelling units, with 206 units, in addition to changes in allowable height and setbacks. The Planned Development designation of R-M(PD) will allow for modification to the number of dwelling units, required setbacks, parking and building height.

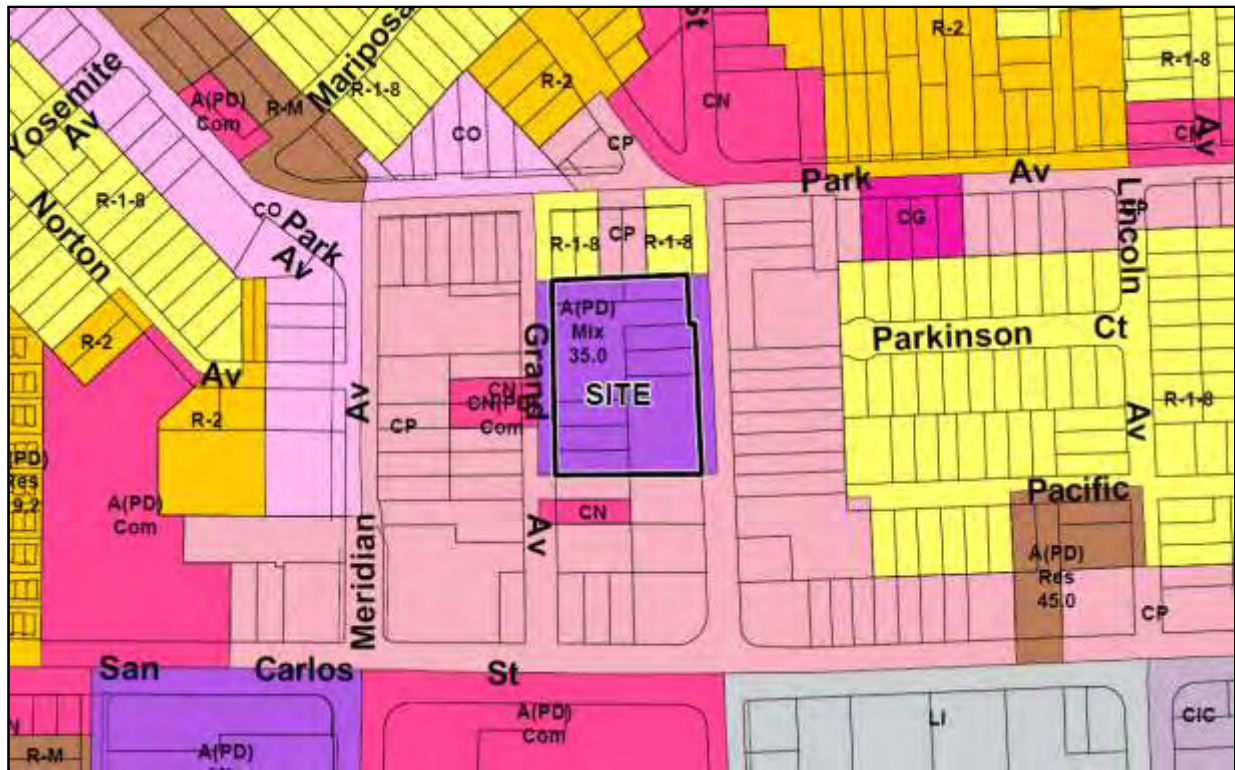


Figure 3: Zoning Designation Map

Pursuant to Table 20-270 in Section 20.120.110 in the San Jose Municipal Code, the conforming zoning district to the General Plan designation of Urban Residential is R-M. General Plan Implementation Policy IP-8.5, allows the Planned Development Rezoning process to be utilized if the Planned Development Rezoning process will better conform to the General Plan goals and policies than a conventional zoning district. It is appropriate for a Planned Development Zoning District to be utilized based on the General Plan Land Use designation of Urban Residential and the limitations of existing available Zoning Districts.

	R-M Zoning District	R-M(PD) Zoning District
Front setback (Race St) ¹	10 feet minimum	4 feet minimum
Right Side setback (North) ¹	5 feet minimum	14 feet minimum
Left Side setback (South) ¹	5 feet minimum	16 feet minimum
Rear setback (Grand St) ¹	15 feet minimum	12 feet minimum
Building A & B Separation	N/A	40 feet minimum
Maximum height	45 feet	80 feet

Parking (Parcel A ²)	Code Requirement	PD Parking Requirement
Studios	1.25 per unit	.57 per unit
1 bedroom	1.25 per unit	.57 per unit
2 bedroom	1.7 per unit	1.7 per unit
Parking (Parcel B ³)	Code Requirement	PD Parking Requirement
Studios	1.25 per unit	1.25 per unit
1 bedroom	1.25 per unit	1.25 per unit
2 bedroom	1.7 per unit	1.7 per unit
3 bedroom	2.0 per unit	2.0 per unit
1. Architectural projections up to 2 feet may be allowed. 2. Parcel A – Senior Housing Development. 3. Parcel B – Multi-Family Housing Development.		

Although the Planned Development does not comply with the required setback for Race Street and Grand Street and building height of the base R-M Zoning, it meets the intent of the General Plan designation of Urban Residential by providing denser development that defines the public realm through building placement and architecture. The proposed parking for the Planned Development includes a slight reduction for Parcel A for studios and one bedroom units since this building is intended to be for senior housing. The remaining two bedroom units for Parcel A and all parking for Parcel B will be code compliant. Additional Development Standards pertaining to active ground floor space on Race Street (including potential commercial space), architecture, site access and other items have been included in the Draft Development Standards (see Attachment A).

Staff requested that the applicant's traffic consultant prepare a shared parking analysis to evaluate the intended future residential uses and the feasibility of shared parking with ground floor commercial space (see Attachment B). The study evaluated the amount of parking required by the Zoning Code and found that the development requires 298 residential and 22 commercial parking spaces. Due to the site proximity to the BRT line on West San Carlos Street, the development is able to apply a 20% parking reduction resulting in a total demand of 238 residential and 18 commercial spaces. The anticipated development would be proposing a total of 242 parking spaces, which would be short 14 spaces for the commercial uses, based on the Zoning Code requirements.

The shared parking analysis evaluates the compatibility of uses on a site and generally, residential and commercial uses can be complimentary since the peak parking demands are often at different times of the day. The study evaluated the proposed parking based on the Urban Land Institute (ULI) Shared Parking study. Based on the ULI data the peak demand hours are between 7:00 and 8:00 p.m. and the total demand of 248 spaces (233 residential parking spaces and 15 commercial spaces). The study further takes into consideration the existing on-street parking and that the development will remove numerous existing curb cuts, which will provide an additional 3 spaces on Race Street. The future project is anticipated to include Senior Housing which has a much lower parking demand than multi-family parking. Taking all these factors into consideration, it is anticipated that the site can accommodate up to 8,500 square feet of commercial space with the proposed 242 parking spaces.

Lastly, should the Planned Development Rezoning not be effectuated with a Planned Development Permit, the base zoning district would be R-M Multiple Residence Zoning District. This would allow the conventional standards of the R-M Multiple Residence Zoning District to be utilized.

CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA)

An Initial Study (IS) and Mitigated Negative Declaration (MND) were prepared by the Director of Planning, Building, and Code Enforcement for the subject Planned Development Rezoning. The environmental review evaluated two development scenarios including: 1) a multi-family residential development of 206 units and 2) an affordable housing development with 116 multi-family dwelling units and 90 senior dwelling units (total of 206 units). Additionally, the IS/MND included the analysis of approximately 8,500 square feet of ground floor commercial and site access from Race Street and Grand Street. The documents were circulated for public review between January 12, 2018 to February 1, 2018 and comments were received from the public. Some of the concerns that was highlighted in the comment letters include: height, density, traffic, parking, and compatibility with the neighborhood.

The final IS/MND states that the proposed project will not have a significant effect on the environment. The primary environmental issues addressed in the final Initial Study include potential impacts on the physical development of the site on: air quality, biological resources, cultural resources, hazards and hazardous materials, and noise. The MND includes mitigation measures that would reduce any potentially significant project impacts to a less-than-significant level. In addition to mitigation measures, other permit conditions would be included in the future Planned Development Permit as conditions of approval.

In addition to CEQA, the project has a component of federal funding. Therefore, an Environmental Assessment (EA), pursuant to the National Environmental Protection Act (NEPA) was prepared jointly with the IS/MND, and circulated from January 19, 2018 to February 5, 2018. One public comment was received specifically for the EA. This comment addresses the concern regarding traffic congestion, air pollution, parking, lack of services in the area, and neighborhood safety and maintenance.

The entire IS/MND, EA, Response to Comment (to both CEQA and NEPA), and other related environmental documents are available on the Planning web site at:

<http://www.sanjoseca.gov/index.aspx?NID=5935>

PUBLIC OUTREACH

A community meeting was held on June 29, 2017 at the Westminster Presbyterian Church (1100 Shasta Avenue, San Jose, CA 95126). The community meeting covered the proposed rezoning and proposed future project of 116 affordable units with multi-family and senior housing. The community meeting was posted on the City's website and a notice was sent to property owners and tenants within 1,000 feet of the subject site. There were approximately 30 community members in attendance at the meeting. The primary concerns were focused on access to the site, parking, building design, active space on the ground floor on Race Street and selection process for future tenants.


Staff followed Council Policy 6-30: Public Outreach Policy in order to inform the public of the proposed project. A notice of the public hearing was distributed two weeks early to the owners and tenants of all properties located within 1,000 feet of the project site. The staff report is posted on the City's website. Staff has been available to respond to questions from the public.

Staff received two letters in opposition to the proposed rezoning which included a signed petition from several neighbors and a separate email (see Attachment F). Additionally, we received one letter in support (see Attachment G). The opposition letter noted the following concerns:

- The proposed height is too tall and 3-4 stories is more appropriate for the area;
- The site is adjacent to the Hanchette Park Historic Preservation Area.
- Traffic and parking are issues for the area.

The current A(PD) Zoning District allows up to 60 feet in height, which is approximately 4-5 stories. The proposed rezoning would allow up to 80 feet in height, which would accommodate up to 6 stories. The applicant has indicated that the 6-story building would be on parcel B with a slightly shorter building on parcel A. The Hanchette Park Historic Preservation Area is generally located on the north side of Park Avenue and the subject site is located outside the preservation area. The project design will be subject to the adopted Development Standards in addition to the City’s Residential Design Guidelines, which will evaluate the streetscape and neighborhood context. Traffic was analyzed in the MND for the project and a response to traffic comments has been included in the MND. Parking was discussed above.

Project Manager: Shaunn Mendrin, AICP

Approved by:  , Planning Official for Rosalynn Hughey,
Acting Planning Director

Date: 2/20/18

Attachments:
A) Draft Development Standards
B) Share Parking Analysis
C) Draft Ordinance
D) Draft Mitigated Negative Declaration Resolution
E) Signed Mitigation Monitoring and Reporting Program
F) Letter in Opposition
G) Letter in Support
H) Reduced Plan Sets

Owner:	Applicant:
Core Race Street, LLC 470 S. Market Street, San Jose, CA 95113	LMPD Attn: Anthony Ho 1288 Kifer Road, Suite 206 Sunnyvale, CA 94086

FILE NO. PDC17-019
237 RACE STREET DRAFT DEVELOPMENT STANDARDS

In any cases where the graphic plans and text may differ, this text takes precedence.

ALLOWED USES

- Up to 206 residential units
- Permitted, Special, and Conditional uses of the R-M Multiple Residence Zoning District of Title 20 of the San José Municipal Code, and as may be as amended in the future. Special and Conditional uses as identified in the R-M Multiple Residence District shall be subject to approval of a Planned Development Permit or Amendment by the Planning Director.

DEVELOPMENT STANDARDS

BUILDING HEIGHT

The maximum height of any buildings shall be 80 feet. Rooftop elements that may need to exceed the maximum building height due to their function, such as stair and elevator towers, shall not exceed 10 feet beyond the maximum building height. Such rooftop elements shall be integrated into the design of the building and shall be setback from the northern property line a minimum of one foot for each foot in height.

SETBACKS

All building setbacks are from the back of the public right-of-way (where private property meets public right-of-way).

Setbacks	
Front setback (Race St)	4 feet minimum
Right Side (North)	14 feet
Left Side (South)	16 feet
Rear setback (Grand St)	12 feet minimum
Building A & B Separation	40 feet minimum
Architectural Projections	2 feet maximum (<i>Any projections into the public right-of-way are subject to additional Municipal Code and City Policy.</i>)
Maximum height	80 feet

OFF STREET PARKING REQUIREMENTS

Residential Uses	
Parking (Parcel A ¹)	
Studios	.57 per unit
1 bedroom	.57 per unit
2 bedroom	1.7 per unit
Parking (Parcel B ²)	
Studios	1.25 per unit
1 bedroom	1.25 per unit
2 bedroom	1.7 per unit
3 bedroom	2.0 per unit
1. Parcel A – Senior Housing Development	
2. Parcel B – Multi-Family Housing Development	
Ground-Floor Commercial	
Ground-floor commercial uses shall conform to Chapter 20.90 of the San Jose Municipal Code (Title 20—Zoning Ordinance), as may be amended in the future. Prohibited uses include day care centers, public or private elementary and secondary schools, and church/religious assembly uses.	
Other Uses	
Parking spaces for all other uses are to be provided per the San José Municipal Code, Title 20, as may be amended. Ground floor commercial uses shall be subject to the requirements of the Shared Parking Analysis, dated January 22, 2018. Parking exceptions and alternative parking arrangements as specified in Chapter 20.90 of the San Jose Municipal Code (Title 20—Zoning Ordinance), as may be amended, may be utilized.	
Bicycle Parking Requirements	
Per Chapter 20.90 of the San Jose Municipal Code (Title 20—Zoning Ordinance), as may be amended in the future.	

ARCHITECTURAL & SITE DESIGN

- Projects shall be consistent with the Residential Design Guidelines, as may be amended.
- Race Street Frontage shall include active spaces at ground floor level for both buildings. This may be accomplished through commercial tenant spaces, providing ground floor or townhouse style units with front entries and stoops, or other active spaces as reviewed through the Planned Development Permit.
- The Grand Avenue frontage shall include appropriate treatment of the garage frontages including a combination of landscaping, glazing and other interesting treatments.
- All ground floor frontages shall include transparent glazing or appropriate architectural and landscaping treatment.
- The architectural style, massing and materials shall be compatible with the residential uses to the north of the site.
- Wall face spans over 30 feet should provide a minimum break in the façade of 5 feet.

- Vehicular site access shall be off of Grand Avenue to reduce pedestrian and traffic conflicts on Race Street.
- Multiple materials and façade variations shall be utilized to increase visual interest. This could include the use of quality windows with ample recesses, smooth stucco finishes, horizontal or vertical siding and other similar elements.

OPEN SPACE

- Open space shall be provided as per the Residential Design Guidelines, as amended in the future.

PERFORMANCE STANDARDS

- Multi-family Residential: In accordance with Chapter 20.30 of the City of San José Municipal Code (Title 20 - Zoning Ordinance), as may be amended in the future.
- Ground-floor Commercial: In accordance with Chapter 20.40 of the City of San José Municipal Code (Title 20 - Zoning Ordinance), as may be amended in the future.

ENVIRONMENTAL MITIGATION

Implement the mitigation measures identified in the Initial Study (IS) and Mitigated Negative Declaration (MND) and Environmental Assessment (EA) and Finding of No Significant Impact (FONSI) for the Race Street and Grand Avenue Residential Development Project (File No. PDC17-019) and Mitigation Monitoring and Reporting Program, as may be amended.



Memorandum

Date: January 22, 2018
To: Hannah Darst, David J. Powers & Associates, Inc.
From: Brian Jackson
Subject: Shared Parking Analysis for the Race Street Residential Project

Hexagon Transportation Consultants, Inc. has completed a shared parking analysis for a proposed residential mixed-use development on Race Street in San Jose, California. The project site is located on the west side of Race Street between W. San Carlos Street and Park Avenue. The project involves removing the existing uses on the site and constructing a mixed-use residential development with 206 multi-family residential units and up to 8,500 square feet (s.f.) of ground floor retail space.

The September 26, 2017 site plan prepared by OJK Architecture and Planning shows that the retail component of the project would be located on Race Street. The purpose of this shared parking analysis memo is to identify the parking requirements for the proposed project, and to develop parking strategies to ensure the parking supply is adequate to serve both the residential and retail components of the project.

Parking Requirements

The off-street parking requirements for the project are based on the City of San Jose parking standards (*San Jose Municipal Code Chapter 20.90*).

The City of San Jose vehicle parking requirements for multiple residential dwellings are as follows:

- 1.25 spaces per one-bedroom unit
- 1.7 spaces per two-bedroom unit
- 2.0 spaces per three-bedroom unit

The City of San Jose vehicle parking requirement for retail/commercial uses located within Urban Villages was applied to the project and is as follows:

- 1 space per 400 s.f.

The project is proposing 138 one-bedroom units, 38 two-bedroom units, 30 three-bedroom units, and up to 8,500 s.f. of ground floor retail space. Based on this breakdown, the project is required to provide a total of 320 vehicle parking spaces: 298 spaces to serve the residential use and 22 spaces to serve the retail use.

Note that since the project is located within 2,000 feet of an existing bus rapid transit (BRT) station, and assuming the project would provide adequate bicycle parking per the City's requirement, the project would be eligible for a 20 percent reduction in off-street vehicle parking (*San Jose Municipal Code Section 20.90.220*). With this 20 percent reduction, the project would be required to provide a total of 256 vehicle parking spaces: 238 spaces for the residential use and 18 spaces for the retail use.



Parking Supply

The project is proposing to provide a total of 242 parking spaces, which would be adequate to serve the residential component of the project (after the 20 percent parking reduction is applied). This leaves 4 parking spaces to serve the retail use. Thus, the project falls short of the City's parking requirement by 14 parking spaces. Hexagon does not believe that this would cause any parking demand issues based on the shared nature of the parking supply. A shared parking analysis was conducted in order to support this claim.

Shared Parking Analysis

Since the project would include complementary land uses, on-site parking could be shared between the retail and residential uses. An analysis was conducted to determine the number of parking spaces that could be shared. The shared parking analysis is based on the Urban Land Institute's publication entitled *Shared Parking*, which provides parking occupancy rates for many land uses according to time of day. The parking occupancy rates can be applied to the peak parking demand for each proposed land use. Comparing the parking requirement for each land use separately with the cumulative parking demand for both land uses will show whether or not parking demand can be reduced through implementation of a shared parking plan. It should be noted that sharing can occur only if the residential parking spaces are not reserved for specific residential units.

Table 1 shows the parking occupancy and the potential for shared parking between the two proposed land uses. The table is based on the ULI *Shared Parking* time of day factors. During the midday, the retail use would require its maximum parking supply, whereas the residential use would not. The peak parking demand for the residential use would occur late in the evening. The results of the shared parking analysis show that parking demand for the proposed land uses are complementary, and some spaces associated with the residential component of the project would remain vacant during the midday hours when the retail use would peak.

According to the shared parking analysis, the combined peak parking demand for the proposed development would occur during the week between 7:00 PM and 8:00 PM. During this time of the evening a total of 248 parking spaces would be required to meet the project's parking demand, including 15 spaces for the retail use and 233 spaces for the residential use. This equates to 8 fewer parking spaces than what the City of San Jose zoning code requires.

Since the project is proposing a total of 242 off-street parking spaces, the project would still fall short of the parking requirement by 6 parking spaces ($248 - 242 = 6$). According to the site plan, the project would reconstruct the sidewalk and curb along its frontage on Race Street, which includes the removal of some of the existing driveways. As a result of these project improvements, 3 additional street parking spaces would be added to this segment of Race Street (12 proposed spaces vs. 9 existing spaces). This leaves a project parking deficit of just 3 spaces. It is presumed that this small parking deficit would be satisfied by the availability of street parking in the area, particularly along Race Street and Grand Avenue.

Parking Reduction Considerations

It is important to note that although this shared parking analysis is based on a project description that includes up to 8,500 s.f. of retail space (in order to be consistent with the recent TIA prepared for the project), it is our understanding that the development would likely include less than half this amount of retail space. In addition, the applicant is considering substituting as many as 90 of the 206 multi-family units with senior housing units. Senior housing units with single room occupancy

(SRO) require less parking than multi-family residential units. For these reasons, the proposed number of off-street parking spaces (242) would most likely be adequate to meet the combined peak parking demand for the project.

Table 1
Shared Parking Analysis

Hour of Day	Retail		Residential		Total Demand	
	Wkdy	Wknd	Wkdy	Wknd	Wkdy	Wknd
Parking Demand by Hour:						
6 a.m.	1	1	238	238	239	239
7 a.m.	1	1	214	214	215	215
8 a.m.	4	3	202	202	206	205
9 a.m.	8	7	190	190	198	197
10 a.m.	12	10	179	179	191	189
11 a.m.	16	13	167	167	182	179
Noon	17	15	155	155	172	170
1 p.m.	18	17	167	167	185	183
2 p.m.	17	18	167	167	184	185
3 p.m.	17	18	167	167	183	185
4 p.m.	17	17	179	179	195	196
5 p.m.	17	16	202	202	219	219
6 p.m.	17	15	214	214	231	229
7 p.m.	17	14	231	231	248	245
8 p.m.	15	12	233	233	248	245
9 p.m.	10	10	236	236	246	245
10 p.m.	6	7	238	238	244	245
11 p.m.	2	3	238	238	240	241
Midnight	0	0	238	238	238	238
City of San Jose Parking Requirement					Max. Demand	
	18	18	238	238	248	245
Source: Urban Land Institute (ULI) <i>Shared Parking, 2nd Edition, 2005.</i>						

DRAFT

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY OF SAN JOSE REZONING CERTAIN REAL PROPERTY OF APPROXIMATELY 2.3 GROSS ACRES SITUATED ON THE WEST SIDE OF RACE STREET, APPROXIMATELY 300 FEET SOUTHERLY OF PARK AVENUE (237-253 RACE STREET & 216-280 GRAND AVENUE) FROM THE A(PD) PLANNED DEVELOPMENT ZONING DISTRICT TO THE R-M(PD) PLANNED DEVELOPMENT ZONING DISTRICT.

WHEREAS, all rezoning proceedings required under the provisions of Chapter 20.120 of Title 20 of the San José Municipal Code have been duly had and taken with respect to the real property hereinafter described; and

WHEREAS, a Mitigated Negative Declaration was prepared in conformance with the California Environmental Quality Act of 1970 (CEQA), as amended, for the subject rezoning to R-M(PD) Planned Development Zoning District under File No. PDC17-019 (the “MND”); and

WHEREAS, the City Council of the City of San José is the decision-making body for the proposed subject rezoning to R-M(PD) Planned Development Zoning District; and

WHEREAS, this Council of the City of San José has considered, approved and adopted said MND and related Mitigation Monitoring and Reporting Program under separate Council resolution prior to taking any approval actions on the project.

NOW, THEREFORE, BE IT ORDAINED BY THE COUNCIL OF THE CITY OF SAN JOSE:

SECTION 1. The recitals above are incorporated herein.

SECTION 2. All that real property hereinafter described in this section, hereinafter referred to as "subject property," is hereby rezoned as R-M(PD) Planned Development Zoning District. The base district zoning of the subject property shall be the R-M Multiple Residence Zoning District. The Planned Development zoning of the subject property shall be that development plan for the subject property entitled, "General Development Plan – Exhibit C, dated February 16, 2018 (“General Development Plan”).

Said General Development Plan is on file in the office of the Director of Planning and is available for inspection by anyone interested therein, and said General Development Plan is by this reference adopted and incorporated herein the same as if it were fully set forth herein.

The subject property referred to in this section is all that real property situated in the County of Santa Clara, State of California, described and depicted in Exhibit “A” attached hereto and incorporated herein by this reference.

SECTION 3. The district map of the City is hereby amended accordingly.

SECTION 4. The land development approval that is the subject of City File No. PDC17-019 is subject to the operation of Part 2.75 of Chapter 15.12 of Title 15 of the San José Municipal Code. The applicant for or recipient of such land use approval hereby acknowledges receipt of notice that the issuance of a building permit to implement such land development approval may be suspended, conditioned or denied where the City Manager has determined that such action is necessary to remain within the aggregate operational capacity of the sanitary sewer system available to the City of San José or to meet the discharge standards of the sanitary sewer system imposed by the California Regional Water Quality Control Board for the San Francisco Bay Region.

PASSED FOR PUBLICATION of title this _____ day of _____, 2018 by the following vote:

AYES:

NOES:

ABSENT:

DISQUALIFIED:

SAM LICCARDO
Mayor

ATTEST:

TONI J. TABER, CMC
City Clerk

RESOLUTION NO. _____

A RESOLUTION OF THE COUNCIL OF THE CITY OF SAN JOSE ADOPTING THE RACE STREET AND GRAND AVENUE RESIDENTIAL DEVELOPMENT PROJECT MITIGATED NEGATIVE DECLARATION, FOR WHICH AN INITIAL STUDY WAS PREPARED, ALL IN ACCORDANCE WITH THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, AS AMENDED, AND ADOPTING A RELATED MITIGATION MONITORING AND REPORTING PROGRAM

WHEREAS, prior to the adoption of this Resolution, the Planning Director of the City of San José prepared an Initial Study and approved for circulation a Mitigated Negative Declaration for the Race Street and Grand Avenue Residential Development Project under Planning File No. PDC17-019 (the “Initial Study/Mitigated Negative Declaration”), all in accordance with the requirements of the California Environmental Quality Act of 1970, together with state and local guidelines implementing said Act, all as amended to date (collectively “CEQA”); and

WHEREAS, the Race Street and Grand Avenue Residential Development Project (the “Project”) analyzed under the Initial Study/Mitigated Negative Declaration consists of a rezoning from the A(PD) Planned Development Zoning District to A(PD) Planned Development Zoning District to allow for the demolition of all buildings and structures on site and construct one of two scenarios: (1) 206 multi-family apartment units with approximately 8,500 square feet of retail/commercial space; or (2) 116 multi-family and 90 senior apartment units with approximately 8,500 square feet of retail/commercial space on an approximate 2.3 gross acre site located west of race Street, east of Grand Avenue, south of Park Avenue, and north of West San Carlos Street in the City of San José (Assessor’s Parcel Number 261-42-007, -008, -011, -058, -069 to -072, and -079), San José, California; and

WHEREAS, the Initial Study/Mitigated Negative Declaration concluded that implementation of the Project could result in certain significant effects on the environment and identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, in connection with the approval of a project involving the preparation of an initial study/mitigated negative declaration that identifies one or more significant environmental effects, CEQA requires the decision-making body of the lead agency to incorporate feasible mitigation measures that would reduce those significant environmental effects to a less-than-significant level; and

WHEREAS, whenever a lead agency approves a project requiring the implementation of measures to mitigate or avoid significant effects on the environment, CEQA also requires a lead agency to adopt a mitigation monitoring and reporting program to ensure compliance with the mitigation measures during project implementation, and such a mitigation monitoring and reporting program has been prepared for the Project for consideration by the decision-maker of the City of San José as lead agency for the Project (the “Mitigation Monitoring and Reporting Program”); and

WHEREAS, the City of San José is the lead agency on the Project, and the City Council is the decision-making body for the proposed approval to undertake the Project; and

WHEREAS, the City Council has reviewed and considered the Initial Study/Mitigated Negative Declaration and related Mitigation Monitoring and Reporting Program for the Project and intends to take actions on the Project in compliance with CEQA and state and local guidelines implementing CEQA; and

WHEREAS, the Initial Study/Mitigated Negative Declaration and related Mitigation Monitoring and Reporting Program for the Project are on file in the Office of the Director

of Planning, located at 200 East Santa Clara Street, 3rd Floor Tower, San José, California, 95113, are available for inspection by any interested person at that location and are, by this reference, incorporated into this Resolution as if fully set forth herein;

NOW, THEREFORE, BE IT RESOLVED BY THE COUNCIL OF THE CITY OF SAN JOSE:

THAT THE CITY COUNCIL does hereby make the following findings: (1) it has independently reviewed and analyzed the Initial Study/Mitigated Negative Declaration and other information in the record and has considered the information contained therein, prior to acting upon or approving the Project, (2) the Initial Study/Mitigated Negative Declaration prepared for the Project has been completed in compliance with CEQA and is consistent with state and local guidelines implementing CEQA, and (3) the Initial Study/ Mitigated Negative Declaration represents the independent judgment and analysis of the City of San José, as lead agency for the Project. The City Council designates the Director of Planning at the Director's Office at 200 East Santa Clara Street, 3rd Floor Tower, San José, California, 95113, as the custodian of documents and records of proceedings on which this decision is based.

THAT THE CITY COUNCIL does hereby find that based upon the entire record of proceedings before it and all information received that there is no substantial evidence that the Project will have a significant effect on the environment and does hereby adopt the Mitigated Negative Declaration and related Mitigation Monitoring and Reporting Program prepared for the Project (Planning File Nos. PDC17-019). The Mitigation Monitoring and Reporting Program for the Project is attached hereto as Exhibit "A" and fully incorporated herein. The Initial Study/ Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program are: (1) on file in the Office of the Director of Planning, located at 200 East Santa Clara Street, 3rd Floor Tower, San José, California, 95113 and (2) available for inspection by any interested person.

ADOPTED this ____ day of _____, 2018, by the following vote:

AYES:

NOES:

ABSENT:

DISQUALIFIED:

SAM LICCARDO
Mayor

ATTEST:

TONI J. TABER, CMC
City Clerk

MITIGATION MONITORING AND REPORTING PROGRAM

Race Street and Grand Avenue Residential Development

File No. PDC17-019

CITY OF SAN JOSÉ
February 2018



P R E F A C E

Section 21081.6 of the California Environmental Quality Act (CEQA) requires a Lead Agency to adopt a Mitigation Monitoring and Reporting Program whenever it approves a project for which measures have been required to mitigate or avoid significant effects on the environment. The purpose of the monitoring and reporting program is to ensure compliance with the mitigation measures during project implementation.

The Initial Study/Mitigated Negative Declaration prepared for the Race Street and Grand Avenue Residential Development Project concluded that the implementation of the project could result in significant effects on the environment and mitigation measures were incorporated into the proposed project or are required as a condition of project approval. This Mitigation Monitoring and Reporting Program addresses those measures in terms of how and when they will be implemented.

This document does *not* discuss those subjects for which the Initial Study/Mitigated Negative Declaration concluded that the impacts from implementation of the project would be less than significant.

I, VINCE CANTORE, the applicant, on the behalf of ^{THE}CORE COMPANIES, hereby agree to fully implement the Mitigation Measures described below which have been developed in conjunction with the preparation of an Initial Study/Mitigated Negative Declaration for my proposed project. I understand that these mitigation measures or substantially similar measures will be adopted as conditions of approval with my development permit request to avoid or significantly reduce potential environmental impacts to a less than significant level, where feasible.

Project Applicant's Signature 

Date 2/15/2018

MITIGATIONS	MONITORING AND REPORTING PROGRAM				
	Documentation of Compliance [Project Applicant/Proponent Responsibility]		Documentation of Compliance [Lead Agency Responsibility]		
	Method of Compliance Or Mitigation Action	Timing of Compliance	Oversight Responsibility	Actions/Reports	Monitoring Timing or Schedule
AIR QUALITY					
Impact MM AIR-1: The project would result in a maximum residential cancer risk during construction activities that would exceed the BAAQMD significance threshold.					
<p>MM-AIR-1.1: The project shall develop a plan demonstrating that the off-road equipment used on-site to construct the project would achieve a fleet-wide average 37 percent reduction in PM₁₀ exhaust emissions (assumed to be diesel particulate matter [DPM]) or more. Feasible methods to achieve this reduction would include, but are not limited to, the following:</p> <ul style="list-style-type: none"> • All mobile diesel-powered off-road equipment larger than 25 horsepower and operating on the site for more than two days continuously shall meet, at a minimum, U.S. EPA particulate matter emissions standards for Tier 2 engines or equivalent and include the use of equipment that includes CARB-certified Level 3 Diesel Particulate Filters. • Use of alternatively-fueled equipment (i.e., non-diesel). • Other measures may be the use of added exhaust devices, or a combination of measures, provided that these measures are approved by the City and demonstrated to reduce community risk impacts to a less than significant level. • The project applicant shall prepare a construction operations plan that includes specifications of the equipment to be used 	<p>The proposed plan to achieve a fleet-wide average 37 percent reduction in diesel particulate matter (DPM) emissions shall be submitted to the Supervising Environmental Planner of the Planning, Building, and Code Enforcement Department for review/approval.</p>	<p>Prior to the issuance of any demolition, grading, and/or building permits (whichever occurs earliest).</p>	<p>Supervising Environmental Planner of Department of Planning, Building, and Code Enforcement.</p>	<p>Review/approve the proposed plan to achieve a fleet-wide average 37 percent reduction in DPM emissions.</p>	<p>Prior to the issuance of any demolition, grading, and/or building permits (whichever occurs earliest).</p>

MITIGATIONS	MONITORING AND REPORTING PROGRAM				
	Documentation of Compliance [Project Applicant/Proponent Responsibility]		Documentation of Compliance [Lead Agency Responsibility]		
	Method of Compliance Or Mitigation Action	Timing of Compliance	Oversight Responsibility	Actions/Reports	Monitoring Timing or Schedule
<p>during construction to demonstrate how a fleet-wide average 37 percent reduction in DMP emissions would be achieved.</p> <p>The plan shall be submitted to the Supervising Environmental Planner of the City of San José Department of Planning, Building, and Code Enforcement prior to the issuance of any demolition, grading, and/or building permits (whichever occurs earliest). The plan shall be accompanied by a letter signed by a qualified air quality specialist, verifying that the equipment included in the plan meets the standards set forth in this mitigation measure.</p>					
BIOLOGICAL RESOURCES					
Impact BIO-1: Demolition, grading, and construction activities and tree removal during nesting season could impact nearby migratory birds.					
<p>MM BIO-1.1: <u>Avoidance:</u> The project applicant shall schedule demolition and construction activities to avoid the nesting season. The nesting season for most birds, including most raptors in the San Francisco Bay area, extends from February 1st through August 31st (inclusive), as amended.</p>	<p>All measures shall be printed on all construction documents, contracts, and project plans.</p> <p>Avoid construction activities during nesting seasons.</p>	<p>Prior to the issuance of any tree removal, demolition, grading, and/or building permits (whichever occurs earliest).</p>	<p>Supervising Environmental Planner of the City of San José Department of Planning, Building, and Code Enforcement.</p>	<p>Ensure that all measures are printed on all construction documents, contracts, and project plans. Confirm if project is scheduled outside of nesting season.</p>	<p>Prior to the issuance of any tree removal, demolition, grading, and/or building permits (whichever occurs earliest).</p>
<p>MM BIO-1.2: <u>Nesting Bird Surveys:</u> If it is not possible to schedule demolition and construction between September 1st and January 31st (inclusive),</p>	<p>All measures shall be printed on all construction</p>	<p>Prior to the issuance of any tree removal, demolition, grading,</p>	<p>Supervising Environmental Planner of the City of San José</p>	<p>Ensure that all measures are printed on all</p>	<p>Prior to the issuance of any tree removal,</p>

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<p>pre-construction surveys for nesting birds shall be completed by a qualified ornithologist to ensure that no nests shall be disturbed during project implementation. This survey shall be completed no more than 14 days prior to the initiation of construction activities during the early part of the breeding season (February 1st through April 30th inclusive) and no more than 30 days prior to the initiation of these activities during the late part of the breeding season (May 1st through August 31st inclusive). During this survey, the ornithologist shall inspect all trees and other possible nesting habitats immediately adjacent to the construction areas for nests.</p>	<p>documents, contracts, and project plans.</p> <p>Submit a report indicating the results of the preconstruction survey and any designated buffer zones to the Supervising Environmental Planner at the Department of Planning, Building, and Code Enforcement.</p>	<p>and/or building permits (whichever occurs earliest).</p>	<p>Department of Planning, Building, and Code Enforcement.</p>	<p>construction documents, contracts, and project plans.</p> <p>Review/approve preconstruction survey report, if applicable.</p>	<p>demolition, grading, and/or building permits (whichever occurs earliest).</p>
<p>MM BIO-1.3: <u>Buffer Zones:</u> If an active nest is found sufficiently close to work areas to be disturbed by construction, the ornithologist, in consultation with the California Department of Fish and Wildlife, shall determine the extent of a construction free buffer zone to be established around the nest, typically 250 feet, to ensure that raptor or migratory bird nests shall not be disturbed during project construction.</p>	<p>All measures shall be printed on all construction documents, contracts, and project plans.</p> <p>Submit a report indicating the results of the preconstruction survey and any designated buffer zones to the Supervising Environmental Planner of the Department of Planning, Building, and Code Enforcement.</p>	<p>Prior to the issuance of any tree removal, demolition, grading, and/or building permits (whichever occurs earliest).</p>	<p>Supervising Environmental Planner of the City of San José Department of Planning, Building, and Code Enforcement.</p>	<p>Ensure that all measures are printed on all construction documents, contracts, and project plans.</p> <p>Review/approve preconstruction survey report.</p>	<p>Prior to the issuance of any tree removal, demolition, grading, and/or building permits (whichever occurs earliest).</p>
<p>MM BIO-1.4: <u>Reporting:</u> Prior to any tree removal, or approval of any grading or demolition permits (whichever occurs first), the ornithologist shall submit a report indicating the results of the survey and any</p>	<p>All measures shall be printed on all construction documents, contracts, and project plans.</p>	<p>Prior to the issuance of any tree removal, demolition, grading, and/or building</p>	<p>Supervising Environmental Planner of the City of San José Department of</p>	<p>Ensure that all measures are printed on all construction</p>	<p>Prior to the issuance of any tree removal, demolition,</p>

MITIGATIONS	MONITORING AND REPORTING PROGRAM				
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designated buffer zones to the satisfaction of the City's Supervising Environmental Planner.	Submit a report indicating the results of the preconstruction survey and any designated buffer zones to the Supervising Environmental Planner at the Department of Planning, Building, and Code Enforcement.	permits (whichever occurs earliest).	Planning, Building, and Code Enforcement.	documents, contracts, and project plans. Review/approve preconstruction survey report.	grading, and/or building permits (whichever occurs earliest).

CULTURAL RESOURCES

Impact CUL-1: Construction activities could impact unknown archaeological resources.

<p>MM CUL-1.1: Preliminary Investigation: Once the buildings and structures have been demolished and the pavement and landscaping removed, a qualified archaeologist shall complete a presence/absence exploration to determine if there are any indications of discrete historic-era subsurface archaeological features. Shallow mechanical excavations shall be focused along the back part of the lot boundaries dividing the eastern and western parcels between Grand Avenue and Race Street where historical outbuildings were once situated. At least one trench shall be excavated to 15 feet deep to address the potential for subsurface Native American archaeological resources within the project area. The results of the presence/absence exploration shall be submitted to the Supervising Environmental Planner and Historic Preservation Officer of the City of San José Department of Planning, Building, and Code Enforcement for review and approval prior to issuance</p>	<p>All measures shall be printed on all construction documents, contracts, and project plans.</p> <p>Complete a presence/absence exploration to determine whether historic archaeological features exist on the site and submit the results to the Supervising Environmental Planner and Historic Preservation Officer of the City of San José Department of Planning, Building, and Code Enforcement.</p>	<p>Prior to issuance of any grading permits and during all phases of construction.</p>	<p>Supervising Environmental Planner and Historic Preservation Officer of the City of San José Department of Planning, Building, and Code Enforcement.</p>	<p>Review the presence/absence exploration results.</p>	<p>Prior to issuance of any grading permits and during all phases of construction.</p>
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MITIGATIONS	MONITORING AND REPORTING PROGRAM				
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	Method of Compliance Or Mitigation Action	Timing of Compliance	Oversight Responsibility	Actions/Reports	Monitoring Timing or Schedule
of any grading permit. Based on the findings of the presence/absence exploration, an archaeological resources treatment plan (as described in MM CUL-1.2) shall be prepared by a qualified archaeologist if necessary.	Prepare and implement a treatment plan for historic and prehistoric artifacts, as necessary.				
<p>MM CUL-1.2 Treatment Plan: If required by MM CUL-1.1, the project applicant shall retain a qualified archaeologist to prepare a treatment plan that reflects the permit-level detail pertaining to depths and locations of all ground disturbing activities. The treatment plan shall be prepared and submitted to the Supervising Environmental Planner and the Historic Preservation Officer of the City of San José Department of Planning, Building, and Code Enforcement prior to approval of any grading permit. The treatment plan shall contain, at a minimum:</p> <ul style="list-style-type: none"> • Identification of the scope of work and range of subsurface effects (including location map and development plan), including requirements for preliminary field investigations. • Description of the environmental setting (past and present) and the historic/prehistoric background of the parcel (potential range of what might be found). • Development of research questions and goals to be addressed by the investigation (what is significant vs. what is redundant information). • Detailed field strategy to record, recover, or avoid the finds and address research goals. 	A qualified archaeologist shall prepare and implement the project-specific archaeological resource treatment plan, as necessary. The plan shall be reviewed and approved by the Supervising Environmental Planner and Historic Preservation Officer of the City of San José Department of Planning, Building, and Code Enforcement.	Prior to issuance of any grading permits and during all phases of construction.	Supervising Environmental Planner and Historic Preservation Officer of the City of San José Department of Planning, Building, and Code Enforcement.	Review the treatment plan for historic and prehistoric artifacts.	Prior to issuance of any grading permits.

MITIGATIONS	MONITORING AND REPORTING PROGRAM				
	Documentation of Compliance [Project Applicant/Proponent Responsibility]		Documentation of Compliance [Lead Agency Responsibility]		
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<ul style="list-style-type: none"> • Analytical methods. • Report structure and outline of document contents. • Disposition of the artifacts. • Appendices: all site records, correspondence, and consultation with Native Americans, etc. <p>Implementation of the plan, by a qualified archaeologist, shall be required prior to the issuance of any grading permits. The treatment plan shall utilize data recovery methods to reduce impacts on subsurface resources.</p>					
<p>MM CUL-1.3: <u>Accidental Discovery:</u> In the event that prehistoric or historic resources are encountered during excavation and/or grading of the site, all activity within a 50-foot radius of the find shall be stopped, the Supervising Environmental Planner and Historic Preservation Officer of the City of San José Department of Planning, Building, and Code Enforcement shall be notified, and a qualified archaeologist will examine the find. Project personnel shall not collect or move any cultural material.</p> <p>The archaeologist shall 1) evaluate the find(s) to determine if they meet the definition of a historical or archaeological resource; and (2) make appropriate recommendations regarding the disposition of such finds prior to issuance of any occupancy permits. Construction and potential impacts to the area(s) within a radius determined by the archaeologist shall not recommence until the assessment is complete. If the</p>	<p>All measures shall be printed on all construction documents, contracts, and project plans.</p> <p>If prehistoric or historic resources are found on the site, stop work and implement additional measures to address the resource(s).</p>	<p>Prior to issuance of any occupancy permits and before and during all phases of construction.</p>	<p>Supervising Environmental Planner and Historic Preservation Officer of the City of San José Department of Planning, Building, and Code Enforcement, and the Northwest Information Center.</p>	<p>Ensure that all measures are printed on all construction documents, contracts, and project plans.</p> <p>Review/approve report of findings of any data recovery.</p>	<p>Prior to issuance of any occupancy permits and before and during all phases of construction.</p>

MITIGATIONS	MONITORING AND REPORTING PROGRAM				
	Documentation of Compliance [Project Applicant/Proponent Responsibility]		Documentation of Compliance [Lead Agency Responsibility]		
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<p>finds do not meet the definition of a historical or archaeological resources, no further study or protection is necessary prior to project implementation. If the find(s) does meet the definition of a historical or archaeological resource, then it shall be avoided by project activities. Project personnel shall not collect or move any cultural material. Fill soils that may be used for construction purposes shall not contain archaeological materials.</p> <p>If avoidance is not feasible, adverse effects to such resources shall be mitigated in accordance with the recommendations of the archaeologist. Recommendations shall include, but are not limited to, collection, recordation, and analysis of any significant cultural materials. Data recovery methods may include, but are not limited to, backhoe trenching, shovel test units, hand augering, and hand-excavation. The techniques used for data recovery shall follow the protocols identified in the approved treatment plan per MM CUL-1.2, or otherwise equivalent as determined by the qualified archaeologist.</p> <p>Data recovery shall include excavation and exposure of features, field documentation, and recordation. A final report documenting any data recovery shall be submitted to the Supervising Environmental Planner and Historic Preservation Officer of the City of San José Department of Planning, Building, and Code Enforcement and the Northwest Information Center (NWIC) prior to issuance of occupancy permits.</p>					

MITIGATIONS	MONITORING AND REPORTING PROGRAM				
	Documentation of Compliance [Project Applicant/Proponent Responsibility]		Documentation of Compliance [Lead Agency Responsibility]		
	Method of Compliance Or Mitigation Action	Timing of Compliance	Oversight Responsibility	Actions/Reports	Monitoring Timing or Schedule

HAZARDS AND HAZARDOUS MATERIALS

Impact HAZ-1: Tetrachloroethylene was detected in soil gas samples on the project site indicating a past release originating offsite.

<p>MM HAZ-1.1: Preliminary Investigation: Additional soil gas investigation and testing shall be completed to determine the extent of tetrachloroethylene (PCE) contamination on the project site. Based on the results of the investigation, additional mitigation measures may be required, including soil removal and vapor barriers. The results of the preliminary investigation shall be submitted to the Santa Clara County Department of Environmental Health (SCCDEH), or equivalent agency. This can also be part of the submittal as mentioned in MM HAZ 2.1 below. A copy of the preliminary investigation results shall be submitted to the Supervising Environmental Planner of the City of San José Department of Planning, Building, and Code Enforcement and Municipal Compliance Officer of the City of San José Environmental Services Department for approval prior to the issuance of any grading permits.</p>	<p>All measures shall be printed on all construction documents, contracts, and project plans.</p> <p>Complete preliminary investigation of soil gas testing and implement additional measures as necessary.</p>	<p>Prior to issuance of any grading permits.</p>	<p>Code Enforcement and Municipal Compliance Officer of the City of San José Environmental Services Department.</p> <p>Supervising Environmental Planner of the City of San José Department of Planning, Building, and Code Enforcement.</p>	<p>Review/approve additional measures to address PCE contamination, as applicable.</p>	<p>Prior to the issuance of any grading permits.</p>
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Impact HAZ-2: Shallow soils to be removed under the proposed project may include elevated hydrocarbon and metals concentrations, and removal of USTs may be needed as part of development.

<p>MM HAZ-2.1: Site Management Plan: Under regulatory oversight from the Santa Clara County Department of Environmental Health (SCCDEH) using their Voluntary Cleanup Program (VCP), or equivalent regulatory agency, the project applicant shall prepare the following documents:</p> <ul style="list-style-type: none"> As mentioned in MM HAZ-1.1, soil gas investigation and testing shall be completed to 	<p>All measures shall be printed on all construction documents, contracts, and project plans.</p> <p>Complete preliminary investigation of soil gas testing and implement</p>	<p>Prior to issuance of any grading permits.</p>	<p>Santa Clara County Department of Environmental Health, Supervising Environmental Planner of the City of San José Department of Planning, Building,</p>	<p>SCCDEH (or equivalent agency) reviews and approves SMP and HSP, if applicable.</p> <p>A copy of the SMP, HSP, and</p>	<p>Prior to issuance of any grading permits.</p>
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MITIGATIONS	MONITORING AND REPORTING PROGRAM				
	Documentation of Compliance [Project Applicant/Proponent Responsibility]		Documentation of Compliance [Lead Agency Responsibility]		
	Method of Compliance Or Mitigation Action	Timing of Compliance	Oversight Responsibility	Actions/Reports	Monitoring Timing or Schedule
<p>determine the extent of PCE contamination on the project site. Based on the results of the investigation, the regulatory agency may require a Site Management Plan (SMP) or similar document to manage the cleanup of potential contamination.</p> <ul style="list-style-type: none"> • If applicable, a SMP shall be prepared prior to construction to reduce or eliminate exposure risk to human health and the environment, specifically, potential risks associated with the presence of lead-contaminated soils. • The SMP shall be prepared by a qualified hazardous materials consultant to establish management practices for handling contaminated soil or other materials encountered during construction activities. The SMP shall include, but is not limited to, the following: <ul style="list-style-type: none"> ○ A detailed discussion of the site background; ○ Proper mitigation as needed for demolition of existing structures; ○ Management of stockpiles, including sampling, disposal, and dust and runoff control including implementation of a stormwater pollution prevention program; ○ Management of underground structures encountered, including utilities and/or underground storage 	<p>additional measures as necessary.</p> <p>Prepare and submit a Site Management Plan and Health and Safety Plan.</p>		<p>and Code Enforcement, and Municipal Compliance Officer of the City of the San José Environmental Services Department.</p>	<p>confirmation from SCCDEH (or equivalent agency) shall be reviewed by City of San José Supervising Environmental Planner of the Department of Planning, Building, and Code Enforcement.</p>	

MITIGATIONS	MONITORING AND REPORTING PROGRAM				
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	Method of Compliance Or Mitigation Action	Timing of Compliance	Oversight Responsibility	Actions/Reports	Monitoring Timing or Schedule
<p>tanks (also specified in MM HAZ-2.2);</p> <ul style="list-style-type: none"> ○ Procedures to follow if evidence of an unknown historic release of hazardous materials (e.g., underground storage tanks, polychlorinated biphenyls, asbestos-containing materials, lead-based paint, etc.) is discovered during excavation or demolition; ○ A health and safety plan (HSP) for each contractor working at the site, in an area below grade, that addresses the safety and health hazards of each site operation phase, including the requirements and procedures for employee protection. The HSP shall outline proper soil handling procedures and health and safety requirements to minimize work and public exposure to hazardous materials during construction; ○ A section about regulatory agencies and protocol if underground storage tanks (USTs) are encountered during construction activities; and ○ A section about regulatory agencies and protocol if complete removal of USTs is needed. 					

MITIGATIONS	MONITORING AND REPORTING PROGRAM				
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	Method of Compliance Or Mitigation Action	Timing of Compliance	Oversight Responsibility	Actions/Reports	Monitoring Timing or Schedule
The SMP shall be submitted to the SCCDEH (or equivalent agency) for review and approval. A copy of the approved SMP shall be submitted to the Supervising Environmental Planner of the City of San José Department of Planning, Building, and Code Enforcement and Municipal Compliance Officer of the City of San José Environmental Services Department for approval prior to the issuance of any grading permits.					
MM HAZ-2.2: Discovery of USTs: If USTs are encountered during demolition, construction, or grading activities, the project applicant shall notify SCCDEH and the City of San José Fire Department. Earthmoving activities shall be ceased until appropriate measures, approved by SCCDEH and/or the City of San José Fire Department, are taken to address the UST.	All measures shall be printed on all construction documents, contracts, and project plans. If USTs are found on the site, stop work and implement additional measures to address the UST(s).	All measures shall be printed on all construction documents, contracts, and project plans prior to the issuance of any grading permits. During all phases of construction.	Santa Clara County Department of Environmental Health and City of San José Fire Department. Supervising Environmental Planner of the City of San José Department of Planning, Building, and Code Enforcement.	Ensure that all measures are printed on all construction documents, contracts, and project plans. Review/approve additional measures to address USTs, as applicable.	All measures shall be printed on all construction documents, contracts, and project plans prior to the issuance of any grading permits. During all phases of construction.
NOISE AND VIBRATION					
Impact NOI-1: Construction noise and vibration generated by the proposed project could impact nearby sensitive receptors.					
MM NOI-1.1: Construction Noise and Vibration Plan: The project applicant shall develop and implement a Construction Noise and Vibration Logistics Plan during all phases of construction on the project site. The Plan shall be included as part of the contracts for	All measures shall be printed on all construction documents, contracts, and project plans.	Prior to issuance of any grading permits.	Supervising Environmental Planner of the City of San José Department of Planning, Building,	Ensure that all measures are printed on all construction documents,	Prior to issuance of any grading permits.

MITIGATIONS	MONITORING AND REPORTING PROGRAM				
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	Method of Compliance Or Mitigation Action	Timing of Compliance	Oversight Responsibility	Actions/Reports	Monitoring Timing or Schedule
<p>construction workers and applicable supervisors. All measures shall be printed on all approved construction documents, contracts, and/or project plans. The project applicant shall submit a copy of all approved plans, construction documents, contracts, and/or project plans to the Supervising Environmental Planner prior to the issuance of any grading permit. The Plan shall include, but is not limited to, the following:</p> <ul style="list-style-type: none"> • A list of all potential equipment (including specs) that will be used during all earthmoving activities. • A schedule of all earthmoving activities. • Responsibilities of personnel on the site. • Outreach strategies to inform nearby residences of construction hours and phase. • Best management practices to reduce construction noise such as, but is not limited to, the following: <ul style="list-style-type: none"> ○ Construct solid plywood fences around construction sites adjacent to operational businesses, residences, or noise-sensitive land uses. ○ Utilize “quiet” models of air compressors and other stationary noise sources where technology exists. ○ Equip all internal combustion engine-driven equipment with mufflers, which are in good condition and appropriate for the equipment. 	<p>Prepare and submit a Construction Noise and Vibration Logistics Plan.</p>		<p>and Code Enforcement.</p>	<p>contracts, and project plans.</p> <p>Review the Construction Noise and Vibration Logistics Plan.</p>	

MITIGATIONS	MONITORING AND REPORTING PROGRAM				
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	Method of Compliance Or Mitigation Action	Timing of Compliance	Oversight Responsibility	Actions/Reports	Monitoring Timing or Schedule
<ul style="list-style-type: none"> ○ Locate all stationary noise-generating equipment, such as air compressors and portable power generators, as far away as possible from adjoining noise-sensitive land uses. ○ Prohibit all unnecessary idling of internal combustion engines. ○ Notify all adjacent business, residences, and other noise-sensitive land uses of the construction schedule, in writing, and provide a written schedule of “noisy” construction activities to the adjacent land uses and nearby residences. ● The name and contact information (i.e., telephone number and email address) of the disturbance coordinator, who would be responsible for responding to complaints about construction noise, shall be posted at the construction site and included in the notice sent to neighboring noise-sensitive land uses regarding the construction schedule. 					
<p>MM NOI-1.2: Construction equipment: In addition to MM NOI-1.1, the project applicant shall include the following requirements in all construction documents, contracts, and project plans to reduce vibration impacts to nearby residences and structures during construction activities.</p> <ul style="list-style-type: none"> ● The contractor shall alert heavy equipment operators to the proximity of the adjacent structures so they can exercise care. 	All measures shall be printed on all construction documents, contracts, and project plans.	Prior to issuance of any grading permits.	Supervising Environmental Planner of the City of San José Department of Planning, Building, and Code Enforcement.	Ensure that all measures are printed on all construction documents, contracts, and project plans.	Prior to issuance of any grading permits.

MITIGATIONS	MONITORING AND REPORTING PROGRAM				
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	Method of Compliance Or Mitigation Action	Timing of Compliance	Oversight Responsibility	Actions/Reports	Monitoring Timing or Schedule
<ul style="list-style-type: none"> The contractor shall retain a qualified firm to complete a pre- and post-construction cosmetic crack survey of the buildings adjacent to the southern boundary and shall repair any cosmetic cracking that is reasonably determined to have occurred due to the construction, based on the recommendation of the qualified firm. Limit the use of heavy vibration-generating construction equipment within 30 feet of the northern and southern site boundaries. 					
Impact NOI-2: Mechanical equipment noise generated by the proposed project could impact nearby sensitive receptors.					
<p>MM NOI-2.1: Mechanical equipment selection: The project applicant shall select and design mechanical equipment that will reduce impacts on surrounding uses to comply with the City's 55 dBA DNL noise level requirement at the property boundary of the nearby noise-sensitive land uses. A qualified acoustical consultant shall be retained to review mechanical equipment noise levels prior to their installation to determine specific noise reduction measures necessary to comply with the City's noise level requirements. The results of the review shall be submitted to the Department of Planning, Building, and Code Enforcement along with the building plans and approved design, prior to issuance of any building permits. Noise reduction measures may include, but are not limited to, selection of equipment that emits low noise levels; and/or installation of noise barriers, such as enclosures and parapet walls, to block the line-</p>	<p>All measures shall be printed on all construction documents, contracts, and project plans.</p> <p>Prepare and implement a mechanical noise review and specific noise reduction measures.</p>	<p>Prior to issuance of any building permits and before and during all phases of construction.</p>	<p>Supervising Environmental Planner of the City of San José Department of Planning, Building, and Code Enforcement.</p>	<p>Ensure that all measures are printed on all construction documents, contracts, and project plans.</p> <p>Review/approve mechanical noise review and specific noise reduction measures.</p>	<p>Prior to issuance of any building permits.</p>

MITIGATIONS	MONITORING AND REPORTING PROGRAM				
	Documentation of Compliance [Project Applicant/Proponent Responsibility]		Documentation of Compliance [Lead Agency Responsibility]		
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<p>of-sight between the noise source and the nearest receptors. Alternate measures may include locating equipment in less noise-sensitive areas, such as the rooftop of the buildings away from the building's edge nearest the noise-sensitive receptors, where feasible. Alternate measures shall be reviewed by the Department of Planning, Building, and Code Enforcement prior to issuance of any building permits.</p>					

Source: City of San José. *Race Street and Grand Avenue Residential Development Initial Study / Environmental Assessment*. January 2018.

From: charlieprn@comcast.net [<mailto:charlieprn@comcast.net>]

Sent: Wednesday, January 31, 2018 3:05 PM

To: Le, Thai-Chau <Thai-Chau.Le@sanjoseca.gov>

Subject: Neighborhood petition

Mr. Mendrin:

I am sorry that I didn't include any rational. I thought it was enough to say that we the undersigned did not want this area to be rezoned. We are not against this project per se. We are NOT opposed to apartments being built on this site. We ARE opposed to the scope of this project.

- There is NO precedence for a 6 story building being built in a primarily single family area. Hanchette Park is a Historic Preservation area. This project comes too close to this area.
- We are asking that the scope of this project be kept to 3-4 stories. NO MORE! Thus NO on rezoning
- When we gathered signatures this past week-end, we explained to each person what the developer wanted to do with the said property. We also told them that the property would have to be rezoned to allow for this 6 story apartment. Every person who signed that petition said the same thing, "way to big for this area". "Why cant we keep it to 3-4 stories?"
- Traffic and parking are a VERY big issue for us that live near Race and Park. While a traffic study was done, there were several flaws. The intersection of Race and the Alameda was NEVER addressed. But the intersection of Sunol and San Carlos was.
- The intersection of Race and Park was given a D. If I got a did rating, I might as well kiss it good-bye. But the city thought it was ok.

So in summary. We would like the developer to come back with a new plan. One that limits the height and most likely limits the number of apartments. Of course, the developer is out to make money. The more apartments, the more money. While the land owners nearby suffer the consequences. I am sorry but I am not going to stand by and do nothing.

Sincerely,
Lee Charles

From: Mendrin, Shaunn [<mailto:shaunn.mendrin@sanjoseca.gov>]

Sent: Thursday, February 01, 2018 1:34 PM

To: charlieprn@comcast.net

Subject: RE: Neighborhood petition

Hello Lee,

My name is Shaunn Mendrin and I am the project planner for the Race Street project. Did you have an accompanying letter explaining why you are not in support of the project? I'm more than happy to forward the petition, but there really isn't any rationale as to why you are not in support. If you can get me something by Monday, that would be helpful for the Planning Commission to consider.

Thanks!
-Shaunn

Shaunn Mendrin | Supervising Planner
City of San Jose | Planning Division | PBCE
shaunn.mendrin@sanjoseca.gov | (408) 535-3885
200 E. Santa Clara Street, San Jose, CA 95113

From: Le, Thai-Chau
Sent: Wednesday, January 31, 2018 3:09 PM
To: charlieprn@comcast.net
Cc: Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>
Subject: FW: Neighborhood petition

Confirm that I have received the document. I will forward this to the project manager to be included in the public record and consideration.

Best regards,
Thai

Thai-Chau Le
Planner | City of San Jose
Environmental Planning
Planning, Building & Code Enforcement
Thai-Chau.Le@sanjoseca.gov
1.408.535.5658

From: charlieprn@comcast.net [<mailto:charlieprn@comcast.net>]
Sent: Wednesday, January 31, 2018 3:05 PM
To: Le, Thai-Chau <Thai-Chau.Le@sanjoseca.gov>
Subject: Neighborhood petition

Ms. Le
These signatures were obtained on Sunday, January 28, 2018. We had only 45 minutes to canvas our neighborhood.
Almost everyone that we approached wanted to sign their name to this petition.
Please consider this neighborhood's request.
Lee Charles
35 Tillman Ave
San Jose, CA 95126
charlieprn@comcast.net

From: Peter Ross [<mailto:pross@scu.edu>]
Sent: Wednesday, February 14, 2018 1:02 AM
To: Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>
Cc: Frank Farris <ffarris@scu.edu>
Subject: project file number PDC17-019

Shaunn,

I live at 195 South Morrison, two blocks from the proposed 6-story and 5-story residential buildings that would allow up to 206 dwelling units between Race St. and Grand Ave., just off of Park Ave. I received a PUBLIC HEARING NOTICE in the mail and have concerns about the proposed project, that I expressed

below. Does this email suffice for submitting them, or should I go to the Planning Commission Hearing at 6:30pm on Feb. 28 at City Hall to submit them?

Sincerely,
Peter Ross
408-279-4266

My concerns about the proposed project are mainly about parking and traffic.

On parking, will there be adequate off-road parking for the occupants of the 206 dwelling units? In 1988 I bought my house on South Morrison just off of Park, two blocks from Race St., when the average number of cars per house in my neighborhood seemed to be a little more than one car per home. But I'd estimate that nowadays the average number of cars per house is between two and three, and parking has gotten very tight, even with St. Leo's neighborhood parking permits required north of Race St. In addition, the recent completion of a bicycle lane on Park between Race and Sunol has made parking even tighter.

On traffic, Race St. between Park and San Carlos has become very congested at rush hour on weekdays, and adding several hundred vehicles there may lead to gridlock at times. In addition, Park Avenue is a major thoroughfare for bicyclists, with a bicycle lane that now goes from Market St. downtown all of the way to Newhall near the edge of Santa Clara, making it a safer alternative to Santa Clara Street and The Alameda for bicycle commuters. I've been biking or driving my car to Santa Clara University since the 1980s, and have serious doubts about Park Ave. and Race St. being comfortably able to handle the increase in traffic due to such large residential buildings as those in the proposed project.

We the undersigned are requesting that **REZONING NOT BE APPROVED** for the proposed project: File No. PDC17-019--253 Race Street, located between Race Street and Grand Avenue 300 feet southerly of Park Ave.

Marshall Ramey - 36 Tillman Ave, San Jose, CA 95126
Kathy Stormberg - 23 Tillman Ave. San Jose CA 95126
Kim Reiff - 40 Tillman Ave, San Jose CA 95126
Bambi Kite - 40 Tillman Ave, San Jose, CA 95126
Tonya Zietlow - 41 Tillman Ave, San Jose, CA 95126
Susan Jabro - 1299 Mariposa Ave, SJ 95126
Susan Krumpholtz - 1252 Mariposa Ave. SJ 95126
Robert Esser - 1180 Mariposa Ave SJ 95126
GARY LAWRENCE - 1164 Mariposa Ave SJ 95126
OLIVER GUNASKARA - 1145 MARIPOSA AVE SJ 95126
GARY ADLER - 1187 MARIPOSA AVE SJ 95126
Charles R Mowry - 1209 MARIPOSA AVE SJ CA 95126
Richard [unclear] - 1301 MARIPOSA AVE, SJ, CA 95126
Robert J. Worman - 1301 MARIPOSA AVE, SJ, CA 95126

Signatures were obtained on Sunday, January 28, 2018
We had only 45 minutes to canvas our neighborhood
Almost Everyone that we approached signed this
petition

Please consider this !!!

Lee Charles
35 Tillman Ave
SJ 95126

From: Marie Phillips [mailto:mariephillips310@gmail.com]
Sent: Monday, February 19, 2018 3:38 PM
To: Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>
Subject: PDC17-019

Hello Shaun,

Regarding PDC17-019. I know this area's development projects have increased their height and density. I am opposed to both the density of the neighborhood and the height increase. I am opposed for the following reasons:

- this will add to already significant traffic and parking problems in the neighborhood
- this project will cast a shadow on my neighborhood near Parkinson Court and Lincoln Ave.
- the privacy of Parkinson Court and Lincoln neighborhood will be affected by this proposed development.

Please take my comments into account in your development projects.

Sincerely,
Marie Phillips

CATALYZE SV

October 14, 2017

The Core Companies
470 South Market Street
San Jose, CA 95113

RE Proposed Affordable and Senior Affordable Housing on Race Street
APN: 261-42-007, 008, 011, 058, 069, 070, 071, 079

Dear Core Companies,

Thank you for presenting information to Catalyze SV regarding the proposed Core Companies development on the 2.3-acre site on Race Street between Park Avenue and San Carlos Avenue. We appreciate the opportunity to learn more about this development and to provide you with our thoughts on the current proposal.

Based on the information Core presented to us, Catalyze SV understands the current proposal includes 91 senior apartments in a 5-story (4 floors living over 1 floor parking) building and 116 family apartments in a 6-story (4 floors living over 2 floors parking) building. Both buildings provide generous common open space in the form of courtyards and indoor amenities. The buildings are separated by a promenade that also serves as a driveway to access both buildings' garages. The site is in a transit-rich neighborhood and within walking distance of two vibrant commercial/retail corridors.

Catalyze SV is pleased to support Core's current proposal in large part because of the substantial number of affordable housing units it provides by capitalizing on the density allowed on the site. Santa Clara County, the Bay Area, and California at large are experiencing a housing crisis of epic proportions. Catalyze SV believes strongly in addressing this problem through smart urban development that includes affordable housing. While we believe there are improvements that could be made to Core's proposal, we also believe that our community needs to see many more projects like this one to make a dent in the housing crisis.

We endorse the following decisions Core has made in its current proposal:

- a 100% affordable project,
- a density of approximately 90 units per acre,
- the integration of senior and family apartments on one site,
- the inclusion of community courtyards and a paseo on site, and

- minimal setbacks for the entire site.

While we believe this project will provide great benefit to the community, we do believe that there are some reasonable changes that would improve the proposal. In particular, Catalyze SV believes that retail commercial would be appropriate for this site, which is ideally situated to act as a bridge between retail located on The Alameda and San Carlos Street. We hope that Core will seriously consider financing opportunities that would enable the inclusion of retail to provide community benefits while actively engaging the street front along Race Street.

Additionally, Catalyze SV would like to see this proposal reconsider some of its transit decisions.

- While we strongly support the parking ratio of under 1.0 for the senior apartments, we believe the parking ratio for the family apartments should be under 1.5 for all unit types.
- We hope to see Core consider further incentives for car-free travel for future residents, including bike features (bike kitchen, shared bikes, and/or quality bike lockers) and providing VTA passes for the nearby bus lines.
- Finally, we strongly support Core's consideration of re-orienting the promenade to provide vehicle entry on Grand Avenue. We believe this would result in a more pedestrian-friendly promenade.

Thank you again for sharing this proposal with Catalyze SV. We look forward to remaining engaged with the project, supporting Core in making decisions consistent with Catalyze SV's mission, and seeing this project come to fruition. If you have any questions, concerns, or need further clarification on any of the above comments, please reach out to us by contacting Catalyze SV Advocacy Committee Chair Michael Branson at michael.branson@gmail.com and/or Catalyze SV Co-Founder Alex Shoor at alexshoor@gmail.com.

Sincerely,

Catalyze SV's Advocacy Committee

Cc: San Jose Planning Department; Office of the Mayor, Office of Councilmember Dev Davis

About Catalyze SV

Catalyze SV's mission is to engage community members, developers and city leaders to envision and create sustainable, equitable and vibrant places for people in Silicon Valley. Our Community Engagement Committee encourages a more inclusive and collaborative community engagement process around new development. Our Advocacy Committee identifies, vets and leads advocacy efforts around, specific development projects.

RACE STREET RESIDENTIAL

SAN JOSE, CALIFORNIA

PROJECT DATA

OVERALL SITE

APN: 261-42-007, 008, 011, 058, 069, 070, 071, 079

Site Area: 2.3 acre (gross)

Number of Units Proposed (Two Parcels Combined): 206 du

Density (Two Parcels Combined): 89.56 du/ac

Coverage:	Building	74,280 sf	74%
	Pavement	2,700 sf	3%
	Landscape	23,208 sf	23%

PARCEL A APARTMENTS

Site Area: 0.92 ac

Number of Units: 90

Parking Ratio:

(82) Studios & 1-Bedrooms @ 0.57 stall/unit = 46.74 stalls
 (8) 2-Bedrooms @ 1.7 stall/unit = 13.6 stalls
 Total Stalls Required = 60.34
 Total Stalls Provided = 61

Setback: North 14'; East (Race St.) 4'; South 40' to other buildings; West (Grand Ave.) 12'-5"

Height: 80 feet

Common Open Space: 100 sf/du

Private Open Space: 60 sf/du (for up to 50% of the units)

PARCEL B APARTMENTS

Site Area: 1.38 ac

Number of Units: 116

Parking Ratio: Per Zoning Ordinance
 1.25 for Studios & 1BR; 1.7 for 2BR; 2.0 for 3BR

Setback: North 40' to other buildings; East (Race St.) 4'; South 16'; West (Grand Ave.) 11'- 0"

Height: 80 feet

Common Open Space: 100 sf/du

Private Open Space: 60 sf/du (for up to 50% of the units)



AERIAL MAP/PROJECT LOCATION



VICINITY MAP

PROJECT DIRECTORY

DEVELOPER

Core Race Street LLC
 470 S. Market Street
 San Jose, CA 95113
 Chris@theCoreCompanies.com 408-292-7416

ARCHITECT

LPMD Architects
 1288 Kifer Road, Ste.206
 Sunnyvale, CA 94086
 Anthony@LPMD-Architects.com 408-859-2845

CIVIL ENGINEER

JMH Weiss
 101 Metro Drive, Ste. 360
 San Jose, CA 95110
 djedwards@JMHW.com 408-315-2084

LANDSCAPE ARCHITECT

Isaacson Wood & Associate
 35802 Hibiscus Court
 Fremont, CA 94536
 jay@IsaacsonWood.com 408-838-2329

SHEET INDEX

- 1 Cover Sheet
- 2.0 Land Use Plan
- 3.0 Development Standards
- 3.1 Development Standards
- C1.0 Existing Condition
- C1.1 Demolition Plan
- C2.0 Preliminary Grading Drainage & Utility Plan
- C2.1 Preliminary Grading Drainage & Utility Plan
- C3.0 Preliminary Stormwater Control Plan
- C3.1 Stormwater Control Plan Notes & Details
- C4.0 Details & Sections
- 7.1 Conceptual Height Diagram
- 9.1 Conceptual First Floor / Site Plan
- 9.2 Conceptual Upper Floor Plans
- L1 Conceptual Overall Landscape Plan
- L2 Conceptual Parcel B Apartments Podium Courtyard Landscape Plan
- L3 Conceptual Parcel A Apartments Podium Courtyard Landscape Plan
- L4 Conceptual Plant Palette & Irrigation Key
- L5 Conceptual Irrigation Details
- L6 Conceptual Irrigation Details
- L7 Conceptual Planting Details

PROJECT DESCRIPTION

Demolish 12 existing structures on the 2.3-acre site and construct 206 residential apartment units in two buildings up to 80 feet tall, with at-grade parking at the lower level(s).

RACE STREET
 SAN JOSE - CALIFORNIA
 PDC 17-019

Date: 09-19-2017 Drawing Title:
 Scale: _____
 Revisions: _____ Sheet No: _____

 _____ of _____ Sheets


COVER SHEET


1

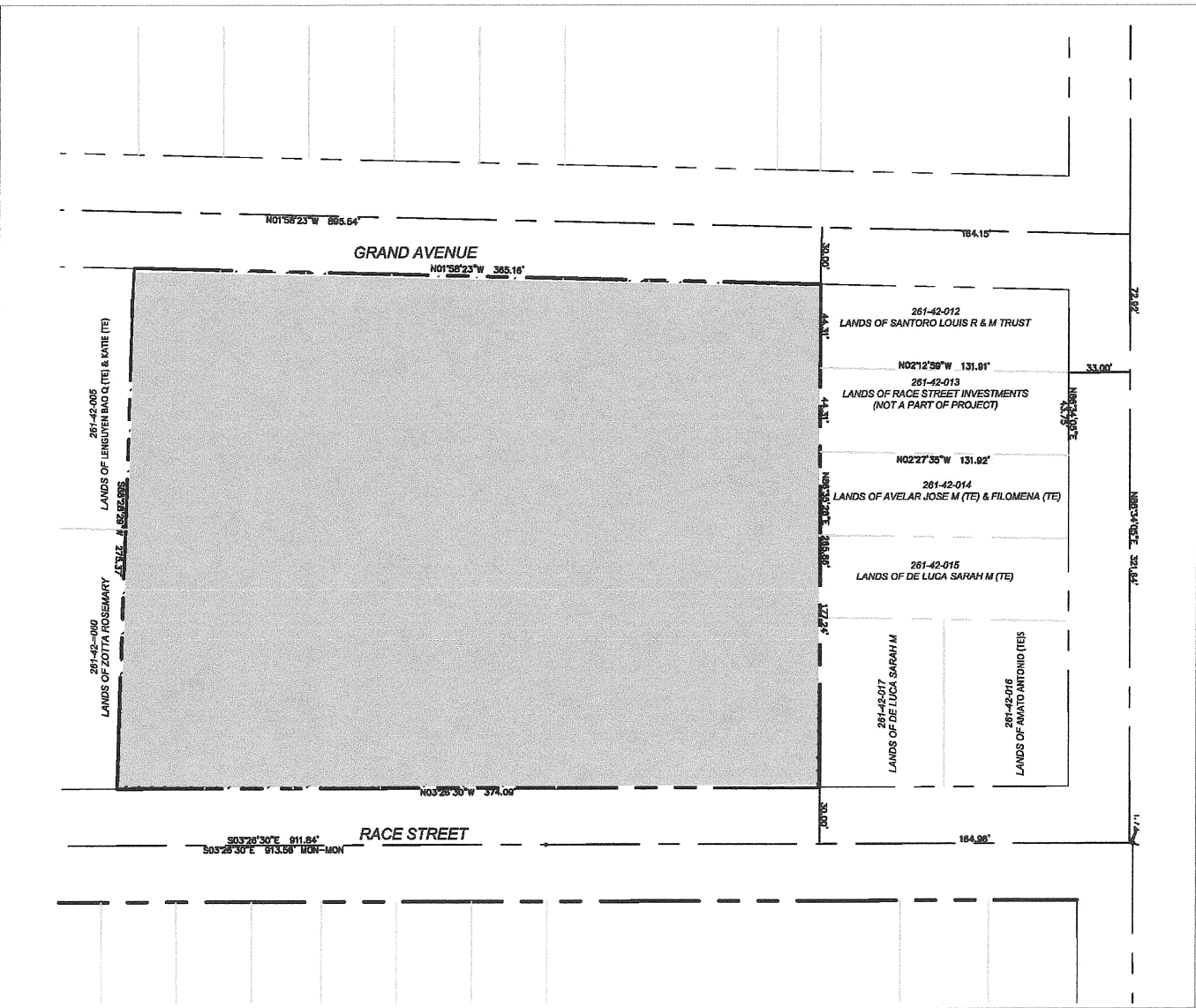
LPMD
Architects
 1288 Kifer Road, Unit 206,
 Sunnyvale, CA 94086
 Telephone : 408-992-0280
 Fax : 408-992-0281

LEGEND

LAND USE AREAS

MULTI-FAMILY RESIDENTIAL 

ZONING R-M (PD) 



1 LAND USE PLAN

RACE STREET
 SAN JOSE - CALIFORNIA
 PDC 17-019

Date:	02-06-2017	Drawing Title:	LAND USE PLAN
Scale:		Sheet No.:	2.0
Revisions:		of	Sheets

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RACE STREET
SAN JOSE - CALIFORNIA
PDC 17-019

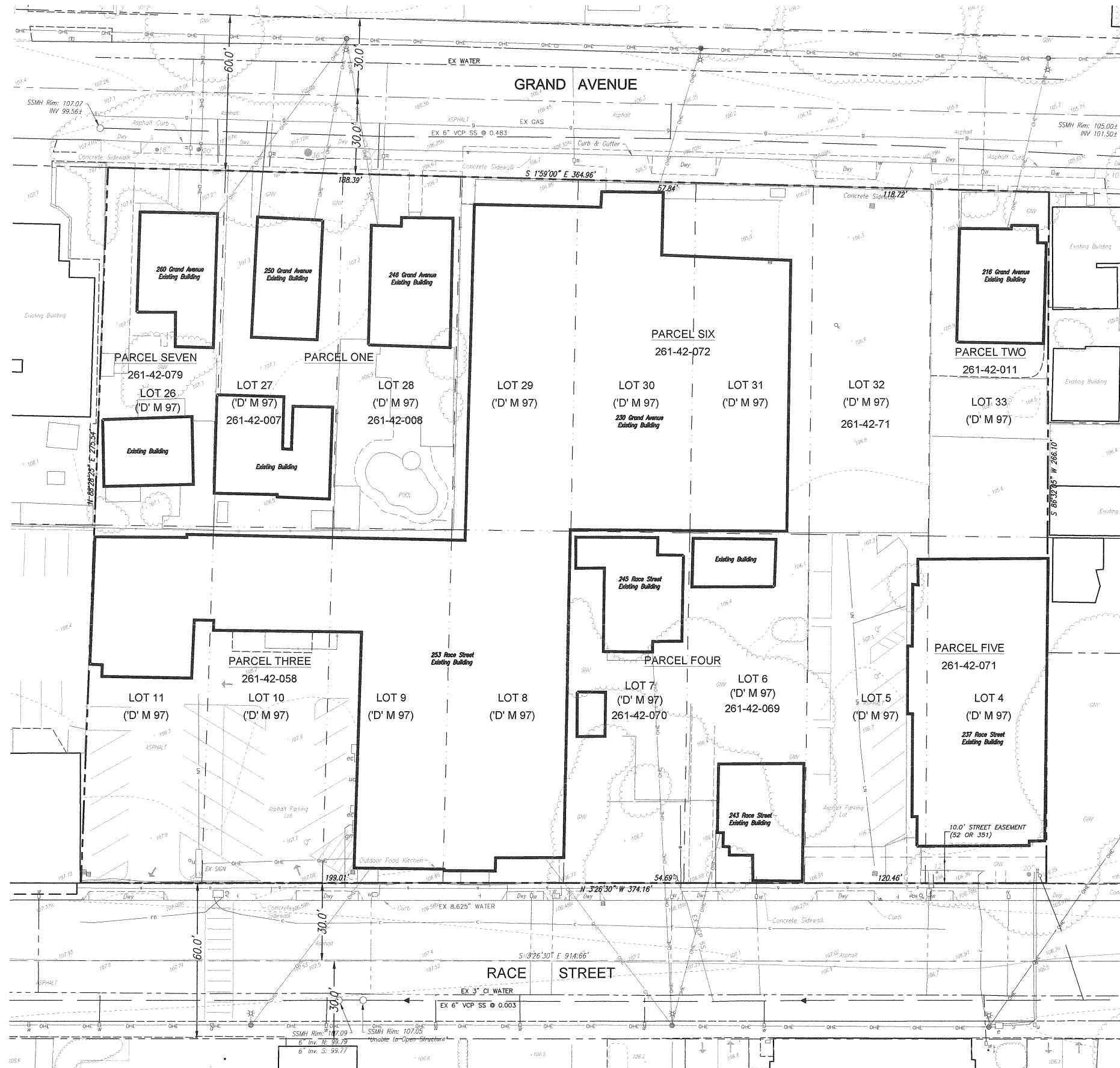
Date: 09-19-2017 Drawing Title:
Scale: DEVELOPMENT
STANDARDS
Revisions: Sheet No:
3.0
of Sheets

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Date: 09-19-2017 Drawing Title:
Scale: DEVELOPMENT
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SITE ADDRESS
 216, 230, 246, 250, 260 Grand Avenue
 and 237, 243, 245, 253 Race Street

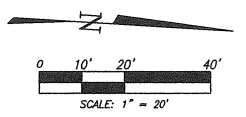
NOTE
 ALL FEATURES SHOWN HEREON REPRESENT SURFACE CONDITIONS OF THE PROJECT AREA COMPILED FROM AN AERIAL TOPOGRAPHIC SURVEY FLOWN ON MARCH 12, 2017.

BASIS OF BEARINGS
 BEARINGS SHOWN ON THIS MAP ARE BASED UPON THE MONUMENT LINE OF RACE STREET AS SHOWN ON THAT CERTAIN RECORD OF SURVEY FILED FOR RECORD IN BOOK 624 OF MAPS AT PAGE 19 IN THE OFFICE OF THE SANTA CLARA RECORDER. BEARING TAKEN AS NORTH 3°26'30" WEST BETWEEN FOUND MONUMENTS SHOWN HEREON.

SITE BENCHMARK
 THE BENCHMARK FOR THIS SURVEY IS CITY OF SAN JOSE BENCHMARK 498-D, THE LETTER "S" IN THE WORD "PRODUCTS" ON THE CATCH BASIN LOCATED AT THE CENTER OF THE NORTHWEST CURB RETURN AT THE INTERSECTION OF SAN CARLOS STREET AND RACE STREET. ELEVATION TAKEN AS 107.99, NGVD29.

LEGEND & ABBREVIATIONS

	EXISTING BUILDING
	PROPERTY LINE - SUBJECT PARCEL
	PROPERTY LINE - ADJACENT PARCEL
	PROPERTY LINE - TO BE REMOVED
	CENTERLINE / MONUMENT LINE, AS NOTED
	EASEMENT LINE
	CONCRETE
	CURB & CUTTER
	FENCELINE
	OVERHEAD UTILITY LINE
	UNDERGROUND COMMUNICATIONS LINE
	UNDERGROUND GAS LINE
	UNDERGROUND SANITARY SEWER LINE
	UNDERGROUND STORM DRAIN LINE
	UNDERGROUND RECLAIMED WATER LINE
	UNDERGROUND WATER LINE
	WALL
	BIKE RACK
	COMMUNICATIONS BOX
	COMMUNICATIONS VAULT
	CATCH BASIN
	ELECTRIC UTILITY BOX
	ELECTRIC VAULT
	FIRE HYDRANT
	GAS VALVE
	IRRIGATION BOX
	STREET LIGHT
	SANITARY SEWER CLEANOUT
	SANITARY SEWER MANHOLE
	SIGN
	SPOT ELEVATION
	STORM DRAIN CLEANOUT
	STORM DRAIN UTILITY MANHOLE
	STREET LIGHT BOX
	TRAFFIC SIGNAL BOX
	TREE TRUNK / SIZE (SYMBOL NOT INDICATIVE OF DRIFLINE LOCATION)
	UTILITY POLE
	UTILITY POLE W/LIGHT
	UTILITY VAULT - TYPE UNKNOWN
	WATER VALVE



RACE STREET

SAN JOSE - CALIFORNIA

Date: 12-05-2017 Drawing Title: EXISTING CONDITIONS
 Scale: AS-SHOWN
 Revisions: _____ Sheet No: _____
 _____ C1.0
 _____ of _____ Sheets

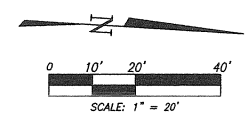
JMH WEISS, INC.
 Civil Engineering - Surveying - Land Planning
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DWG NAME: P:\5100 - Race Street - San Jose\5100\Engineering\Engineering Plans\Planning\5100 1.0 Existing Condition.dwg, LAST EDITED: Tue, Dec 05, 2017 11:16am
 USER: Itron, AutoCad V.20.0a (LMS Tech), Microsoft Windows NT Version 6.1 (x64)



LEGEND & ABBREVIATIONS

	EXISTING BUILDING
	PROPERTY LINE - SUBJECT PARCEL
	PROPERTY LINE - ADJACENT PARCEL
	PROPERTY LINE - TO BE REMOVED
	CENTERLINE / MONUMENT LINE, AS NOTED
	EASEMENT LINE
	CONCRETE
	CURB & GUTTER
	FENCELINE
	UNDERGROUND COMMUNICATIONS LINE
	UNDERGROUND GAS LINE
	UNDERGROUND SANITARY SEWER LINE
	UNDERGROUND STORM DRAIN LINE
	UNDERGROUND RECLAIMED WATER LINE
	UNDERGROUND WATER LINE
	WALL
	BIKE RACK
	COMMUNICATIONS BOX
	COMMUNICATIONS VAULT
	CATCH BASIN
	ELECTRIC UTILITY BOX
	ELECTRIC VAULT
	FIRE HYDRANT
	GAS VALVE
	IRRIGATION BOX
	STREET LIGHT
	SANITARY SEWER CLEANOUT
	SANITARY SEWER MANHOLE
	SIGN
	SPOT ELEVATION
	STORM DRAIN CLEANOUT
	STORM DRAIN MANHOLE
	STREET LIGHT BOX
	TRAFFIC SIGNAL BOX
	TREE TRUNK / SIZE (SYMBOL NOT INDICATIVE OF DIAPHRAGM LOCATION)
	UTILITY POLE
	UTILITY POLE W/LIGHT
	UTILITY VAULT - TYPE UNKNOWN
	WATER VALVE
	TO BE REMOVED
	TO REMAIN



RACE STREET

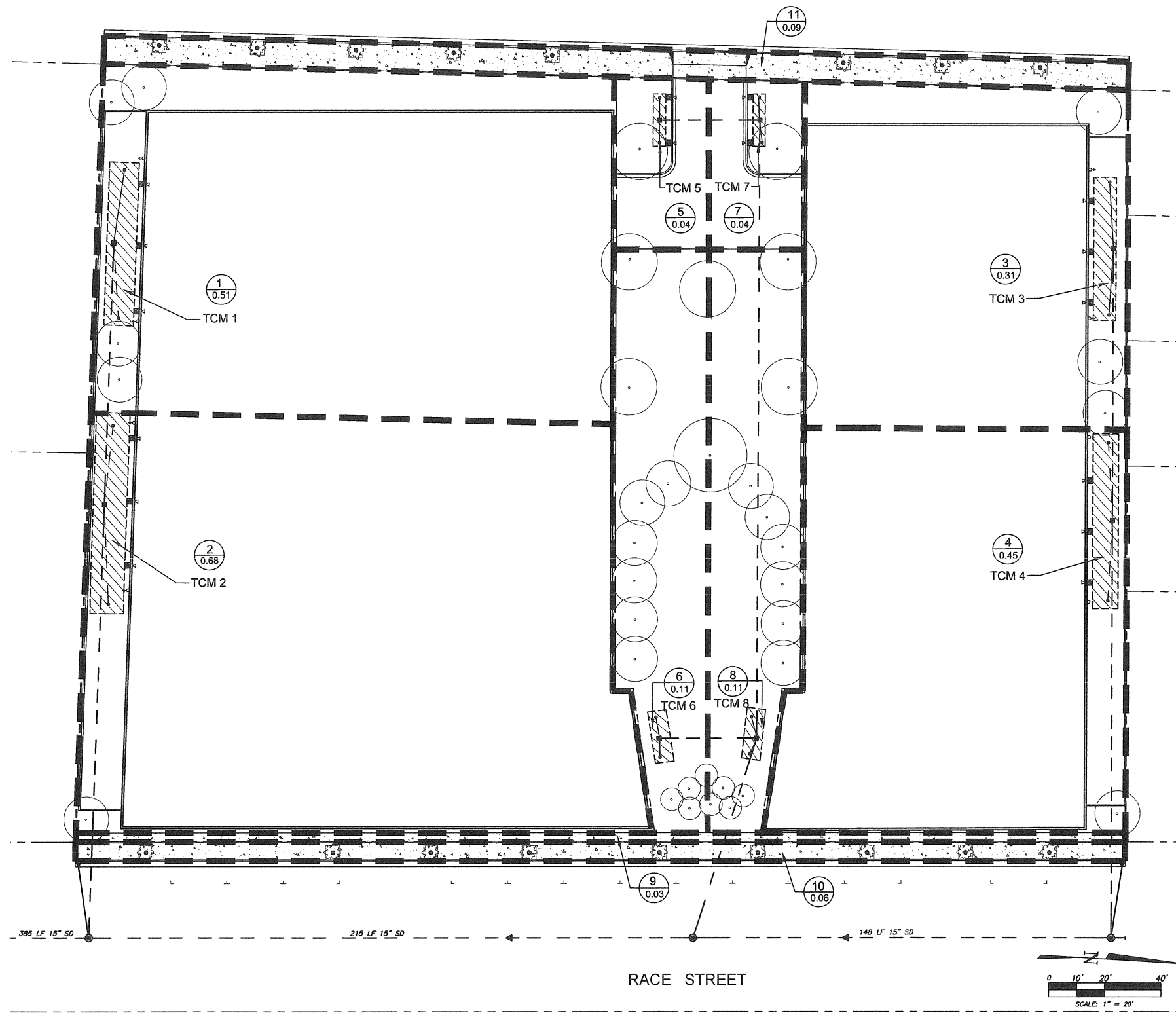
SAN JOSE - CALIFORNIA

Date:	12-05-2017	Drawing Title:	DEMOLITION PLAN
Scale:	AS-SHOWN	Sheet No.:	C1.1
Revisions:		of	Sheets

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DWG NAME: P:\5100 - Race Street - San Jose\5100\Engineering\Engineering Plans\Planning\5100 1.1 Demo Plan.dwg, LAST EDITED: Tue, Dec 05, 2017 11:18am
 USER: Itron, AutoCad V.20.06 (LMS Tech), Microsoft Windows NT Version 6.1 (646)

GRAND AVENUE



PERVIOUS AND IMPERVIOUS SURFACES COMPARISON TABLE			
		PROJECT PHASE NUMBER: ONE (1)	
TOTAL SITE (SQUARE FOOT):	99,915	TOTAL AREA OF SITE DISTURBED (ACRES):	2.30
IMPERVIOUS SURFACES	EXISTING CONDITION OF SITE AREA DISTURBED (SQUARE FEET)	PROPOSED CONDITION OF SITE AREA DISTURBED (SQUARE FEET)	
		REPLACED	NEW
ROOF AREA(S)	46,572	70,350	3,830
PARKING	43,330	0	0
SIDEWALKS, PATIOS, PATHS, ETC	0	320	0
STREETS (PUBLIC)	0	1,245	0
STREETS (PRIVATE)	0	2,700	0
TOTAL IMPERVIOUS SURFACES:	83,036	74,615	3,830
PERVIOUS SURFACES			
LANDSCAPED AREAS	16,879	12,949	8,421
PERVIOUS PAVERS	0	0	0
OTHER PERVIOUS SURFACES (GREEN ROOF, ETC.)	0	0	0
TOTAL PERVIOUS SURFACES:	16,879	12,949	8,421
TOTAL PROPOSED REPLACED + NEW IMPERVIOUS SURFACES:		78,545	
TOTAL PROPOSED REPLACED + NEW PERVIOUS SURFACES:		21,370	

TREATMENT CONTROL MEASURE (TCM) SUMMARY TABLE															
DMA	TCM No.	Landscape Area (sf)	Roof Area (sf)	(Private)	Impervious Sidewalk / Parking Area (sf)	Total Area (sf)	Total Area (acres)	Total Impervious Area (sf)	Treatment Type	Treatment Area Required (sf)	Treatment Area Provided (sf)	Req'd Ponding Depth (in)	Provided Ponding Depth (in)	Treatment Sizing	Lined or Unlined
1	1	3,865	18,340	0	0	22,205	0.51	18,340	Bio-Retention	603	610	6.0	6.0	Flow-Combo	Lined
2	2	1,845	27,920	0	0	29,765	0.68	27,920	Bio-Retention	838	850	6.0	6.0	Flow-Combo	Lined
3	3	2,560	11,040	0	0	13,600	0.31	11,040	Bio-Retention	367	390	6.0	6.0	Flow-Combo	Lined
4	4	2,434	16,980	0	0	19,414	0.45	16,980	Bio-Retention	535	560	6.0	6.0	Flow-Combo	Lined
5	5	575	0	1,350	0	1,925	0.04	1,350	Bio-Retention	50	80	6.0	6.0	Flow-Combo	Unlined
6	6	4,758	0	0	160	4,918	0.11	160	Bio-Retention	30	50	6.0	6.0	Flow-Combo	Lined
7	7	575	0	1,350	0	1,925	0.04	1,350	Bio-Retention	50	80	6.0	6.0	Flow-Combo	Unlined
8	8	4,758	0	0	160	4,918	0.11	160	Bio-Retention	30	50	6.0	6.0	Flow-Combo	Lined
9	-	0	0	0	1,245	1,245	0.03	1,245	Road Project **	-	-	-	-	-	-
10	-	175	0	0	2,510	2,685	0.06	2,510	Maintenance	-	-	-	-	-	-
11	-	250	0	0	3,655	3,905	0.09	3,655	Maintenance	-	-	-	-	-	-
Total Area		21,795	74,280	2,700	7,730	106,505	2.45	84,710		2,593	2,670				

** New pavement not creating a travel lane and new sidewalk created on an existing street are exempt from the C.3 requirements for public roadway projects per Table 2-2 of Chapter 2.3 of the C.3 Stormwater Control Handbook (SCVURPPP, June 2016).

TABLE 1 ROUTINE MAINTENANCE ACTIVITIES FOR BIORETENTION AREAS		
NO.	MAINTENANCE TASK	FREQUENCY OF TASK
1	REMOVE OBSTRUCTIONS, WEEDS, DEBRIS AND TRASH FROM BIORETENTION AREA AND ITS INLETS AND OUTLETS; AND DISPOSE OF PROPERLY.	QUARTERLY, OR AS NEEDED AFTER STORM EVENTS
2	INSPECT BIORETENTION AREA FOR STANDING WATER. IF STANDING WATER DOES NOT DRAIN WITHIN 2-3 DAYS, TILL AND REPLACE THE SURFACE BIOTREATMENT SOIL WITH THE APPROVED SOIL MIX AND REPLANT.	QUARTERLY, OR AS NEEDED AFTER STORM EVENTS
3	CHECK UNDERDRAINS FOR CLOGGING. USE THE CLEANOUT RISER TO CLEAN ANY CLOGGED UNDERDRAINS.	QUARTERLY, OR AS NEEDED AFTER STORM EVENTS
4	MAINTAIN THE IRRIGATION SYSTEM AND ENSURE THAT PLANTS ARE RECEIVING THE CORRECT AMOUNT OF WATER (IF APPLICABLE).	QUARTERLY
5	ENSURE THAT THE VEGETATION IS HEALTHY AND DENSE ENOUGH TO PROVIDE FILTERING AND PROTECT SOILS FROM EROSION. PRUNE AND WEED THE BIORETENTION AREA. REMOVE AND/OR REPLACE ANY DEAD PLANTS.	ANNUALLY, BEFORE THE WET SEASON BEGINS
6	USE COMPOST AND OTHER NATURAL SOIL AMENDMENTS AND FERTILIZERS INSTEAD OF SYNTHETIC FERTILIZERS, ESPECIALLY IF THE SYSTEM USES AN UNDERDRAIN.	ANNUALLY, BEFORE THE WET SEASON BEGINS
7	CHECK THAT MULCH IS AT APPROPRIATE DEPTH (2 - 3 INCHES PER SOIL SPECIFICATIONS) AND REPLENISH AS NECESSARY BEFORE WET SEASON BEGINS. IT IS RECOMMENDED THAT 2" - 3" OF ARBOR MULCH BE REAPPLIED EVERY YEAR.	ANNUALLY, BEFORE THE WET SEASON BEGINS
8	INSPECT THE ENERGY DISSIPATION AT THE INLET TO ENSURE IT IS FUNCTIONING ADEQUATELY, AND THAT THERE IS NO SCOUR OF THE SURFACE MULCH. REMOVE ACCUMULATED SEDIMENT.	ANNUALLY, BEFORE THE WET SEASON BEGINS
9	INSPECT OVERFLOW PIPE TO ENSURE THAT IT CAN SAFELY CONVEY EXCESS FLOWS TO A STORM DRAIN. REPAIR OR REPLACE DAMAGED PIPING. REPLACE BIOTREATMENT SOIL AND MULCH, IF NEEDED. CHECK FOR STANDING WATER, STRUCTURAL FAILURE AND CLOGGED OVERFLOWS. REMOVE TRASH AND DEBRIS. REPLACE DEAD PLANTS.	ANNUALLY, BEFORE THE WET SEASON BEGINS
11	INSPECT BIORETENTION AREA USING THE ATTACHED INSPECTION CHECKLIST.	ANNUALLY, BEFORE THE WET SEASON

RACE STREET

SAN JOSE - CALIFORNIA

Date: 12-05-2017 Drawing Title:
 Scale: AS-SHOWN PRELIMINARY STORMWATER CONTROL PLAN
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 C3.0
 of Sheets

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SIZING FOR VOLUME BASED TREATMENT	
DMA #	1
A=	22205 s.f.
Impervious Area =	18340 s.f.
% Imperviousness=	82.59%
MAPsite =	14.5
MAPpage =	13.9
Correction Factor=	1.04317
Clay (D):	Sandy Clay (D): Clay Loam (D):
Silt Loam/Loam (B):	X Not Applicable (100% Impervious):
Are the soils outside the building footprint not graded/compacted? <input type="checkbox"/> No <input type="checkbox"/> Yes/No	
If no, and the soil will be compacted during site preparation and grading, the soils infiltration ability will be decreased. Modify your answer to a soil with a lower infiltration rate (eg. Silt Loam to Clay)	
Modified Soil Type:	D
S=	1.00%
UBS Volume for 1% Slope (UBS1%) =	0.483048638 inches (Use Figure B-2)
UBS Volume for 15% Slope (UBS15%) =	0.498000901 inches (Use Figure B-5)
UBS Volume for X% Slope (UBSX%) =	0.483048638 inches (Corrected Slope for the site)
Adjusted UBS =	Correction Factor (Step 2) x UBSx% (Step 5)
Adjusted UBS =	0.50389658 inches
Design Volume =	Adjusted UBS (Step 6) x Drainage Area (Step 1) x 1ft/12inch
Design Volume =	932.42 ft ³
COMBO FLOW & VOLUME BIOTRETENTION CALCULATION	
Total Drainage Area =	22,205 sq. ft
Impervious Area =	18,340 sq. ft
Pervious Area =	3,865 sq. ft
Equivalent Impervious Area =	387 sq. ft
Total Equivalent Impervious =	18,727 sq. ft
Rainfall Intensity =	0.2 in/hr
Duration =	Adjusted UBS (Step 6) / Rainfall Intensity
Duration =	2.5194829 hrs
Estimate the Surface Area =	603 sq. ft (Typically start with Total Impervious x 0.03)
Volume of Treated Runoff =	633.023945 cu. ft
Volume in Ponding Area =	299.40038 cu. ft
Depth of Ponding =	0.49651804 ft
Depth of Ponding =	6 inches (Round up)
If Depth of Ponding is less than 6" the design can be optimized with a smaller surface area. (repeat)	
If Depth of Ponding is greater than 12" a larger surface area will be required (repeat)	
If Depth of Ponding is between 6" to 12" this is the range allowable for bioretention of flow through planters.	

SIZING FOR VOLUME BASED TREATMENT	
DMA #	2
A=	29765 s.f.
Impervious Area =	27920 s.f.
% Imperviousness=	93.80%
MAPsite =	14.5
MAPpage =	13.9
Correction Factor=	1.04317
Clay (D):	Sandy Clay (D): Clay Loam (D):
Silt Loam/Loam (B):	X Not Applicable (100% Impervious):
Are the soils outside the building footprint not graded/compacted? <input type="checkbox"/> No <input type="checkbox"/> Yes/No	
If no, and the soil will be compacted during site preparation and grading, the soils infiltration ability will be decreased. Modify your answer to a soil with a lower infiltration rate (eg. Silt Loam to Clay)	
Modified Soil Type:	D
S=	1.00%
UBS Volume for 1% Slope (UBS1%) =	0.545474047 inches (Use Figure B-2)
UBS Volume for 15% Slope (UBS15%) =	0.563676466 inches (Use Figure B-5)
UBS Volume for X% Slope (UBSX%) =	0.545474047 inches (Corrected Slope for the site)
Adjusted UBS =	Correction Factor (Step 2) x UBSx% (Step 5)
Adjusted UBS =	0.569019689 inches
Design Volume =	Adjusted UBS (Step 6) x Drainage Area (Step 1) x 1ft/12inch
Design Volume =	1,411.41 ft ³
COMBO FLOW & VOLUME BIOTRETENTION CALCULATION	
Total Drainage Area =	29,765 sq. ft
Impervious Area =	27,920 sq. ft
Pervious Area =	1,845 sq. ft
Equivalent Impervious Area =	185 sq. ft
Total Equivalent Impervious =	28,105 sq. ft
Rainfall Intensity =	0.2 in/hr
Duration =	Adjusted UBS (Step 6) / Rainfall Intensity
Duration =	2.845098445 hrs
Estimate the Surface Area =	838 sq. ft (Typically start with Total Impervious x 0.03)
Volume of Treated Runoff =	993.41354 cu. ft
Volume in Ponding Area =	417.99238 cu. ft
Depth of Ponding =	0.49879759 ft
Depth of Ponding =	6 inches (Round up)
If Depth of Ponding is less than 6" the design can be optimized with a smaller surface area. (repeat)	
If Depth of Ponding is greater than 12" a larger surface area will be required (repeat)	
If Depth of Ponding is between 6" to 12" this is the range allowable for bioretention of flow through planters.	

SIZING FOR VOLUME BASED TREATMENT	
DMA #	3
A=	13600 s.f.
Impervious Area =	11040 s.f.
% Imperviousness=	81.18%
MAPsite =	14.5
MAPpage =	13.9
Correction Factor=	1.04317
Clay (D):	Sandy Clay (D): Clay Loam (D):
Silt Loam/Loam (B):	X Not Applicable (100% Impervious):
Are the soils outside the building footprint not graded/compacted? <input type="checkbox"/> No <input type="checkbox"/> Yes/No	
If no, and the soil will be compacted during site preparation and grading, the soils infiltration ability will be decreased. Modify your answer to a soil with a lower infiltration rate (eg. Silt Loam to Clay)	
Modified Soil Type:	D
S=	1.00%
UBS Volume for 1% Slope (UBS1%) =	0.475152941 inches (Use Figure B-2)
UBS Volume for 15% Slope (UBS15%) =	0.489694118 inches (Use Figure B-5)
UBS Volume for X% Slope (UBSX%) =	0.475152941 inches (Corrected Slope for the site)
Adjusted UBS =	Correction Factor (Step 2) x UBSx% (Step 5)
Adjusted UBS =	0.49566314 inches
Design Volume =	Adjusted UBS (Step 6) x Drainage Area (Step 1) x 1ft/12inch
Design Volume =	561.75 ft ³
COMBO FLOW & VOLUME BIOTRETENTION CALCULATION	
Total Drainage Area =	13,600 sq. ft
Impervious Area =	11,040 sq. ft
Pervious Area =	2,560 sq. ft
Equivalent Impervious Area =	256 sq. ft
Total Equivalent Impervious =	11,296 sq. ft
Rainfall Intensity =	0.2 in/hr
Duration =	Adjusted UBS (Step 6) / Rainfall Intensity
Duration =	2.4783157 hrs
Estimate the Surface Area =	367 sq. ft (Typically start with Total Impervious x 0.03)
Volume of Treated Runoff =	378.975776 cu. ft
Volume in Ponding Area =	182.775783 cu. ft
Depth of Ponding =	0.49802666 ft
Depth of Ponding =	6 inches (Round up)
If Depth of Ponding is less than 6" the design can be optimized with a smaller surface area. (repeat)	
If Depth of Ponding is greater than 12" a larger surface area will be required (repeat)	
If Depth of Ponding is between 6" to 12" this is the range allowable for bioretention of flow through planters.	

SIZING FOR VOLUME BASED TREATMENT	
DMA #	4
A=	19414 s.f.
Impervious Area =	16980 s.f.
% Imperviousness=	87.46%
MAPsite =	14.5
MAPpage =	13.9
Correction Factor=	1.04317
Clay (D):	Sandy Clay (D): Clay Loam (D):
Silt Loam/Loam (B):	X Not Applicable (100% Impervious):
Are the soils outside the building footprint not graded/compacted? <input type="checkbox"/> No <input type="checkbox"/> Yes/No	
If no, and the soil will be compacted during site preparation and grading, the soils infiltration ability will be decreased. Modify your answer to a soil with a lower infiltration rate (eg. Silt Loam to Clay)	
Modified Soil Type:	D
S=	1.00%
UBS Volume for 1% Slope (UBS1%) =	0.510166993 inches (Use Figure B-2)
UBS Volume for 15% Slope (UBS15%) =	0.526531163 inches (Use Figure B-5)
UBS Volume for X% Slope (UBSX%) =	0.510166993 inches (Corrected Slope for the site)
Adjusted UBS =	Correction Factor (Step 2) x UBSx% (Step 5)
Adjusted UBS =	0.53218859 inches
Design Volume =	Adjusted UBS (Step 6) x Drainage Area (Step 1) x 1ft/12inch
Design Volume =	860.99 ft ³
COMBO FLOW & VOLUME BIOTRETENTION CALCULATION	
Total Drainage Area =	19,414 sq. ft
Impervious Area =	16,980 sq. ft
Pervious Area =	2,434 sq. ft
Equivalent Impervious Area =	243 sq. ft
Total Equivalent Impervious =	17,223 sq. ft
Rainfall Intensity =	0.2 in/hr
Duration =	Adjusted UBS (Step 6) / Rainfall Intensity
Duration =	2.660942949 hrs
Estimate the Surface Area =	536 sq. ft (Typically start with Total Impervious x 0.03)
Volume of Treated Runoff =	593.168532 cu. ft
Volume in Ponding Area =	267.823908 cu. ft
Depth of Ponding =	0.50060544 ft
Depth of Ponding =	6 inches (Round up)
If Depth of Ponding is less than 6" the design can be optimized with a smaller surface area. (repeat)	
If Depth of Ponding is greater than 12" a larger surface area will be required (repeat)	
If Depth of Ponding is between 6" to 12" this is the range allowable for bioretention of flow through planters.	

SIZING FOR VOLUME BASED TREATMENT	
DMA #	5
A=	1925 s.f.
Impervious Area =	1350 s.f.
% Imperviousness=	70.13%
MAPsite =	14.5
MAPpage =	13.9
Correction Factor=	1.04317
Clay (D):	Sandy Clay (D): Clay Loam (D):
Silt Loam/Loam (B):	X Not Applicable (100% Impervious):
Are the soils outside the building footprint not graded/compacted? <input type="checkbox"/> No <input type="checkbox"/> Yes/No	
If no, and the soil will be compacted during site preparation and grading, the soils infiltration ability will be decreased. Modify your answer to a soil with a lower infiltration rate (eg. Silt Loam to Clay)	
Modified Soil Type:	D
S=	1.00%
UBS Volume for 1% Slope (UBS1%) =	0.413623377 inches (Use Figure B-2)
UBS Volume for 15% Slope (UBS15%) =	0.424961039 inches (Use Figure B-5)
UBS Volume for X% Slope (UBSX%) =	0.413623377 inches (Corrected Slope for the site)
Adjusted UBS =	Correction Factor (Step 2) x UBSx% (Step 5)
Adjusted UBS =	0.431477623 inches
Design Volume =	Adjusted UBS (Step 6) x Drainage Area (Step 1) x 1ft/12inch
Design Volume =	69.22 ft ³
COMBO FLOW & VOLUME BIOTRETENTION CALCULATION	
Total Drainage Area =	1,925 sq. ft
Impervious Area =	1,350 sq. ft
Pervious Area =	575 sq. ft
Equivalent Impervious Area =	58 sq. ft
Total Equivalent Impervious =	1,408 sq. ft
Rainfall Intensity =	0.2 in/hr
Duration =	Adjusted UBS (Step 6) / Rainfall Intensity
Duration =	2.157388115 hrs
Estimate the Surface Area =	49.5 sq. ft (Typically start with Total Impervious x 0.03)
Volume of Treated Runoff =	44.4961299 cu. ft
Volume in Ponding Area =	24.7200722 cu. ft
Depth of Ponding =	0.49693954 ft
Depth of Ponding =	6 inches (Round up)
If Depth of Ponding is less than 6" the design can be optimized with a smaller surface area. (repeat)	
If Depth of Ponding is greater than 12" a larger surface area will be required (repeat)	
If Depth of Ponding is between 6" to 12" this is the range allowable for bioretention of flow through planters.	

SIZING FOR VOLUME BASED TREATMENT	
DMA #	6
A=	4918 s.f.
Impervious Area =	160 s.f.
% Imperviousness=	3.25%
MAPsite =	14.5
MAPpage =	13.9
Correction Factor=	1.04317
Clay (D):	Sandy Clay (D): Clay Loam (D):
Silt Loam/Loam (B):	X Not Applicable (100% Impervious):
Are the soils outside the building footprint not graded/compacted? <input type="checkbox"/> No <input type="checkbox"/> Yes/No	
If no, and the soil will be compacted during site preparation and grading, the soils infiltration ability will be decreased. Modify your answer to a soil with a lower infiltration rate (eg. Silt Loam to Clay)	
Modified Soil Type:	D
S=	1.00%
UBS Volume for 1% Slope (UBS1%) =	0.041121187 inches (Use Figure B-2)
UBS Volume for 15% Slope (UBS15%) =	0.03306466 inches (Use Figure B-5)
UBS Volume for X% Slope (UBSX%) =	0.041121187 inches (Corrected Slope for the site)
Adjusted UBS =	Correction Factor (Step 2) x UBSx% (Step 5)
Adjusted UBS =	0.042896203 inches
Design Volume =	Adjusted UBS (Step 6) x Drainage Area (Step 1) x 1ft/12inch
Design Volume =	17.58 ft ³
COMBO FLOW & VOLUME BIOTRETENTION CALCULATION	
Total Drainage Area =	4,918 sq. ft
Impervious Area =	1,600 sq. ft
Pervious Area =	4,758 sq. ft
Equivalent Impervious Area =	476 sq. ft
Total Equivalent Impervious =	636 sq. ft
Rainfall Intensity =	0.2 in/hr
Duration =	Adjusted UBS (Step 6) / Rainfall Intensity
Duration =	0.214481014 hrs
Estimate the Surface Area =	30 sq. ft (Typically start with Total Impervious x 0.03)
Volume of Treated Runoff =	2.68101267 cu. ft
Volume in Ponding Area =	14.8992811 cu. ft
Depth of Ponding =	0.4966427 ft
Depth of Ponding =	6 inches (Round up)
If Depth of Ponding is less than 6" the design can be optimized with a smaller surface area. (repeat)	
If Depth of Ponding is greater than 12" a larger surface area will be required (repeat)	
If Depth of Ponding is between 6" to 12" this is the range allowable for bioretention of flow through planters.	

SIZING FOR VOLUME BASED TREATMENT	
DMA #	7
A=	1925 s.f.
Impervious Area =	1350 s.f.
% Imperviousness=	70.13%
MAPsite =	14.5
MAPpage =	13.9
Correction Factor=	1.04317
Clay (D):	Sandy Clay (D): Clay Loam (D):
Silt Loam/Loam (B):	X Not Applicable (100% Impervious):
Are the soils outside the building footprint not graded/compacted? <input type="checkbox"/> No <input type="checkbox"/> Yes/No	
If no, and the soil will be compacted during site preparation and grading, the soils infiltration ability will be decreased. Modify your answer to a soil with a lower infiltration rate (eg. Silt Loam to Clay)	
Modified Soil Type:	D
S=	1.00%
UBS Volume for 1% Slope (UBS1%) =	0.413623377 inches (Use Figure B-2)
UBS Volume for 15% Slope (UBS15%) =	0.424961039 inches (Use Figure B-5)
UBS Volume for X% Slope (UBSX%) =	0.413623377 inches (Corrected Slope for the site)
Adjusted UBS =	Correction Factor (Step 2) x UBSx% (Step 5)
Adjusted UBS =	0.431477623 inches
Design Volume =	Adjusted UBS (Step 6) x Drainage Area (Step 1) x 1ft/12inch
Design Volume =	69.22 ft ³
COMBO FLOW & VOLUME BIOTRETENTION CALCULATION	
Total Drainage Area =	1,925 sq. ft
Impervious Area =	1,350 sq. ft
Pervious Area =	575 sq. ft
Equivalent Impervious Area =	58 sq. ft
Total Equivalent Impervious =	1,408 sq. ft
Rainfall Intensity =	0.2 in/hr
Duration =	Adjusted UBS (Step 6) / Rainfall Intensity
Duration =	2.157388115 hrs
Estimate the Surface Area =	49.5 sq. ft (Typically start with Total Impervious x 0.03)
Volume of Treated Runoff =	44.4961299 cu. ft
Volume in Ponding Area =	24.7200722 cu. ft
Depth of Ponding =	0.49693954 ft
Depth of Ponding =	6 inches (Round up)
If Depth of Ponding is less than 6" the design can be optimized with a smaller surface area. (repeat)	
If Depth of Ponding is greater than 12" a larger surface area will be required (repeat)	
If Depth of Ponding is between 6" to 12" this is the range allowable for bioretention of flow through planters.	

SIZING FOR VOLUME BASED TREATMENT	
DMA #	8
A=	4918 s.f.
Impervious Area =	160 s.f.
% Imperviousness=	3.25%
MAPsite =	14.5
MAPpage =	13.9
Correction Factor=	1.04317
Clay (D):	Sandy Clay (D): Clay Loam (D):
Silt Loam/Loam (B):	X Not Applicable (100% Impervious):
Are the soils outside the building footprint not graded/compacted? <input type="checkbox"/> No <input type="checkbox"/> Yes/No	
If no, and the soil will be compacted during site preparation and grading, the soils infiltration ability will be decreased. Modify your answer to a soil with a lower infiltration rate (eg. Silt Loam to Clay)	
Modified Soil Type:	D
S=	1.00%
UBS Volume for 1% Slope (UBS1%) =	0.041121187 inches (Use Figure B-2)
UBS Volume for 15% Slope (UBS15%) =	0.03306466 inches (Use Figure B-5)
UBS Volume for X% Slope (UBSX%) =	0.041121187 inches (Corrected Slope for the site)
Adjusted UBS =	Correction Factor (Step 2) x UBSx% (Step 5)
Adjusted UBS =	0.042896203 inches
Design Volume =	Adjusted UBS (Step 6) x Drainage Area (Step 1) x 1ft/12inch
Design Volume =	17.58 ft ³
COMBO FLOW & VOLUME BIOTRETENTION CALCULATION	
Total Drainage Area =	4,918 sq. ft
Impervious Area =	1,600 sq. ft
Pervious Area =	4,758 sq. ft
Equivalent Impervious Area =	476 sq. ft
Total Equivalent Impervious =	636 sq. ft
Rainfall Intensity =	0.2 in/hr
Duration =	Adjusted UBS (Step 6) / Rainfall Intensity
Duration =	0.214481014 hrs
Estimate the Surface Area =	30 sq. ft (Typically start with Total Impervious x 0.03)
Volume of Treated Runoff =	2.68101267 cu. ft
Volume in Ponding Area =	14.8992811 cu. ft
Depth of Ponding =	0.4966427 ft
Depth of Ponding =	6 inches (Round up)
If Depth of Ponding is less than 6" the design can be optimized with a smaller surface area. (repeat)	
If Depth of Ponding is greater than 12" a larger surface area will be required (repeat)	
If Depth of Ponding is between 6" to 12" this is the range allowable for bioretention of flow through planters.	

STANDARD STORMWATER CONTROL NOTES:

STANDARD WATER SHALL NOT REMAIN IN THE TREATMENT MEASURES FOR MORE THAN FIVE DAYS, TO PREVENT MOSQUITO GENERATION. SHOULD ANY MOSQUITO ISSUE ARISE, CONTACT THE SANTA CLARA VALLEY VECTOR CONTROL DISTRICT. MOSQUITO LARVICIDES SHALL BE APPLIED ONLY WHEN ABSOLUTELY NECESSARY, AS INDICATED BY THE DISTRICT, AND THEN ONLY BY A LICENSED PROFESSIONAL OR CONTRACTOR. CONTACT INFORMATION FOR THE DISTRICT IS PROVIDED BELOW.

DO NOT USE PESTICIDES OR OTHER CHEMICAL APPLICATIONS TO TREAT DISEASED PLANTS, CONTROL WEEDS OR REMOVED UNWANTED GROWTH. EMPLOY NON-CHEMICAL CONTROLS (BIOLOGICAL, PHYSICAL AND CULTURAL CONTROLS) TO TREAT A PEST PROBLEM. PRUNE PLANS PROPERTY AND AT THE APPROPRIATE TIME OF YEAR. PROVIDE ADEQUATE IRRIGATION FOR LANDSCAPE PLANS. DO NOT OVER WATER.

TABLE 1 ROUTINE MAINTENANCE ACTIVITIES FOR BIOTRETENTION AREAS.

BIOTREATMENT SOIL REQUIREMENTS

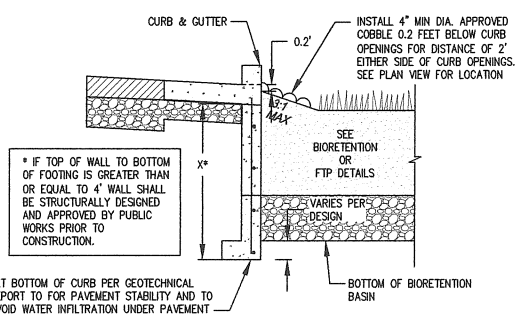
PRIOR TO ORDERING THE BIOTREATMENT SOIL MIX OR DELIVERY TO THE PROJECT SITE, CONTRACTOR SHALL PROVIDE A BIOTREATMENT SOIL MIX SPECIFICATION CHECKLIST, COMPLETED BY THE SOIL MIX SUPPLIER AND CERTIFIED TESTING LAB.

BIOTREATMENT SOIL REQUIREMENTS

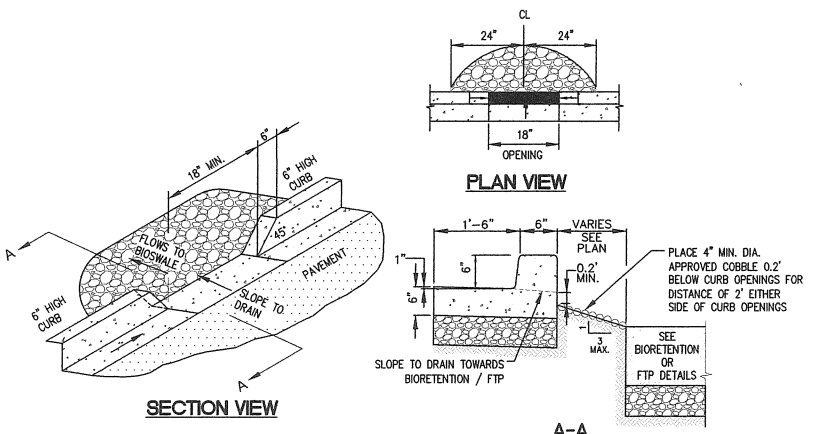
- BIOTRETENTION SOIL MIX SHALL MEET THE REQUIREMENTS AS OUTLINED IN APPENDIX C OF THE C-3 STORM WATER HANDBOOK AND SHALL BE A MIXTURE OF FINE SAND AND COMPOST MEASURED ON A VOLUME BASIS OF 60-70% SAND AND 30-40% COMPOST. CONTRACTOR TO REFER TO APPENDIX C FOR SAND AND COMPOST MATERIAL SPECIFICATIONS. CONTRACTOR MAY OBTAIN A COPY OF THE C3 HANDBOOK AT: [HTTP://WWW.SANJOSECA.GOV/INDEX.ASPX?NID=1761](http://www.sanjooseca.gov/index.aspx?nid=1761)
- PRIOR TO ORDERING THE BIOTREATMENT SOIL MIX OR DELIVERY TO THE PROJECT SITE, CONTRACTOR SHALL PROVIDE A BIOTREATMENT SOIL MIX SPECIFICATION CHECKLIST, COMPLETED BY THE SOIL MIX SUPPLIER AND CERTIFIED TESTING LAB.

BIOTREATMENT & FLOW-THROUGH PLANTER NOTES:

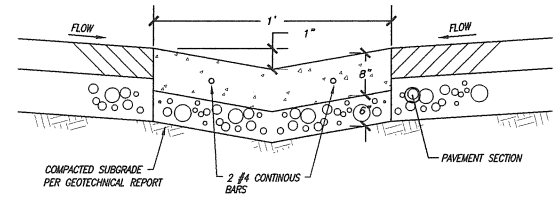
- SEE GRADING PLAN FOR BASIN FOOTPRINT AND DESIGN ELEVATIONS.
- PLACE 3 INCHES OF COMPOSTED, NON-FLOATABLE MULCH IN AREAS BETWEEN STORMWATER PLANTINGS.
- SEE LANDSCAPE PLAN FOR MULCH, PLANT MATERIALS AND IRRIGATION REQUIREMENTS
- CURB CUTS SHALL BE A MINIMUM 18" WIDE AND SPACED AT 10' O.C. INTERVALS AND SLOPED TO DIRECT STORMWATER TO DRAIN INTO THE BASIN. CURB CUTS SHALL ALSO NOT BE PLACED IN LINE WITH OVERFLOW CATCH BASIN. SEE GRADING PLAN FOR MORE DETAIL ON LOCATIONS OF CURB CUTS.
- A MINIMUM 0.2' DROP BETWEEN STORM WATER ENTRY POINT (I.E. CURB OPENING, FLUSH CURB, ETC.) AND ADJACENT LANDSCAPE FINISHED GRADE.
- DO NOT COMPACT NATIVE SOIL / SUBGRADE AT BOTTOM OF BASIN



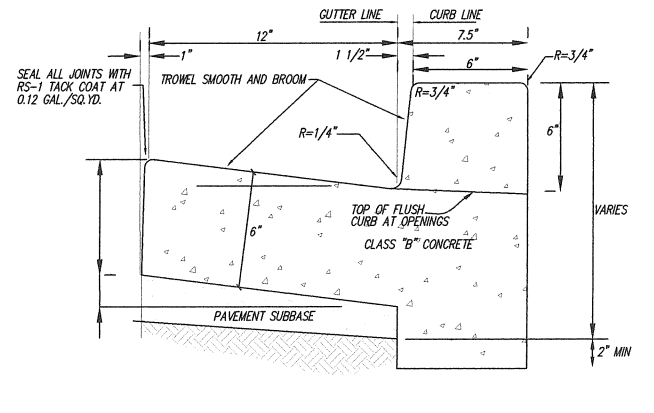
CURB ADJACENT TO BIORETENTION
SCALE: N.T.S.



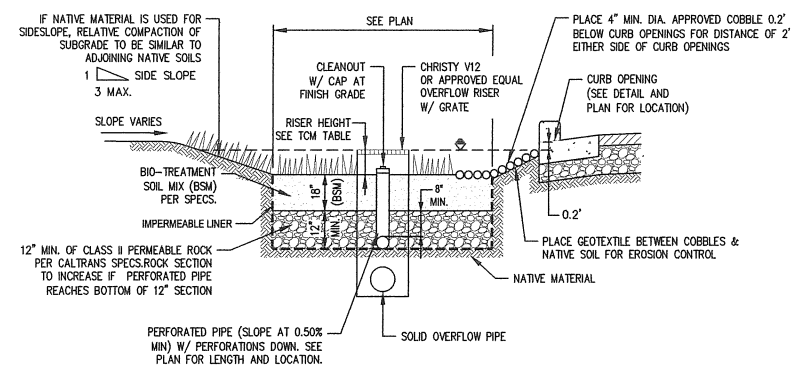
CURB OPENING
SCALE: N.T.S.



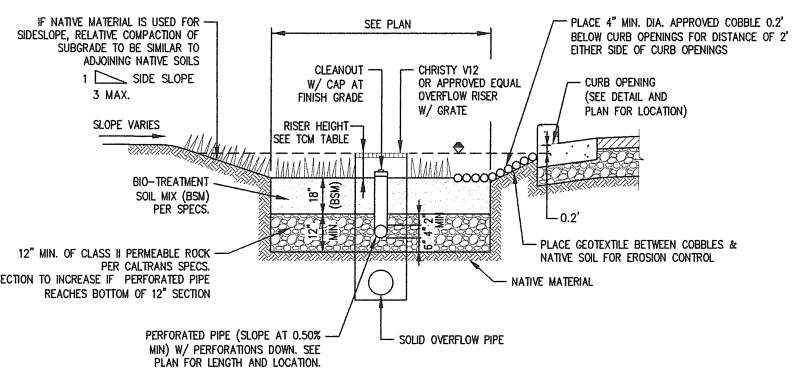
1-FOOT WIDE VALLEY GUTTER
SCALE: N.T.S.



P.C.C. CURB AND GUTTER, TYP.
SCALE: N.T.S.



BIORETENTION BASIN W/ LINER
SCALE: N.T.S.



BIORETENTION BASIN W/O LINER
SCALE: N.T.S.

PROJECT SITE INFORMATION:

- SOILS TYPE: B
- GROUND WATER DEPTH: 20
- NAME OF RECEIVING BODY: GUADALUPE & LOS GATOS
- FLOOD ZONE: D
- FLOOD ELEVATION (IF APPLICABLE): N/A

OPERATION AND MAINTENANCE INFORMATION:

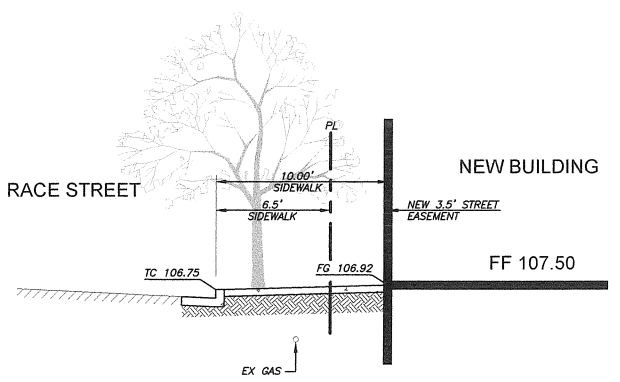
- PROPERTY INFORMATION:
 - I.A. PROPERTY ADDRESS: 253 RACE STREET, SAN JOSE, CA
 - I.B. PROPERTY OWNER: CORE DEVELOPERS, LLC, 470 S. MARKET STREET, SAN JOSE, CA 95113
 - I.C. EMAIL: CHERTEL@THECORECOMPANIES.COM
 - I.D. ADDRESS: CORE DEVELOPERS, LLC, 470 S. MARKET STREET, SAN JOSE, CA 95113
- RESPONSIBLE PARTY FOR MAINTENANCE:
 - II.A. CONTACT: CARL HERTEL
 - II.B. PHONE NUMBER OF CONTACT: 408-282-7841

SITE DESIGN MEASURES:

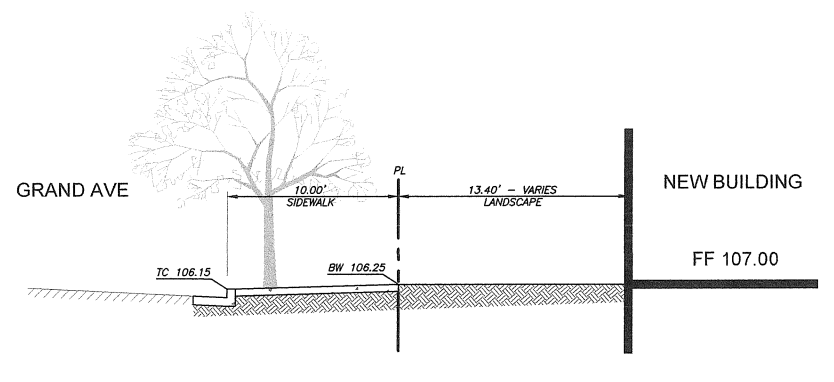
- PRESERVE OPEN SPACE AND NATURAL DRAINAGE PATTERNS.
- REDUCE EXISTING IMPERVIOUS SURFACES.
- CREATE NEW PERVIOUS AREAS.
- LANDSCAPING.
- DIRECT RUNOFF FROM ROOFS, SIDEWALKS, PATIOS TO LANDSCAPED AREAS.
- PARKING:
 - a. NOT PROVIDED IN EXCESS OF CODE.

SOURCE CONTROL MEASURES:

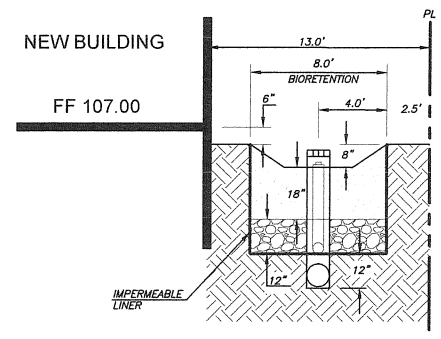
- CONNECT THE FOLLOWING FEATURES TO SANITARY SEWER:
 - a. COVERED TRASH/ RECYCLING ENCLOSURES.
 - b. INTERIOR PARKING STRUCTURES.
- BENEFICIAL LANDSCAPING.
- USE OF WATER EFFICIENT IRRIGATION SYSTEMS.
- MAINTENANCE (PAVEMENT SWEEPING, CATCH BASIN CLEANING, GOOD HOUSEKEEPING).
- STORM DRAIN LABELING.
- OTHER: IMPERVIOUS SURFACES DRAIN TO A SELF-RETAINING AREA THAT IS SIZED PER THE DESIGN CRITERIA LISTED IN THE C.3 STORMWATER HANDBOOK.



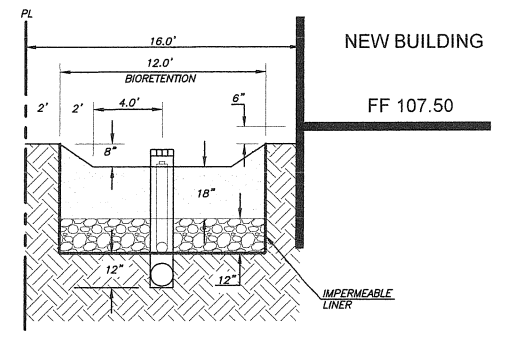
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SCALE: 1"=5"



SECTION B-B
SCALE: 1"=5"



SECTION C-C
SCALE: 1"=5"



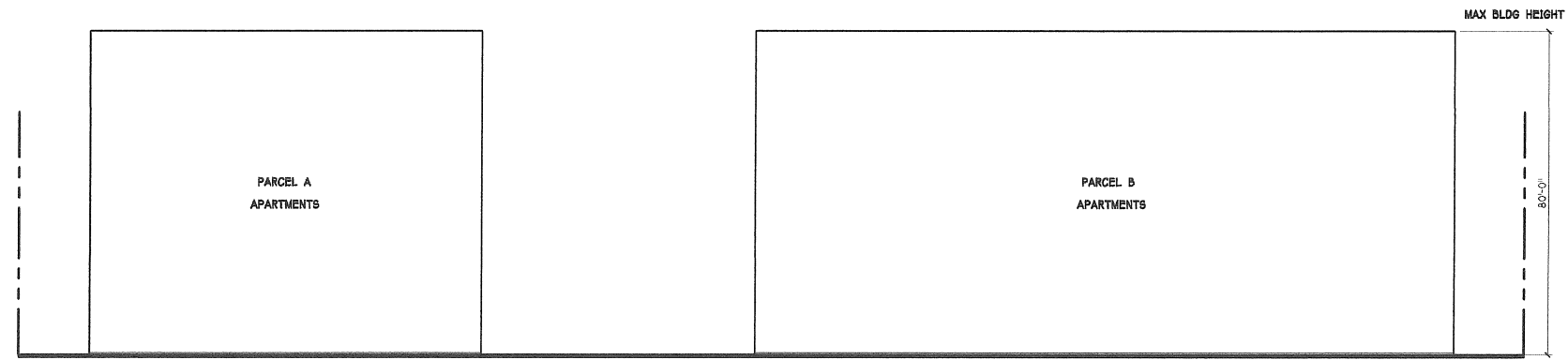
SECTION D-D
SCALE: 1"=5"

RACE STREET

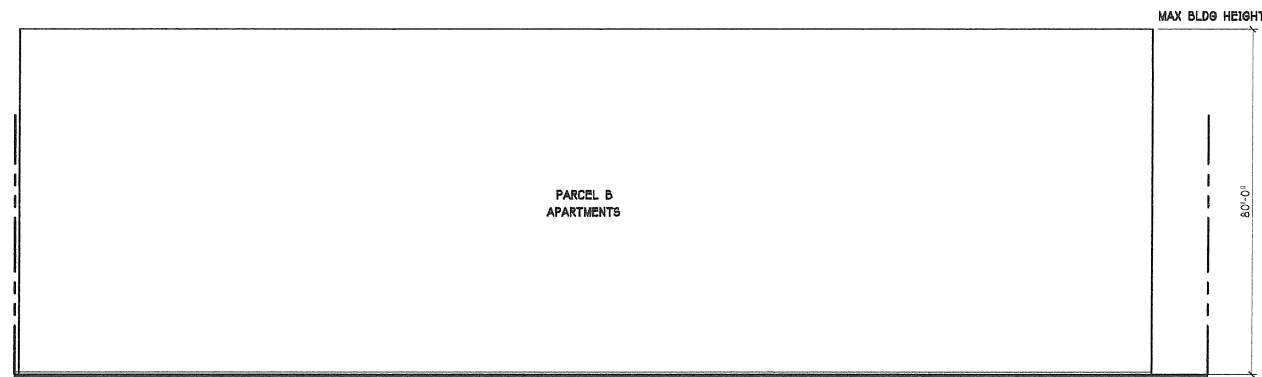
SAN JOSE - CALIFORNIA

Date: 12-05-2017 Drawing Title: DETAILS & SECTIONS
 Scale: AS-SHOWN
 Revisions: Sheet No: C4.0
 of Sheets

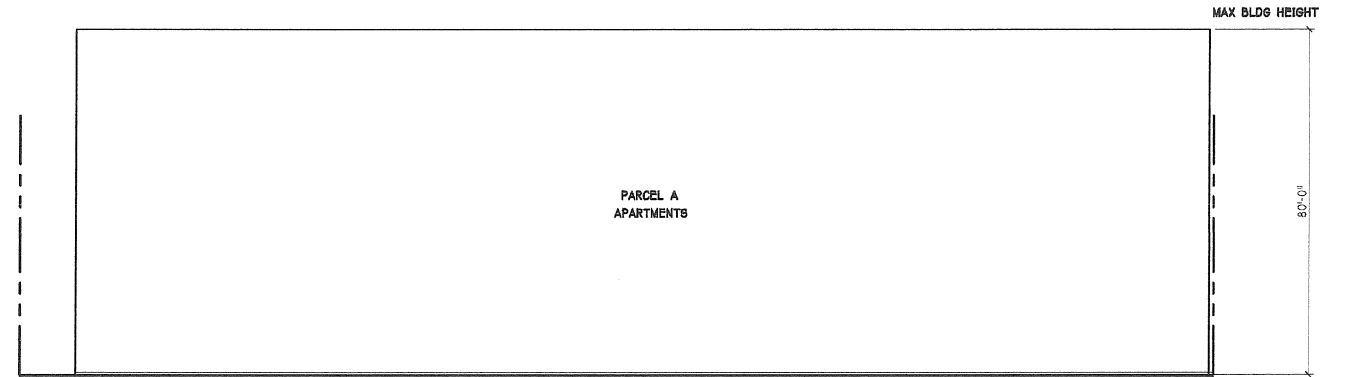
JMH WEISS, INC.
 Civil Engineering - Surveying - Land Planning
 1731 Technology Drive, Suite 880
 San Jose, CA 95110
 (408) 286-4555



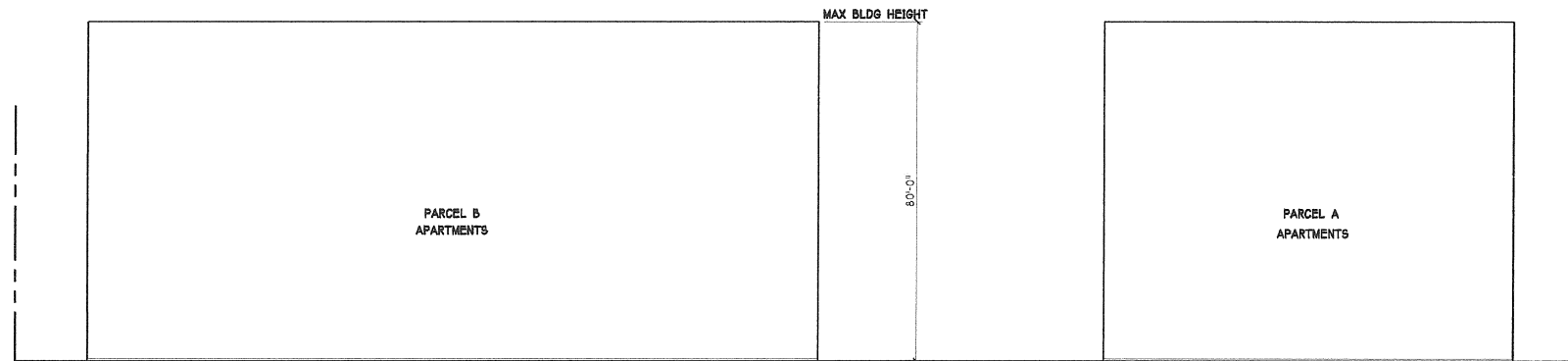
4 CONCEPTUAL REAR ELEVATION (VIEW FROM GRAND AVENUE)
 T = 20'-0"



2 CONCEPTUAL LEFT SIDE ELEVATION
 T = 20'-0"



3 CONCEPTUAL RIGHT SIDE ELEVATION
 T = 20'-0"

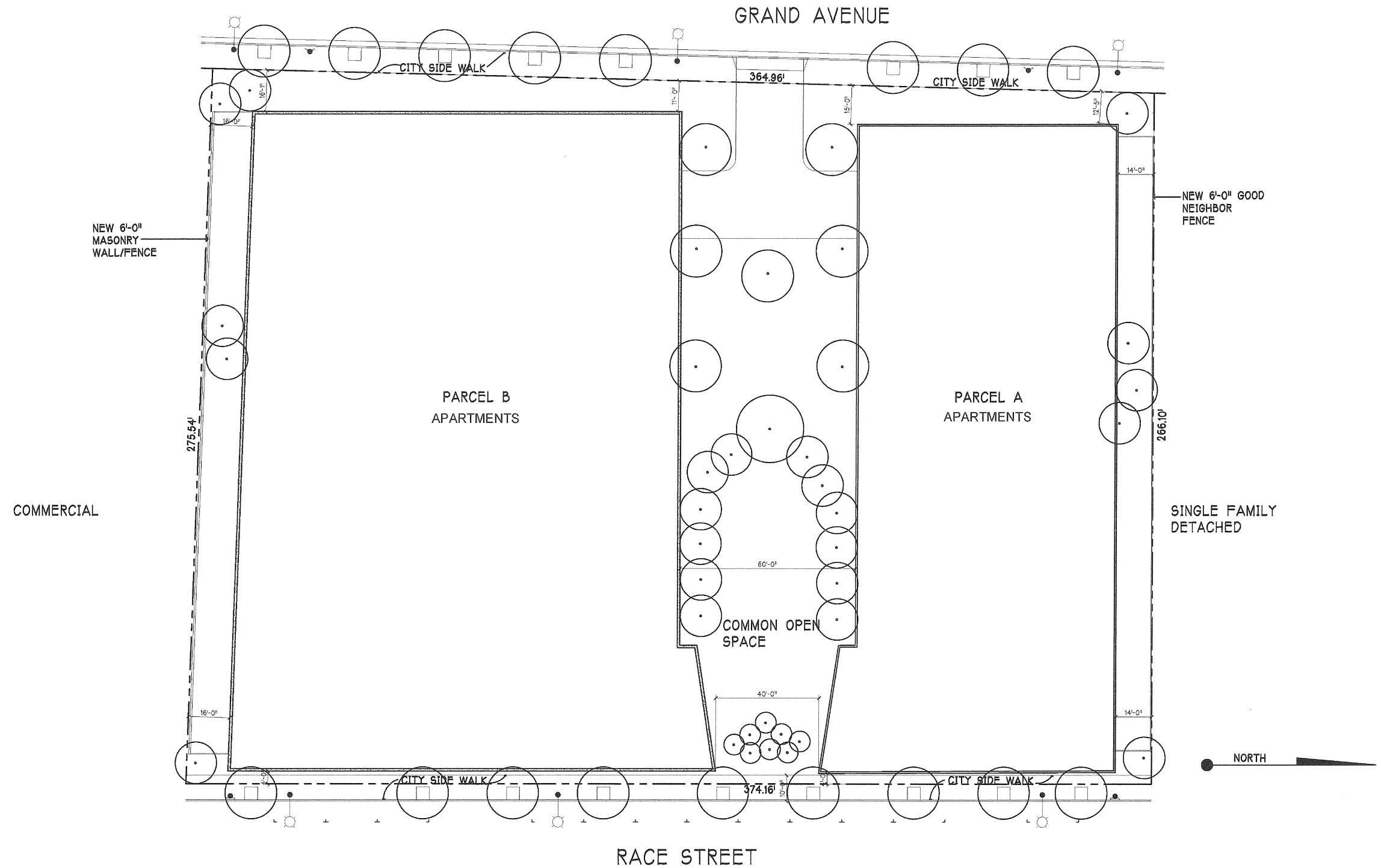


1 CONCEPTUAL FRONT ELEVATION (VIEW FROM RACE STREET)
 T = 20'-0"

RACE STREET
 SAN JOSE - CALIFORNIA
 PDC 17-019

Date:	09-19-2017	Drawing Title:	CONCEPTUAL HEIGHT DIAGRAM
Scale:		Sheet No.:	7.1
Revisions:		of	Sheets

LPMD
Architects
 1288 Kifer Road, Unit 206,
 Sunnyvale, CA 94086
 Telephone : 408-992-0280
 Fax : 408-992-0281



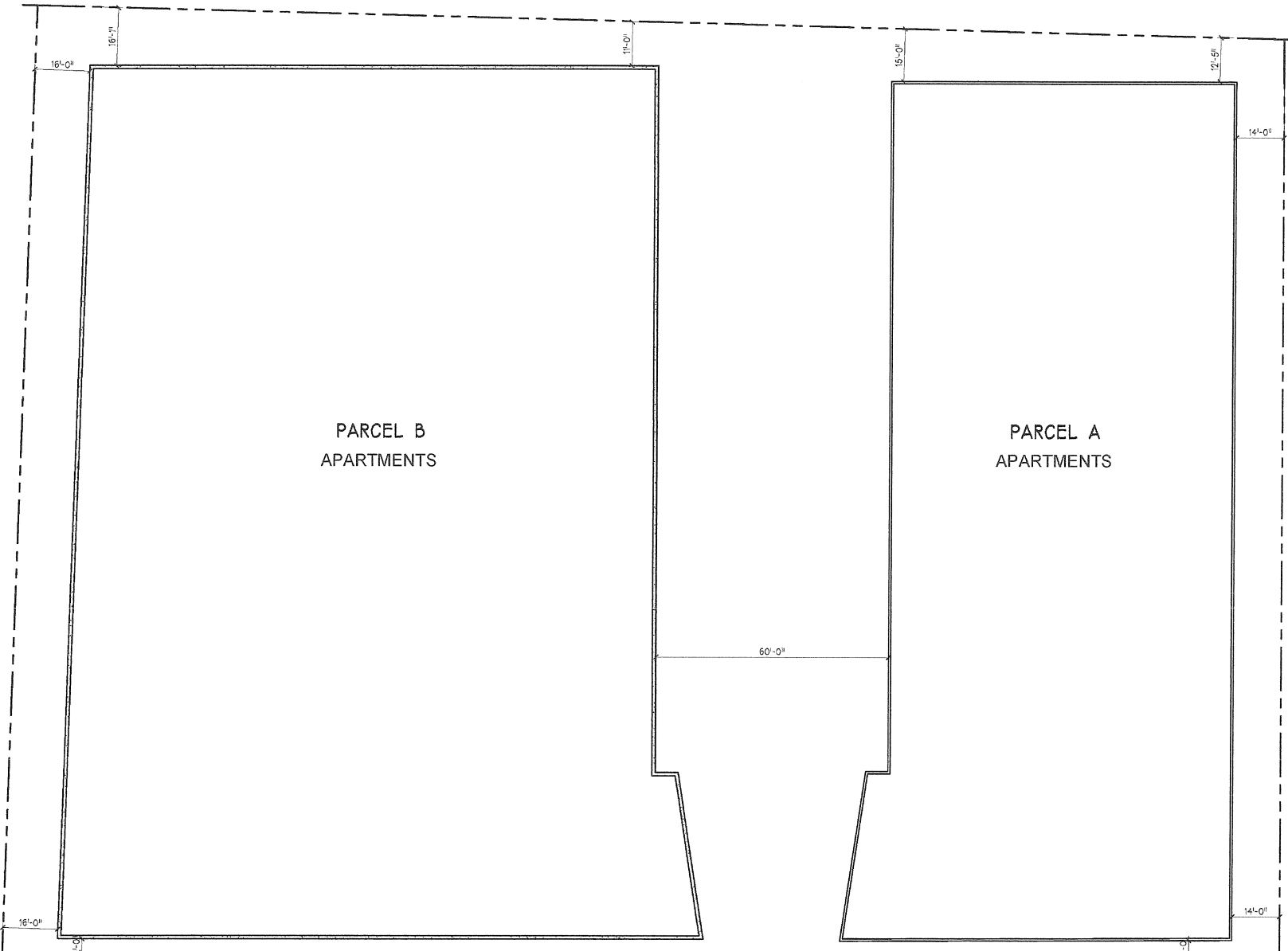
1 CONCEPTUAL SITE PLAN - FIRST FLOOR PLAN
T = 20'-0"

RACE STREET
SAN JOSE - CALIFORNIA
PDC 17-019

Date:	08-18-2017	Drawing Title:	CONCEPTUAL FIRST FLOOR/SITE PLAN
Scale:		Sheet No.:	9.1
Revisions:		of	Sheets

LPMD
Architects
1288 Kifer Road, Unit 206,
Sunnyvale, CA 94086
Telephone : 408-992-0280
Fax : 408-992-0281

GRAND AVENUE



PARCEL B
APARTMENTS

PARCEL A
APARTMENTS

60'-0"

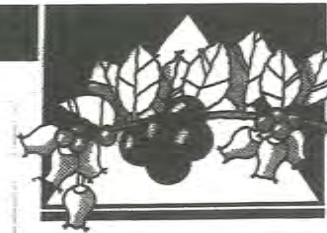
RACE STREET

1 CONCEPTUAL UPPER FLOOR PLANS
T = 20'-0"

RACE STREET
SAN JOSE - CALIFORNIA
PDC 17-019

Date: 08-18-2017 Drawing Title:
 Scale:
 Revisions:
 Sheet No:
 9.2
 of Sheets

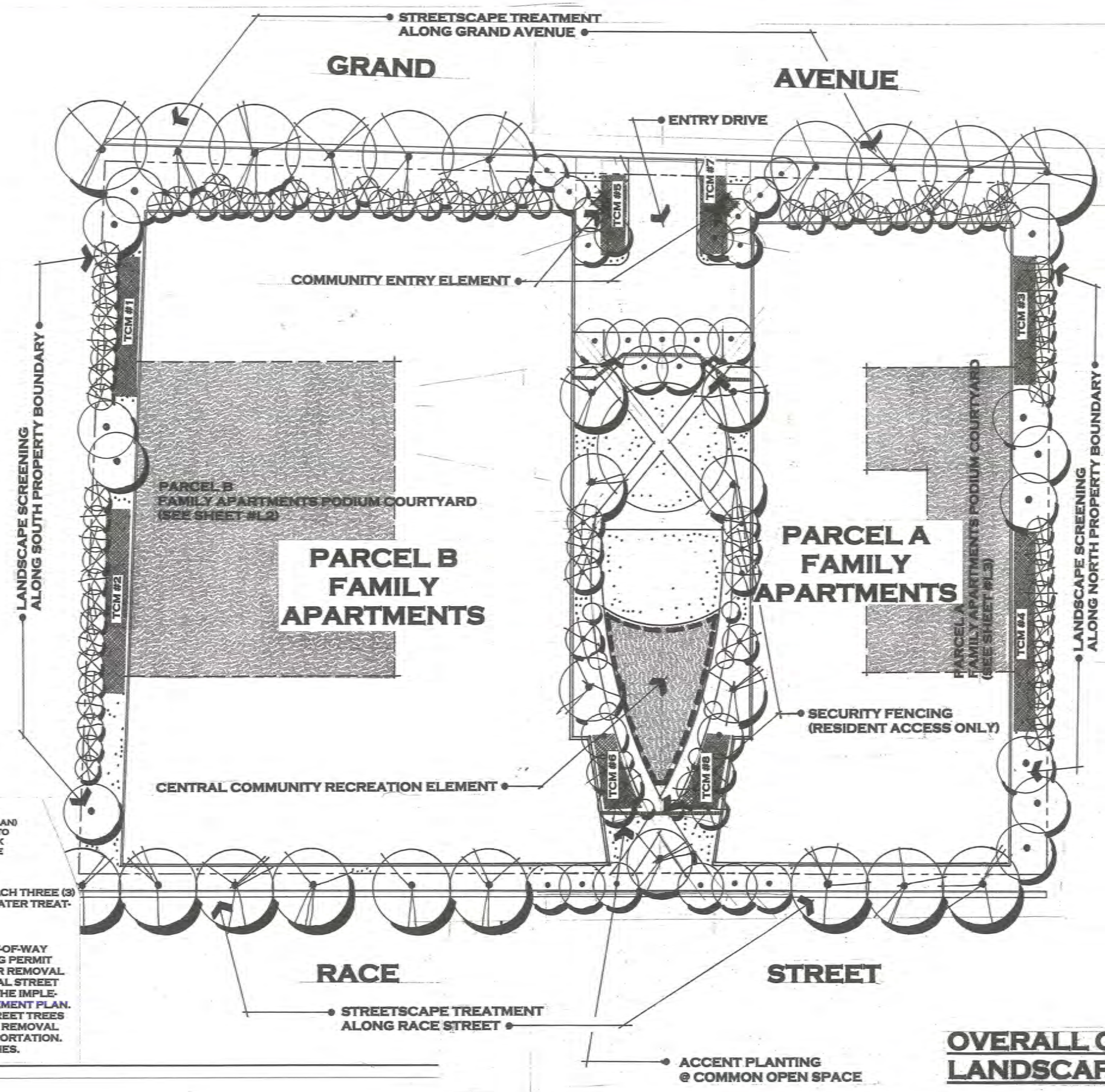
LPMD
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 1288 Kifer Road, Unit 206,
 Sunnyvale, CA 94086
 Telephone : 408-992-0280
 Fax : 408-992-0281



ISAACSON, WOOD & ASSOCIATES
LANDSCAPE ARCHITECTURE
35802 HIBISCUS COURT, FREMONT, CA 9453
PHONE: (408) 838-2329
jw@isaacsonwood.com

253
RACE STREET
A
RESIDENTIAL
COMMUNITY
SAN JOSE
CALIFORNIA

CORE
AFFORDABLE HOUSING, LLC
470 SOUTH MARKET STREET
SAN JOSE, CA 95113

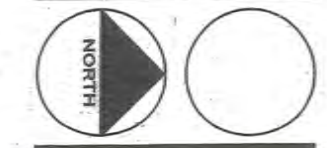


STORM WATER TREATMENT AREA
(PER CIVIL ENGINEER'S STORM WATER CONTROL PLAN)
PLANTING WITHIN THESE AREAS SHALL CONFORM TO
"APPENDIX D" OF THE C3 STORM WATER HANDBOOK
AND PROPOSED PLANTS ARE SPECIFIED WITHIN THE
PROPOSED PLANT PALETTE (SEE SHEET #L4).

NOTE:
INSTALL COMPOSTED, NON-FLOATABLE MULCH THREE (3)
INCHES DEEP IN AREAS BETWEEN STORM WATER TREAT-
MENT PLANTINGS.

NOTE:
STREET TREES SHOWN IN THE PUBLIC RIGHT-OF-WAY
ARE FOR INFORMATION ONLY. THE PLANNING PERMIT
DOES NOT AUTHORIZE THE INSTALLATION OR REMOVAL
OF TREES IN THE PUBLIC RIGHT-OF-WAY. FINAL STREET
TREE LOCATIONS WILL BE DETERMINED AT THE IMPLI-
MENTATION STAGE ON THE PUBLIC IMPROVEMENT PLAN.
THE INSTALLATION OR REMOVAL OF THE STREET TREES
WILL REQUIRE A STREET TREE PLANTING OR REMOVAL
PERMIT FROM THE DEPARTMENT OF TRANSPORTATION.
THE CITY ARBORIST WILL SPECIFY THE SPECIES.

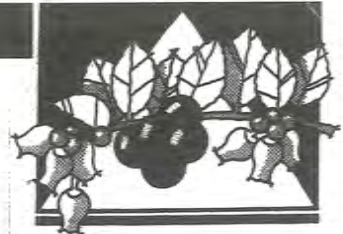
JOB #	REVISIONS:
17001	04.25.17
	09.07.17
DATE:	09.13.17
04.17.17	09.19.17
	1.02.17



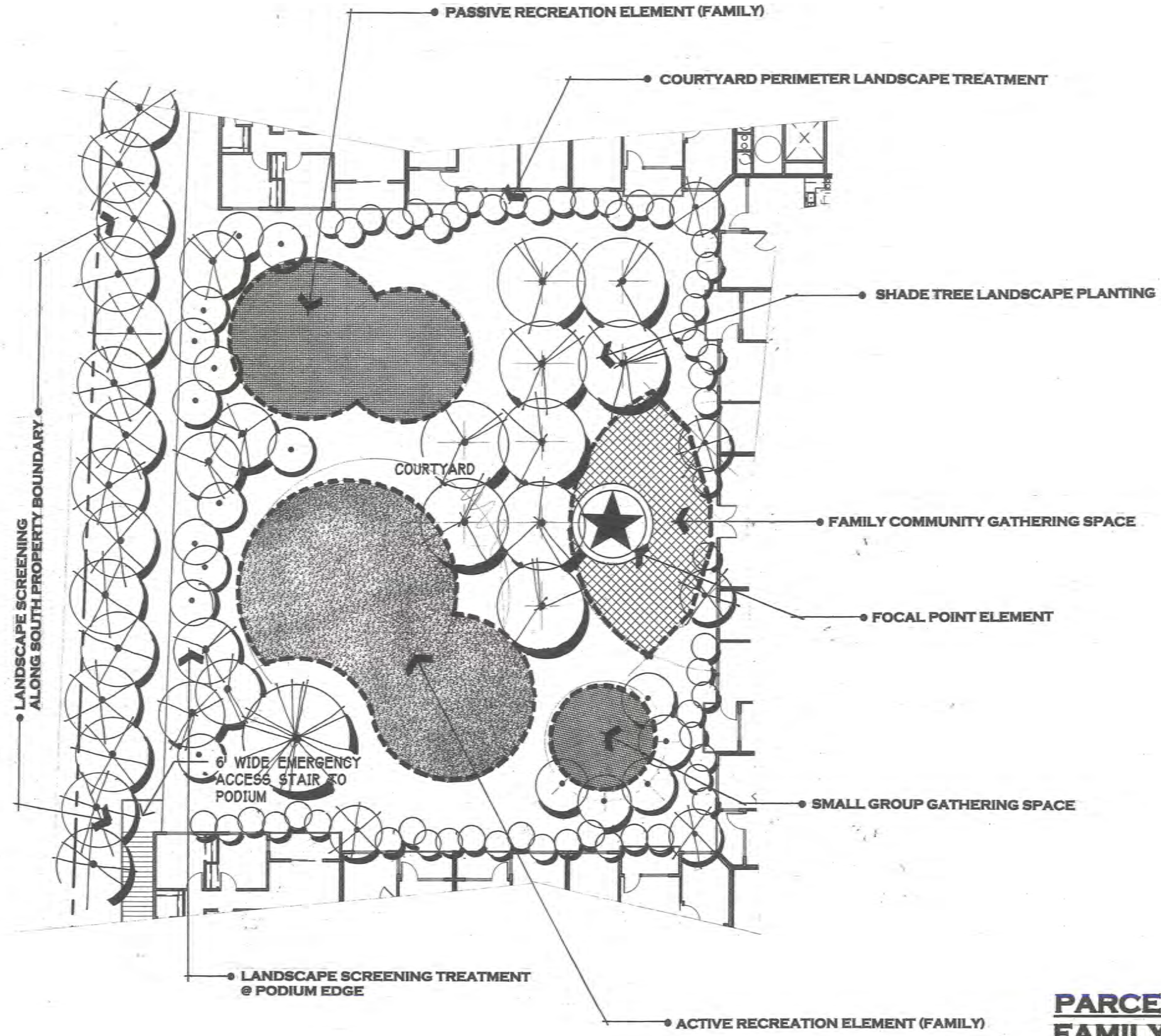
0 20 SCALE: 1"=20'

**OVERALL CONCEPTUAL
LANDSCAPE PLAN**

SHEET
L1
OF



ISAACSON, WOOD & ASSOCIATES
 LANDSCAPE ARCHITECTURE
 35802 HIBISCUS COURT, FREMONT, CA 94536
 PHONE: (408) 838-2329
 iv@isaacsonwood.com

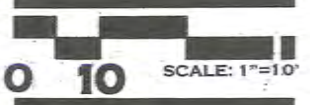
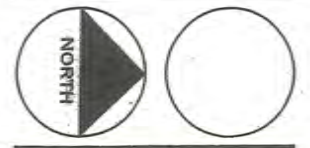


**253
 RACE STREET
 A
 RESIDENTIAL
 COMMUNITY**

**SAN JOSE
 CALIFORNIA**

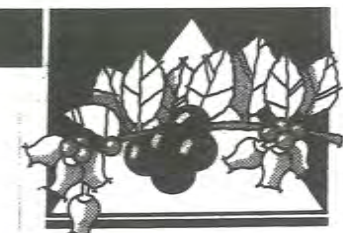
**CORE
 AFFORDABLE HOUSING, LLC
 470 SOUTH MARKET STREET
 SAN JOSE, CA 95113**

JOB #	REVISIONS:
17001	▲ 04.25.17
	▲ 09.07.17
DATE: 04.17.17	▲ 09.13.17
	▲ 09.19.17
	▲ 1.02.17



**PARCEL B
 FAMILY APARTMENTS
 PODIUM COURTYARD
 CONCEPTUAL LANDSCAPE
 PLAN**

**SHEET
 L2
 OF**



ISAACSON, WOOD & ASSOCIATES
 LANDSCAPE ARCHITECTURE
 35802 HIBISCUS COURT, FREMONT, CA 94536
 PHONE: (408) 838-2329
 lav@isaacsonwood.com



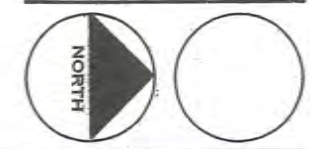
**253
 RACE STREET
 A
 RESIDENTIAL
 COMMUNITY**

**SAN JOSE
 CALIFORNIA**

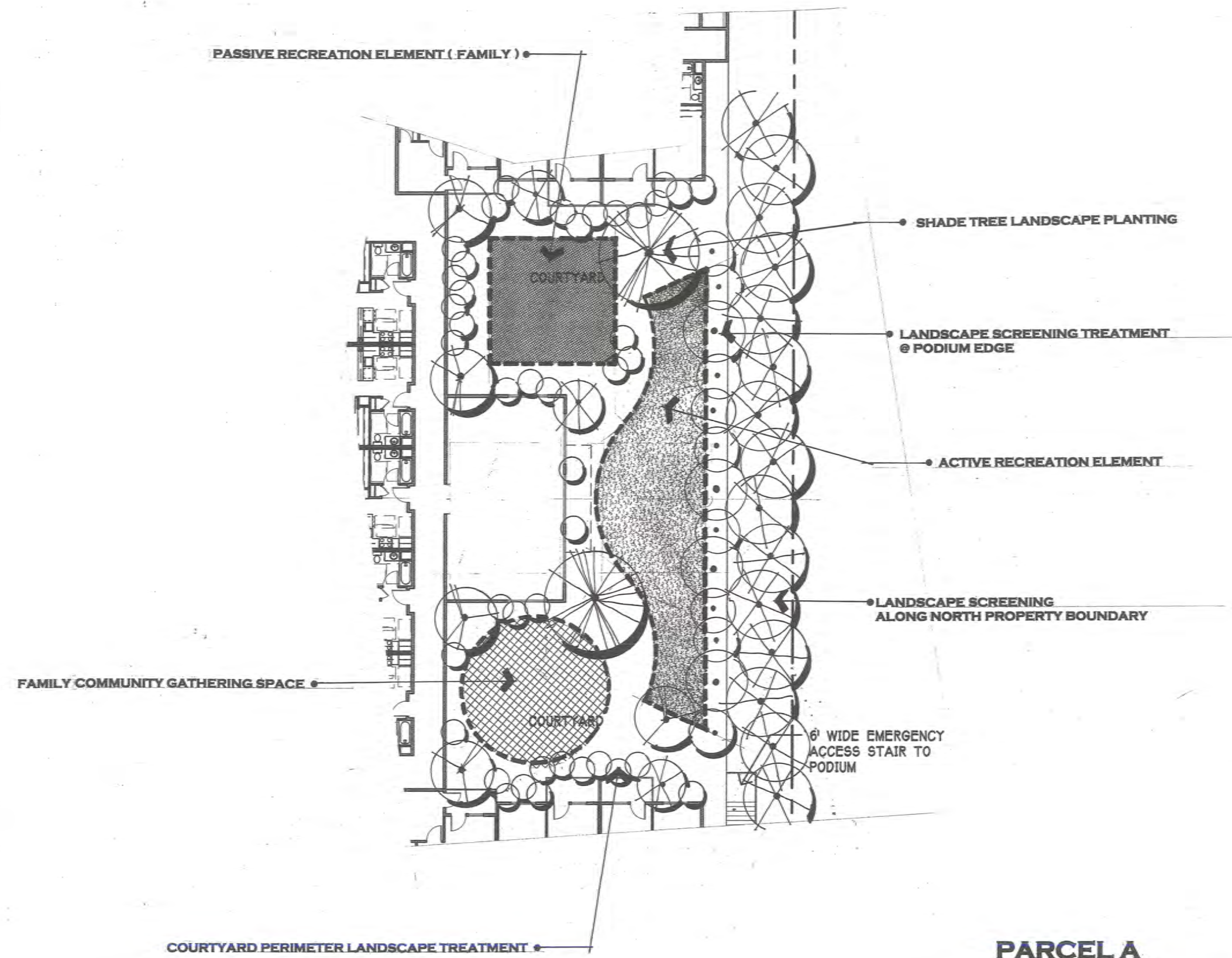
**CORE
 AFFORDABLE HOUSING, LLC
 470 SOUTH MARKET STREET
 SAN JOSE, CA 95113**



JOB #	REVISIONS:
17001	04.25.17 12.06.17
DATE:	09.07.17
04.17.17	09.13.17
	09.19.17
	1.02.17



**SHEET
 L3
 OF**



**PARCEL A
 FAMILY APARTMENTS
 PODIUM COURTYARD
 CONCEPTUAL LANDSCAPE
 PLAN**

PROPOSED PLANT PALETTE

STREETSCAPE ALONG RACE STREET FRONTAGE

- TREES:**
- PISTACIA CHINENSIS (CHINESE PISTACHE) L

STREETSCAPE ALONG GRAND AVENUE

- TREES:**
- LAGERSTROEMIA INDICA "MUSKOGEE" MULTI-TRUNK (CRAPE MYRTLE) L
 - PLATANUS ACERIFOLIA "YARWOOD" (LONDON PLANE TREE) L

- SHRUBS & PERENNIALS:**
- LOROPETALUM CHINESE "RAZZLEBERRI" (N.C.N.) L
 - MYRSINE AFRICANA (AFRICAN BOXWOOD) L
 - ROSMARINUS OFFICINALIS "HUNTINGTON CARPET" (ROSEMARY) L
 - PHORMIUM HYBRID "MAORI MAIDEN" (FLAX) L
 - PHORMIUM HYBRID "MAORI QUEEN" (FLAX) L
 - PHORMIUM HYBRID "MAORI SUNRISE" (FLAX) L

- GROUND COVERS:**
- ARCTOSTAPHYLOS UVA URSI "POINT REYES" (KINKIKINICK) L
 - MYOPORUM PARVIFOLIUM "PROSTRATUM" (N.C.N.) L

COMMUNITY ENTRY & MEWS / PLAZA

- TREES:**
- CELTIS SINENSIS (CHINESE HACKBERRY) L
 - LAGERSTROEMIA INDICA "TUSCARORA" MULTI-TRUNK (CRAPE MYRTLE) L
 - PISTACIA CHINENSIS (CHINESE PISTACHE) L
 - QUERCUS ILEX (HOLLY OAK) L

- SHRUBS & PERENNIALS:**
- CYCAS REVOLUTA (SAGO PALM) M
 - DIETES HYBRID "JACK CATLIN" (FORTNIGHT LILY) L
 - ECHIUM CANDICANS (PRIDE OF MADEIRA) L
 - ROSA "RED MEIDLAND" (SHRUB ROSE) M
 - ROSA "WHITE MEIDLAND" (SHRUB ROSE) M

- GROUND COVERS:**
- FESTUCA GLAUCA "ELIJAH BLUE" (CLUMPING FESCUE) L
 - GAZANIA HYBRID "COPPER KING" (GAZANIA) M

- VINES:**
- FARTHENOCISSUS TRICUSPIDATA (BOSTON IVY) M

PODIUM LEVEL FAMILY & SENIOR COURTYARDS

- TREES:**
- CHAMEROPS HUMILIS (MEDITERRANEAN FAN PALM) L
 - LAGERSTROEMIA INDICA "MUSKOGEE" MULTI-TRUNK (CRAPE MYRTLE) L
 - OLEA EUROPAEA "SWAN HILL" - MULTI TRUNK (FRUITLESS OLIVE) VL
 - RHAPHIOLEPIS INDICA "MAJESTIC BEAUTY" STANDARD (INDIA HAWTHORN) L
 - SYAGRUS ROMANZOFFIANA (QUEEN PALM) L

- SHRUBS & PERENNIALS:**
- ABUTILON HYBRIDS - MIX CULTIVARS (CHINESE LANTERN) M
 - BAMBUSA MULTIPLEX "GOLDEN GODDESS" (CLUMPING BAMBOO) L
 - CITRUS "DWARF MEYERS LEMON" (LEMON) M
 - COLEONEMA PULCHRUM "SUNSET GOLD" (DWARF BREATH OF HEAVEN) M
 - DIETES BICOLOR (FORTNIGHT LILY) L
 - LOROPETALUM CHINESE "RAZZLEBERRI" (N.C.N.) L

- GROUND COVERS:**
- LANTANA MONTEVIDENSIS "GOLD RUSH" (N.C.N.) L

- VINES:**
- DISTICTIS BUCCINATORIA (BLOOD-RED TRUMPET VINE) M

BOUNDARY EDGE SCREENING

- TREES:**
- CUPRESSUS SEMPERVIRENS "STRICTA" (ITALIAN CYPRESS) L
 - LAURUS NOBILIS "SARATOGA" (SWEET BAY) L

- SHRUBS & PERENNIALS:**
- MAHONIA "GOLDEN ABUNDANCE" (N.C.N.) L
 - PHORMIUM TENAX (NEW ZEALAND FLAX) L
 - PITOSPORUM TOBIRA "VARIEGATA" (TOBIRA) L

- GROUND COVERS:**
- COPROSMA x KIRKII (N.C.N.) L

STORMWATER MANAGEMENT AREA

- SHRUBS & PERENNIALS:**
- CAREX DIVULSA (SEDGE) M
 - JUNCUS PATENS (CALIFORNIA GRAY RUSH) M

PLANT MATERIAL CONTAINER SIZE SPECIFICATION:

- TREES: 24" BOX
- SHRUBS & PERENNIALS: 5 GALLON
- GROUND COVERS: 1 GALLON @ 24" O.C.
- VINES: 5 GALLON

PRELIMINARY IRRIGATION SYSTEM COMPONENT SPECIFICATION

- BUBBLER HEADS (TREES & VINES):**
- RAIN BIRD 1804SAM 1300A-F 30 360(MPR) FLOOD 1.70
 - NOTE: TWO (2) BUBBLERS AT EACH TREE AND ONE (1) BUBBLER AT EACH VINE (SEE DETAIL).

- DRIP EMITTERS:**
- PEFCO QUADRA-BUBBLER: 0.64 G.P.M.
 - PEFCO OCTA-BUBBLER: 1.28 G.P.M.
 - NOTE:
 - 1 GAL. & 5 GAL. PLANTS TO HAVE TWO (2) DISTRIBUTION TUBE OUTLETS EACH.
 - 15 GAL. PLANTS TO HAVE THREE (3) DISTRIBUTION TUBE OUTLETS EACH.

- AUTOMATIC IRRIGATION CONTROLLER:**
- HUNTER AUTOMATIC IRRIGATION CONTROLLER WITH SEASONAL ADJUSTMENT AND SENSOR PROGRAMMABILITY; MODEL #PCC-1200 (12-STATIONS) WITH PC-SERIES STATION EXPANSION MODULE #PCM-300 INTERNAL 120 VAC TRANSFORMER; OUTDOOR MODEL.

- WEATHER SENSOR DEVICE:**
- HUNTER SOLAR-SYNC SYSTEM (INCLUDES: WIRELESS SOLAR-SYNC SENSOR, WIRELESS RECEIVER AND MODULE); WITH AUTOMATED DAILY WEATHER ADJUSTMENT TO PROGRAM RUN TIMES AND RAIN / FREEZE SHUTOFF FEATURES. INSTALL PER MANUFACTURER'S SPECIFICATIONS AND RECOMMENDATIONS

- REMOTE CONTROL VALVES (DRIP HYDROZONES):**
- RAIN BIRD ELECTRIC REMOTE CONTROL VALVE FOR DRIP & QUADRA-BUBBLER HYDROZONES; MODEL #PEB-PRS-D SERIES (SIZES: 1" #100 AND 1-1/2" #150). INSTALL WITH BALL VALVE MODEL: KBI OR APPROVED EQUAL, RAIN BIRD PRESSURE REGULATING MODULE AND HARDIE #420 FILTER (150 MESH). BALL VALVE AND FILTER SIZES TO MATCH REMOTE CONTROL VALVE SIZE. INSTALL COMPONENTS WITHIN AMETEK GREEN PLASTIC VALVE BOX; PER DETAIL.

- REMOTE CONTROL VALVES (SPRAY, STREAM ROTOR & BUBBLER HYDROZONES):**
- RAIN BIRD ELECTRIC REMOTE CONTROL VALVE FOR SPRAY, STREAM ROTOR AND BUBBLER HYDROZONES; MODEL #PEB-PRS-D SERIES (SIZES: 3/4" #75, 1" #100, 1-1/2" #150 AND 2" #200). INSTALL WITH BALL VALVE MODEL: KBI (SIZE TO MATCH REMOTE CONTROL VALVE SIZE) AND PRESSURE REGULATING MODULE. INSTALL COMPONENTS WITHIN AMETEK GREEN PLASTIC VALVE BOX; PER DETAIL.

- BACFLOW PREVENTION UNIT (B.P.U.):**
- FEBCO MODEL #825-Y REDUCED PRESSURE BACFLOW PREVENTION DEVICE. SIZE TO BE 2" (UNLESS OTHERWISE NOTED ON PLANS). B.P.U. TO BE INSTALLED WITH FREEZE PROTECTION JACKET WITHIN VANDAL RESISTANT ENCLOSURE (PER DETAIL). CONTRACTOR SHALL SUBMIT MATERIAL SPECIFICATIONS FOR JACKET & ENCLOSURE FOR APPROVAL, PRIOR TO INSTALLATION.

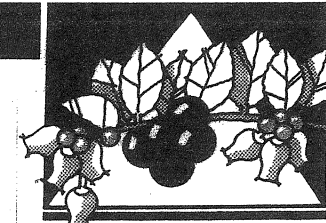
- QUICK-COUPLER VALVE (Q.V.C.):**
- RAIN BIRD MODEL #44RC (SIZE: 1") QUICK-COUPLER VALVE WITH YELLOW CAP; INSTALL WITHIN AMETEK GREEN ROUND PLASTIC VALVE BOX (PER DETAIL). Q.V.C. TO BE LOCATED @ APPROXIMATELY 100' O.C. SPACING; AS SHOWN ON PLANS.

- IRRIGATION SYSTEM PIPE:**
- LATERAL LINE PIPE TO BE CLASS 200 P.V.C. TYPE 1 120-1220. PIPE SIZE PER PIPE SIZING CHART BASED ON G.P.M. DEMANDS (OR AS SHOWN ON PLANS). TWELVE (12) INCH MINIMUM COVER FROM TOP OF PIPE TO FINISH GRADE. INSTALL PER DETAIL.

- STATIC PRESSURE MAINLINE PIPE TO BE SCHEDULE 40 P.V.C. TYPE 1 120-1220. PIPE SIZE TO BE TWO (2) INCH MINIMUM UNLESS OTHERWISE DESIGNATED ON PLANS. EIGHTEEN (18) INCH MINIMUM COVER FROM TOP OF PIPE TO FINISH GRADE. INSTALL PER DETAIL.**

- IRRIGATION SYSTEM PIPE SLEEVES (FOR PLACEMENT OF LATERAL LINE PIPING, STATIC PRESSURE MAINLINE PIPING AND CONTROL WIRE CABLING). SLEEVE PIPE TO BE SCHEDULE 40 P.V.C. TYPE 1 120-1220. INSTALL THREE (3) SLEEVES AT EACH LOCATION; SLEEVE SIZE TO BE SIX (6) INCH. TWENTY-FOUR (24) INCH MINIMUM COVER FROM TOP OF PIPE TO HARDSURFACE SU-GRADE. SLEEVES TO EXTEND TWO (2) FEET BEYOND EDGE OF HARDSURFACE.**

- IRRIGATION SYSTEM WATER METER:**
- WATER METER (SIZE: 1-1/2"), WITH TWO (2) INCH WATER SERVICE LATERAL LINE. SEE CIVIL ENGINEER'S IMPROVEMENT PLANS; VERIFY EXACT LOCATION IN-FIELD.



ISAACSON, WOOD & ASSOCIATES
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jay@isaacsonwood.com

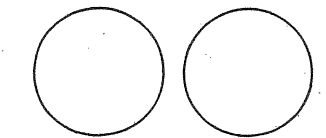
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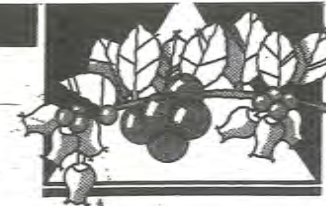
SAN JOSE
CALIFORNIA

CORE
AFFORDABLE HOUSING, LLC
470 SOUTH MARKET STREET
SAN JOSE, CA 95113

PRELIMINARY PLANT PALETTE IRRIGATION KEY

JOB #	REVISIONS:
17001	04.25.17
DATE:	09.07.17
	09.13.17
	09.19.17
	1.02.17





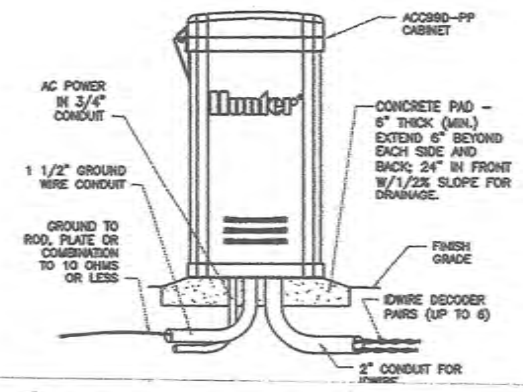
ISAACSON, WOOD & ASSOCIATES
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 35802 Hibiscus Court, Fremont, CA 94536
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 jay@isaacsonwood.com



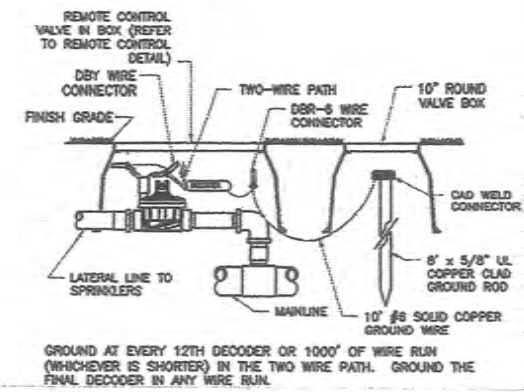
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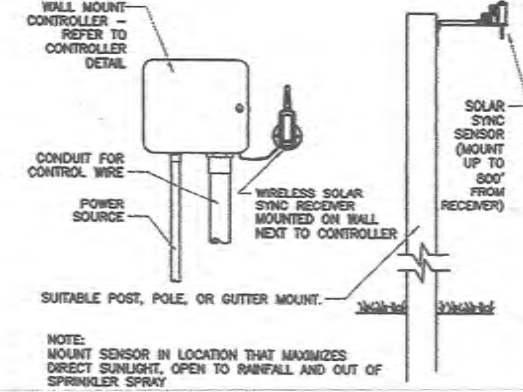
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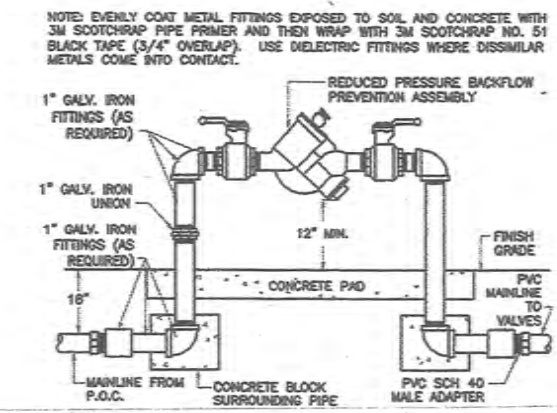
11 PEDESTAL MOUNT AUTOMATIC CONTROLLER



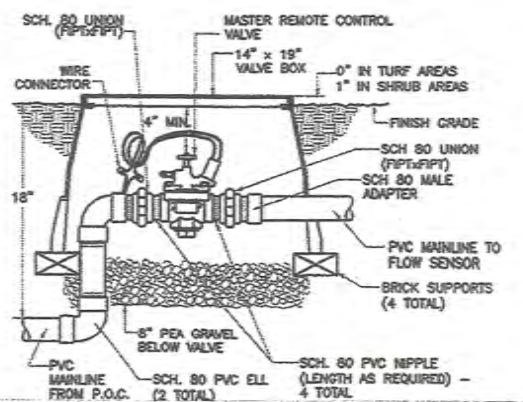
12 TWO WIRE DECODER GROUNDING



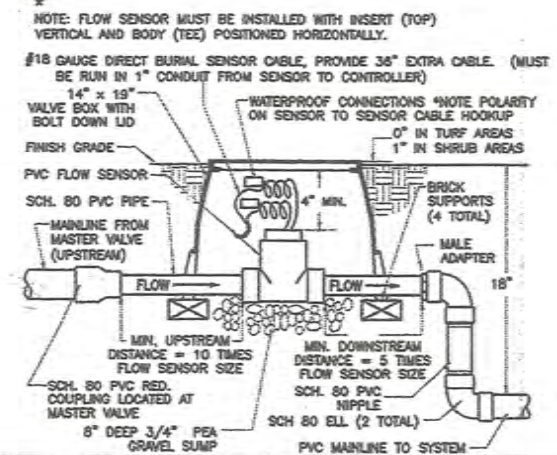
13 SOLAR SYNC WEATHER SENSOR



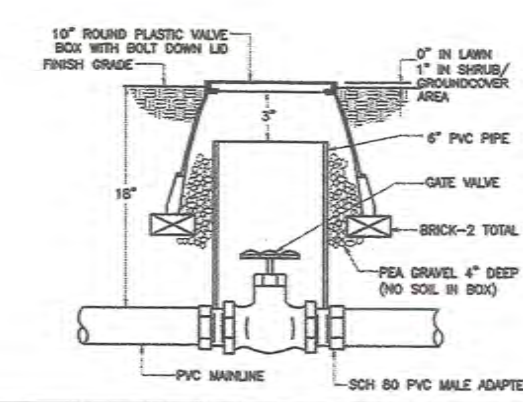
14 REDUCED PRESSURE BACKFLOW ASSEMBLY



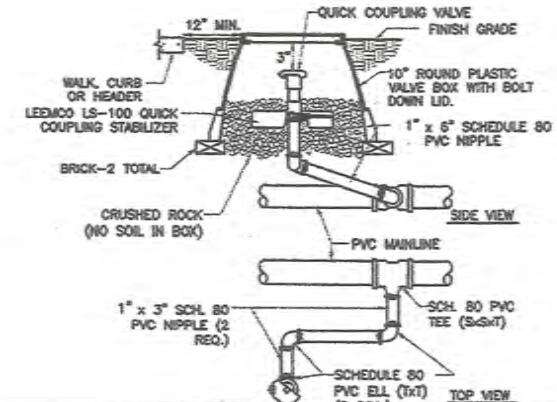
15 MASTER REMOTE CONTROL VALVE



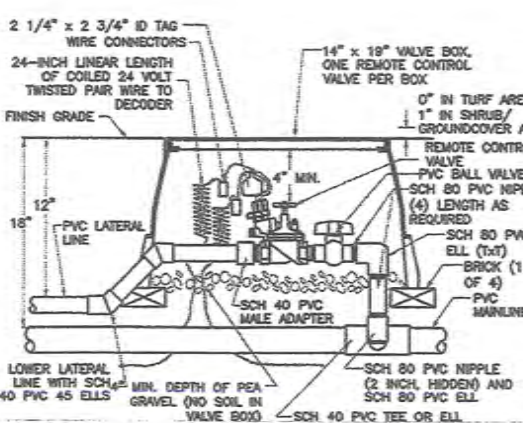
16 PVC FLOW SENSOR



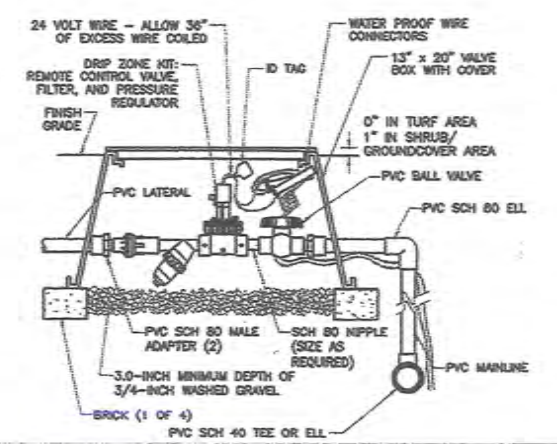
17 GATE VALVE



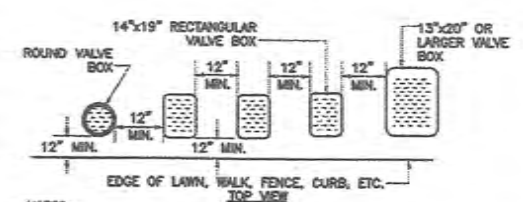
18 QUICK COUPLER VALVE



19 REMOTE CONTROL VALVE & BALL VALVE

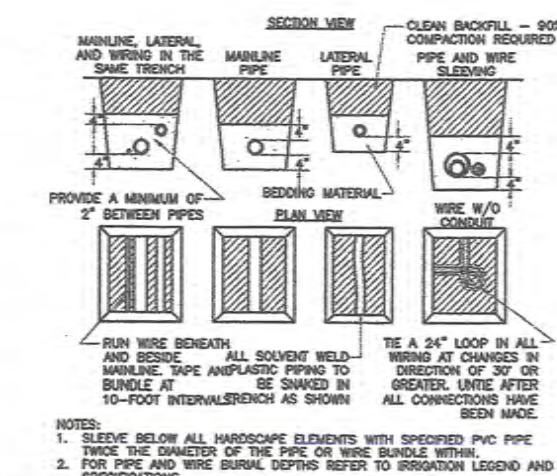


20 DRIP ZONE REMOTE CONTROL VALVE ASSEMBLY



NOTES:
 1. CENTER BOX OVER VALVE TO FACILITATE SERVICING VALVE.
 2. SET BOXES 1" ABOVE FINISH GRADE OR MULCH COVER IN GROUND COVER/SHRUB AREA AND FLUSH WITH FINISH GRADE IN TURF AREA.
 3. SET VALVE BOX ASSEMBLY IN GROUND COVER/SHRUB AREA WHERE POSSIBLE. INSTALL IN LAWN AREA ONLY IF GROUND COVER/SHRUB AREA DOES NOT EXIST ADJACENT TO LAWN.
 4. SET BOXES PARALLEL TO EACH OTHER AND PERPENDICULAR TO EDGE.
 5. AVOID HEAVILY COMPACTING SOIL AROUND VALVE BOX EDGES TO PREVENT COLLAPSE AND DEFORMATION OF VALVE BOX SIDES.
 6. VALVE BOXES SHALL HAVE BOLT DOWN LIDS WITH BOLTS INSTALLED.
 7. VALVE BOXES SHALL BE BY CARSON, APPLIED ENGINEERING, OR EQUAL.

21 MULTIPLE IRRIGATION BOX LAYOUT



NOTES:
 1. SLEEVE BELOW ALL HARDSCAPE ELEMENTS WITH SPECIFIED PVC PIPE TWICE THE DIAMETER OF THE PIPE OR WIRE BUNDLE WITHIN.
 2. FOR PIPE AND WIRE BURIAL DEPTHS REFER TO IRRIGATION LEGEND AND SPECIFICATIONS.

22 PIPE & CONTROL WIRE TRENCHING

IRRIGATION DETAILS

JOB #	17001	REVISIONS:	04.25.17	2.06.17
DATE:	09.07.17		09.13.17	09.19.17
			1.02.17	



STEP 1: STRIP WIRES 1/2" FROM ENDS.

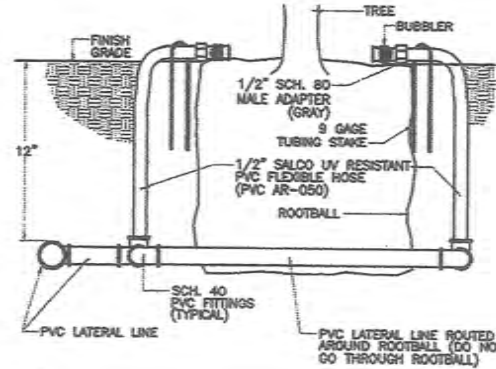
STEP 2: APPLY SCOTCHLOK Y SPRING CONNECTOR IN A CLOCKWISE DIRECTION.

STEP 3: INSERT SPLICE TO BOTTOM OF GEL-FILLED TUBE. CHECK TO MAKE SURE CONNECTOR HAS BEEN PUSHED PAST LOCKING FINGERS AND IS SEATED AT BOTTOM OF TUBE.

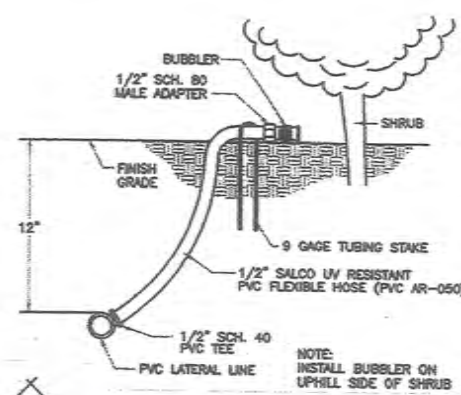
STEP 4: POSITION WIRES IN WIRE CHANNELS AND CLOSE INSULATOR TUBE COVER.

NOTE: MAXIMUM WIRE SIZES PER DBY-6 CONNECTOR ARE THREE #14'S OR TWO #12'S AND PER DBY-8 CONNECTOR ARE FOUR #14'S OR THREE #12'S.

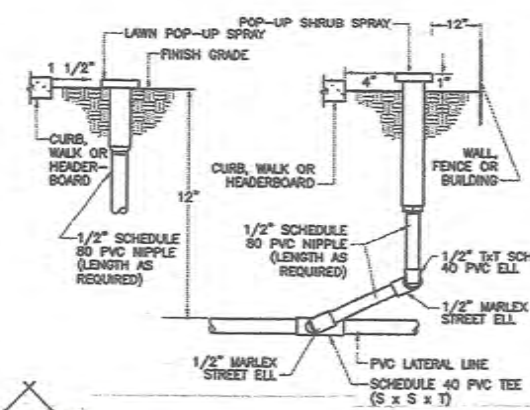
13 CONTROL WIRE CONNECTION



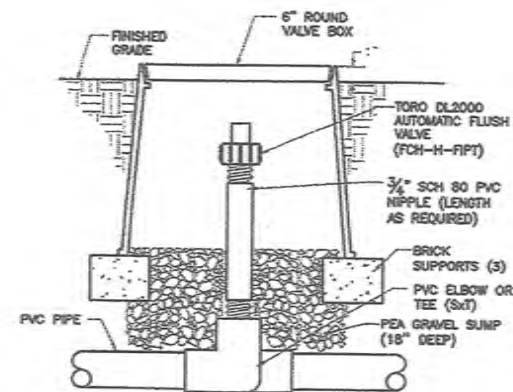
14 TREE BUBBLER



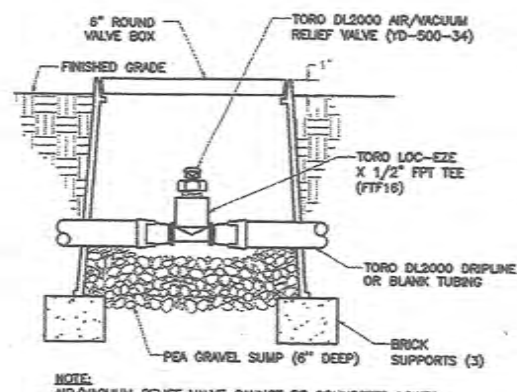
15 SHRUB / VINE BUBBLER



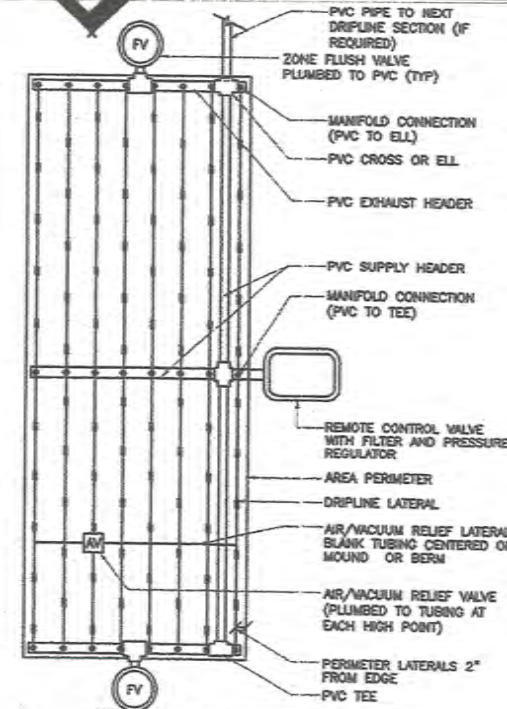
16 POP-UP SPRINKLER



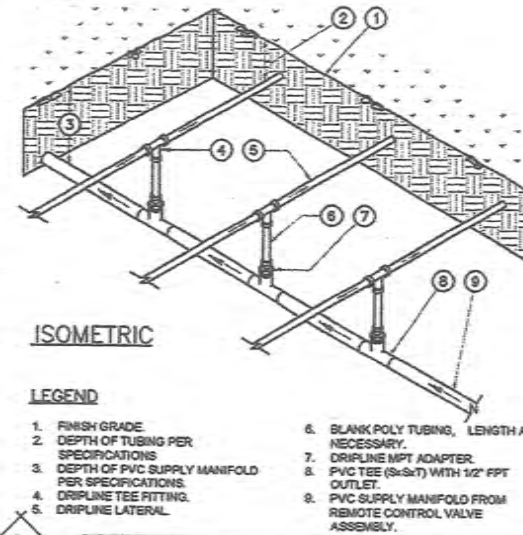
17 AUTOMATIC FLUSH VALVE



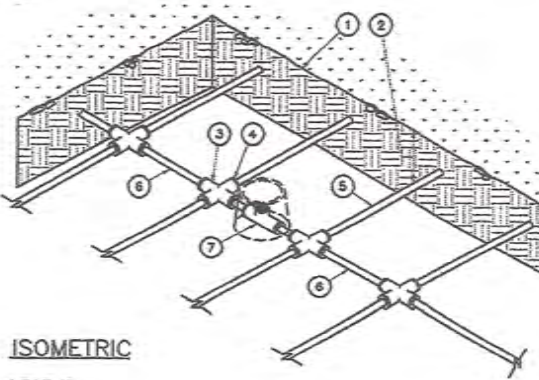
18 AIR / VACUUM RELIEF VALVE



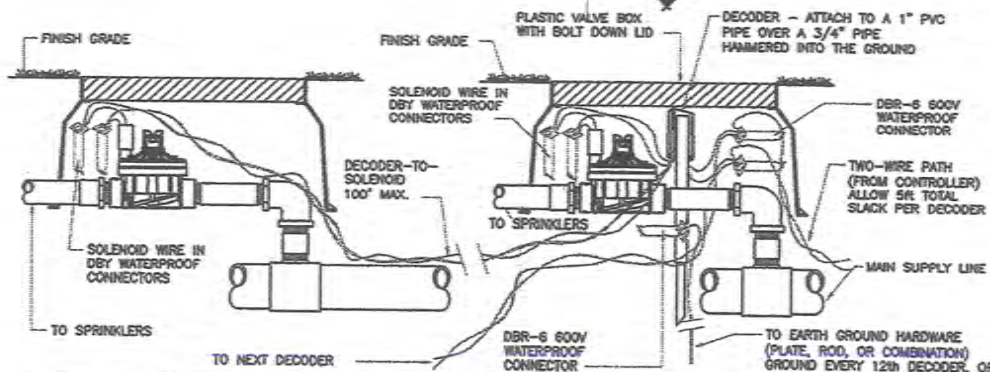
19 CENTER FEED IN-LINE DRIP LAYOUT



20 CENTER FEED MANIFOLD



21 AIR / VACUUM RELIEF LATERAL



22 EXAMPLE OF DECODER WIRING TO SEPARATE BOXES

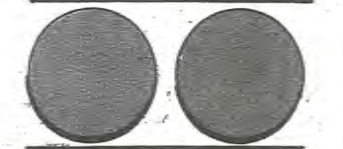
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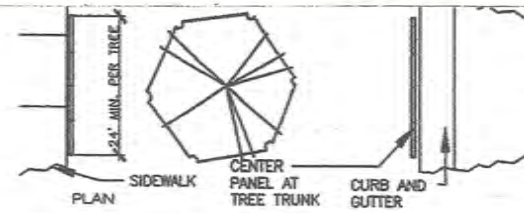
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IRRIGATION DETAILS

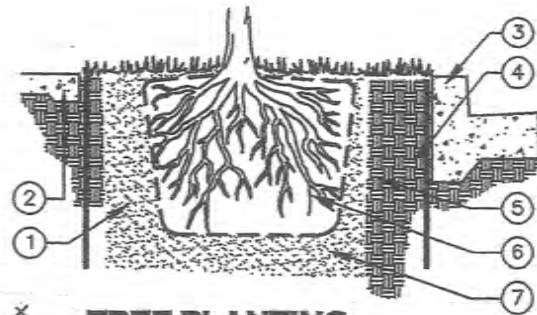
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DATE:	09.07.17			
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	09.19.17			
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ROOT CONTROL BARRIER 2x4" MIN. PER TREE

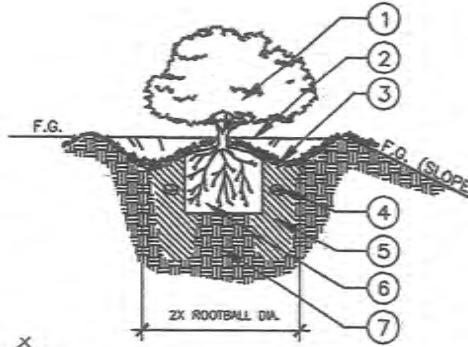
DEPTH MINIMUM 50% POST CONSUMER RECYCLED POLYPROPYLENE PLASTIC WITH ADDED ULTRAVIOLET INHIBITORS WITH A MINIMUM THICKNESS OF 0.085". PANELS SHALL HAVE ROOT DEFLECTING CHANNELS 6" O.C.. CENTER PANELS ON TREE TRUNK AND SET PANEL MIN. 1/2" ABOVE FIN. GRADE.



2. FINISH GRADE OF SIDEWALK.
3. CONCRETE CURB AND GUTTER.
4. NATIVE SOIL.
5. 3/4" GRAVEL BACKFILL.
6. ROOTBALL CROWN AT OR ABOVE FINISH GRADE.
7. SEE TREE PLANTING DETAIL.

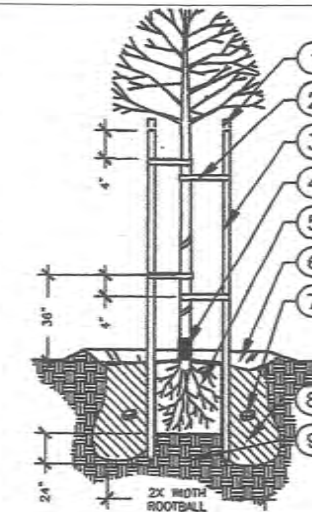
NOTES:
A. PROVIDE ROOT BARRIERS AT ALL TREES PLANTED WITHIN 5' OF CURBS, STREETS, OR SIDEWALKS.

P1 TREE PLANTING W/ ROOT CONTROL BARRIER



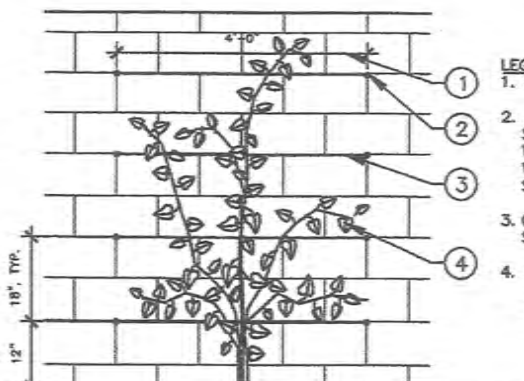
- LEGEND:
1. SHRUB PLANT MATERIAL SEE PLANTING PLANS AND LEGEND.
 2. MINIMUM 2"-3" HIGH WATER BASIN.
 3. TOP DRESSING PER PLANTING PLANS AND LOCAL GOVERNING AGENCY STANDARDS AND SPECIFICATIONS.
 4. FERTILIZER PLANT TABLETS. SEE SPECIFICATIONS FOR SIZE AND QUANTITY.
 5. PLANTING BACKFILL MIX PER SPECIFICATIONS.
 6. SHRUB ROOTBALL, AT OR ABOVE FINISH GRADE.
 7. NATIVE SOIL (OR APPROVED IMPORT).

P2 SHRUB PLANTING



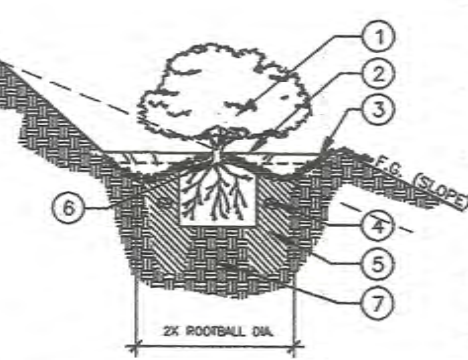
- LEGEND:
1. CUT OFF ENDS DAMAGED BY DRIVING.
 2. FOUR (4) "CINCH-TIE" RUBBER TREE STRAPS ATTACHED TO STAKES WITH 1-1/4" THREADED GALVANIZED NAILS.
 3. TWO (2) 2" DIA. LODGEPOLE STAKES. DO NOT DRIVE STAKE(S) INTO ROOT BALL AND AVOID CONTACT WITH BRANCHES WHEREVER POSSIBLE. SINGLE STAKE CONIFERS. IF TRUNK IS 4" OR LESS, ONLY ONE SUPPORT IS REQUIRED APPROX. 6" BELOW PRIMARY BRANCHES.
 4. APPROVED TRUNK PROTECTOR, ARBOR GUARD OR EQUAL, IN TURF AREAS ONLY.
 5. ROOT BALL.
 6. CONTINUOUS 3" HEIGHT WATERING BASIN, EXCEPT IN TURF AREAS, REMOVE BASIN AT END OF MAINTENANCE PERIOD. PROVIDE BARK MULCH PER SPECS (KEEP 6" AWAY FROM TRUNK).
 7. PLANT TABLETS PER SPECS.
 8. BACKFILL MIX - SEE DRAWINGS OR SPECS.
 9. NATIVE SOIL.

P3 TREE STAKING



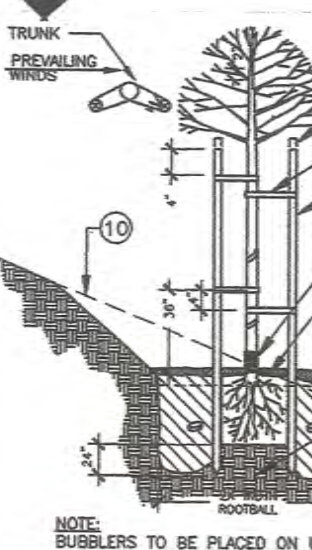
- LEGEND:
1. MASONRY WALL.
 2. 1-1/2" x 3/8" STAINLESS STEEL EYE BOLT. FASTEN TO WALL IN PRE-DRILLED HOLE WITH LEAD EXPANSION SHIELD IN MORTAR JOINT.
 3. COLD DRAWN, STAINLESS STEEL, 18 GAUGE WIRE.
 4. VINE OR ESPALIER PER PLAN. ATTACH PLANT TO WIRE WITH GREEN NURSERY TAPE.

P4 VINE / ESPALIER ON HORIZONTAL WIRE SUPPORT



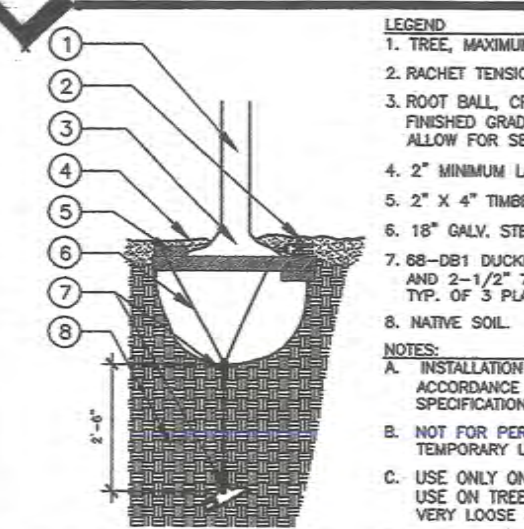
- LEGEND:
1. SHRUB PLANT MATERIAL SEE PLANTING PLANS AND LEGEND.
 2. 4" HIGH x 2' DIA. PLANT BASIN. COVER WITH TOP DRESSING.
 3. TOP DRESSING PER PLANTING PLANS AND LOCAL GOVERNING AGENCY STANDARDS AND SPECIFICATIONS.
 4. FERTILIZER PLANT TABLETS. SEE SPECIFICATIONS FOR SIZE AND QUANTITY.
 5. PLANTING BACKFILL MIX PER SPECIFICATIONS.
 6. PLANTING DEPTH: TOP OF ROOTBALL 1" ABOVE FINISH GRADE.
 7. NATIVE SOIL (OR APPROVED IMPORT).
- NOTE: BUBBLERS TO BE PLACED ON UPHILL SIDE OF SHRUB.

P5 SHRUB PLANTING SLOPE CONDITION



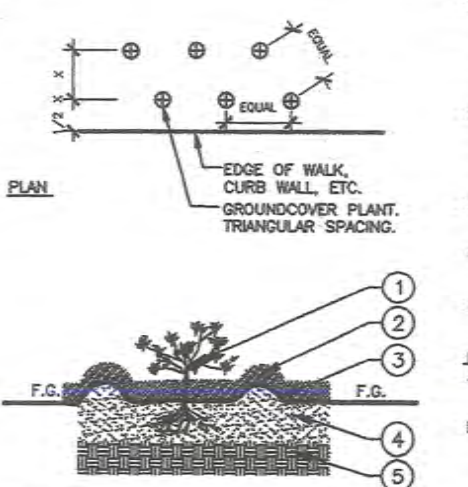
- LEGEND:
1. CUT OFF ENDS DAMAGED BY DRIVING.
 2. FOUR (4) "CINCH-TIE" RUBBER TREE STRAPS ATTACHED TO STAKES WITH 1-1/4" THREADED GALVANIZED NAILS.
 3. TWO (2) 2" DIA. LODGEPOLE STAKES. DO NOT DRIVE STAKE(S) INTO ROOT BALL AND AVOID CONTACT WITH BRANCHES WHEREVER POSSIBLE. SINGLE STAKE CONIFERS. IF TRUNK IS 4" OR LESS, ONLY ONE SUPPORT IS REQUIRED APPROX. 6" BELOW PRIMARY BRANCHES.
 4. APPROVED TRUNK PROTECTOR, ARBOR GUARD OR EQUAL, IN TURF AREAS ONLY.
 5. ROOT BALL PLANTING DEPTH: TOP OF ROOT BALL TO BE 1" ABOVE FINISH GRADE.
 6. 4" HIGH x 2' DIA. PLANT BASIN. PROVIDE BARK MULCH PER SPECS (KEEP 6" AWAY FROM TRUNK).
 7. PLANT TABLETS PER SPECS.
 8. BACKFILL MIX - SEE DRAWINGS OR SPECS.
 9. NATIVE SOIL.
 10. SLOPE TO BE 2:1 MAXIMUM. BLEND INTO EXISTING SLOPE.
- NOTE: BUBBLERS TO BE PLACED ON UPHILL SIDE OF SHRUB.

P6 TREE PLANTING SLOPE CONDITION



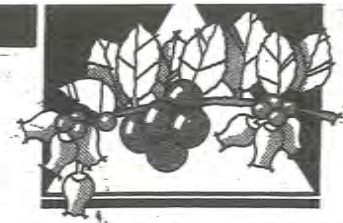
- LEGEND:
1. TREE, MAXIMUM 3" CALIPER.
 2. RACHET TENSIONER.
 3. ROOT BALL, CROWN TO BE AT OR ABOVE FINISHED GRADE. SET 1-1/2" ABOVE F.G. AND ALLOW FOR SETTLEMENT.
 4. 2" MINIMUM LAYER OF DECOMPOSED GRANITE.
 5. 2" X 4" TIMBER TRIANGLE.
 6. 18" GALV. STEEL TENSIONING CABLE 1/8" x 7' x 7'.
 7. 68-DB1 DUCKBILL ANCHOR (INCLUDES ANCHOR AND 2-1/2" x 7/8" x 7' GALV. STEEL CABLE). TYP. OF 3 PLACES.
 8. NATIVE SOIL.
- NOTES:
A. INSTALLATION TO BE COMPLETED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.
B. NOT FOR PERMANENT APPLICATION - FOR TEMPORARY USE ONLY.
C. USE ONLY ON FIRM ROOT BALLS - NOT FOR USE ON TREES GROWN IN SAWDUST MIX OR VERY LOOSE DIRT.
D. TREE CANOPY SHOULD BE KEPT CROPPED TO MINIMIZE WIND RESISTANCE.

P7 TREE ROOTBALL STRAP

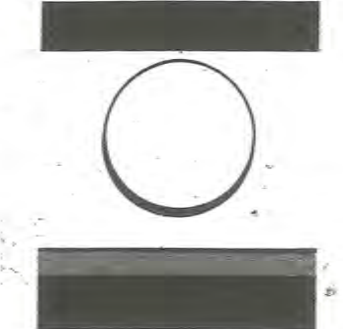


- LEGEND:
1. GROUND COVER PLANT MATERIAL FROM 1 GALLON CONTAINER, FLAT, OR LINER. TRIANGULAR SPACING. SEE PLANTING PLANS AND LEGEND.
 2. TOP DRESSING PER PLANTING PLANS AND LOCAL GOVERNING AGENCY STANDARDS AND SPECIFICATIONS.
 3. MINIMUM 2"-3" HIGH WATER BASIN.
 4. AMENDED SOIL PER PLANTING PLANS AND SPECIFICATIONS.
 5. SCARIFIED SUB-GRADE. SEE SPECIFICATIONS.
- NOTES:
A. PLANT GROUND COVER AT EQUAL SPACING PER PLANT LEGEND.
B. PROVIDE GROUND COVER UP TO THE EDGE OF SOIL BERMS AT ALL TREES AND SHRUBS.

P8 GROUND COVER PLANTING



ISAACSON, WOOD & ASSOCIATES
LANDSCAPE ARCHITECTURE
35802 Hibiscus Court, Fremont, CA 94536
Phone: (408) 838-2329
jwy@isaacsonwood.com



253 RACE STREET A RESIDENTIAL COMMUNITY

SAN JOSE CALIFORNIA

CORE AFFORDABLE HOUSING, LLC
470 SOUTH MARKET STREET
SAN JOSE, CA 95113

PLANTING DETAILS

JOB #	REVISIONS:
17001	04.25.17
	09.07.17
	09.13.17
	09.19.17
	1.02.17



SHEET
L7
OF

The following
items were
received after
packets were
distributed.

From: Nathan Ho <nho@svlg.org>

Sent: Wednesday, February 21, 2018 10:33 PM

To: City Clerk

Subject: Planning Commission Agenda Item 5a: Race St Planned Dev Rezoning Support Letter from SVLG

Dear Ms. Taber,

Please find attached a support letter from the Silicon Valley Leadership Group for the Feb 28 Planning Commission Agenda Item 5a - Planned Development Rezoning of the Race St and Grand Ave Development.

Please feel free to let me know if you have any questions - my contact info is below.

Thank you,

Nathan

Nathan Ho
Senior Director, Housing & Community Development
Silicon Valley Leadership Group
(408) 501-7859 office
nho@svlg.org

2001 Gateway Place, Suite 101E
San Jose, California 95110
(408)501-7864 svlg.org

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President & CEO

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BEN MINICUCCI
Alaska Airlines
KEVIN MURAI
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MARY PAPAIZIAN
San Jose State University
JES PEDERSEN
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ANDY PIERCE
Stryker Endoscopy
KIM POLESE
ClearStreet
RYAN POPPLE
Proterra
RUDY REYES
Verizon
BILL RUH
GE
SHARON RYAN
Bay Area News Group
RON SEGE
Echelon
DARREN SNELLGROVE
Johnson & Johnson
JEFF THOMAS
Nasdaq
JED YORK
San Francisco 49ers

Established in 1978 by
David Packard

February 21, 2018

Planning Commission
City of San Jose
200 East Santa Clara Street
San Jose, CA 95814-4900

**RE: February 28, 2018 Planning Commission Meeting, Agenda Item 5a
Support for Planned Development Rezoning 237-253 Race St & 216-280 Grand Ave**

Dear Chair Pham, Vice Chair Allen, and Honorable Planning Commissioners,

On behalf of the Silicon Valley Leadership Group, I express our support for the proposed 206 affordable apartments of Race St and Grand Ave Residential and the Planned Development Rezoning to facilitate its development.

The Silicon Valley Leadership Group, founded in 1978 by David Packard of Hewlett-Packard, represents more than 375 of Silicon Valley's most respected employers in issues, programs and campaigns that affect the economic quality of life in Silicon Valley. Collectively, Leadership Group members provide nearly one out of every three private sector jobs in Silicon Valley.

The proposed Race St and Grand Ave development has the potential to provide much-needed affordable housing for our community's low-income workers, families, and seniors. The 206 affordable apartments would be rightly situated near public transit, within walking distance of the bus rapid transit (BRT) line on W. San Carlos St and within one-mile of the Diridon Station.

We are encouraged by the partnership between Core Companies and the Santa Clara County Housing Authority to realize this important development, which will provide homes for those that would otherwise be pushed out of the city and the region.

On an annual basis, the Leadership Group surveys the CEOs of our member companies to find out which issues they think are the most important to a healthy economy in the Silicon Valley. Each year, housing affordability and attainability are selected as the top impediments to our economy. We believe that the proposed affordable housing development is part of the solution to our housing crisis.

Our communities need to build more housing and affordable housing to remain competitive in the innovation economy. Our workforce needs a place to go home at night and we applaud the City of San Jose for doing its part to provide homes and to continue building new housing.

Sincerely,



Carl Guardino
President & CEO
Silicon Valley Leadership Group

From: amberly@webfeathers.com [mailto:amberly@webfeathers.com]

Sent: Thursday, February 22, 2018 4:24 PM

To: Planning Commission 1 <PlanningCom1@sanjoseca.gov>; Planning Commission 2 <PlanningCom2@sanjoseca.gov>; Planning Commission 3 <PlanningCom3@sanjoseca.gov>; Planning Commission 4 <PlanningCom4@sanjoseca.gov>; Planning Commission 5 <PlanningCom5@sanjoseca.gov>; Planning Commission 6 <PlanningCom6@sanjoseca.gov>; Planning Commission 7 <PlanningCom7@sanjoseca.gov>; Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>

Subject: I support the Race Street apartment building

I live in the Rose Garden neighborhood on Hanchett Ave, and I support the Race Street apartment building. Please follow the advice of your planning staff and approve the apartment building.

We are tired of having friends and family move away to less expensive areas, and states. Please help housing become less of an issue with this one little step in the right direction.

Amberly Feathers
Hanchett Ave

From: Emma Rawnsley [mailto:emma.rawnsley@gmail.com]

Sent: Thursday, February 22, 2018 4:16 PM

To: Planning Commission 1 <PlanningCom1@sanjoseca.gov>; Planning Commission 2 <PlanningCom2@sanjoseca.gov>; Planning Commission 3 <PlanningCom3@sanjoseca.gov>; Planning Commission 4 <PlanningCom4@sanjoseca.gov>; Planning Commission 5 <PlanningCom5@sanjoseca.gov>; Planning Commission 6 <PlanningCom6@sanjoseca.gov>; Planning Commission 7 <PlanningCom7@sanjoseca.gov>; Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>

Subject: Item 5.a. "Race Street Investments" - I support! Please Approve!

Dear Planning Commissioners,

I have lived in several rental properties within the Rosegarden/Shasta-Hanchett neighborhood since 2008 and have owned a home on Hoover Avenue since 2012. I frequently travel along Race Street to access Safeway, Walgreens, my optometrist & mechanic, Hapas Brewery and several other destinations along or to the south of San Carlos. I used to frequently visit the fish market/kitchen on this site when it was operating. And Eduardo's next door to the site is our family's favorite taqueria! I drive and/or bike along Park Avenue daily as part of my commute to downtown from my children's school.

The Race Street property is the perfect location to add more desperately needed housing in our neighborhood. I understand that some neighbors oppose the height and/or density of the project. But to house the same number of dwelling units as this project would require a much greater footprint if the density or height is reduced. I for one support well designed, higher density development (such as the proposed project) in appropriate parts of our neighborhood (such as the project site), so that the historic fabric of our wonderful neighborhood is not destroyed by adhoc, lower density developments that do not significantly increase our housing stock or provide affordable housing options.

I understand that some neighbors are concerned about parking and/or traffic from the development, but people need to live somewhere, and this site is well located on transit routes, with great walkability/bikeability to allow residents to use alternative transportation options for many trips. If we don't allow dense housing in our well situated neighborhoods close to transit and downtown, then those people will have to live further afield, increasing the vehicle miles travelled, further clogging our freeways, and further decreasing our air quality.

Traffic on Park Avenue is not currently congested during the AM or School PM Peak periods when I travel it. If I drive, it typically takes 5-10 minutes for me to get from Trace Elementary to downtown in the morning, or vice-versa in the afternoon. I am fully supportive of a small increase in travel time on these local roads, if it reduces the overall vehicle miles traveled and associated air quality/GHG/noise/etc impacts that would occur if this development is not approved and those people could not live in our neighborhood and are forced to live further from their workplaces. Just because we already live in this great neighborhood and have short commutes and enjoy local amenities, doesn't mean we get to deny others the same privilege.

While I understand that my house is further from the project than many, who may argue that I wouldn't support this if it was closer to my house, I would point out that I do live close to Stockton Avenue, which is slated for several similar mid-rise housing developments, which I also fully support and hope will be as dense, tall, and well designed as this proposal.

Please vote Yes and follow the recommendation of the staff report to approve this project.

Kind regards,

Emma Rawnsley
Hoover Avenue

From: kellyosha@gmail.com [mailto:kellyosha@gmail.com] **On Behalf Of** Kelly Snider
Sent: Thursday, February 22, 2018 2:24 PM
To: Planning Commission 1 <PlanningCom1@sanjoseca.gov>; Planning Commission 2 <PlanningCom2@sanjoseca.gov>; Planning Commission 3 <PlanningCom3@sanjoseca.gov>; Planning Commission 4 <PlanningCom4@sanjoseca.gov>; Planning Commission 5 <PlanningCom5@sanjoseca.gov>; Planning Commission 6 <PlanningCom6@sanjoseca.gov>; Planning Commission 7 <PlanningCom7@sanjoseca.gov>; Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>
Subject: Item 5.a. "Race Street Investments" - I support! Please Approve!

Dear Planning Commissioners,

I have lived in the Shasta-Hanchett neighborhood since 2001 and owned a home here since 2002. I bike, walk, and drive along Race Street ALL THE TIME going to Safeway, Walgreen's, and more recently to Hapa's Brewing Company. So do my husband and three children.

I have never once in my life said "I wish there was a bunch more vacant retail along Race St." or "Buildings should only be 3 stories tall on this busy urban street that's 4 short blocks from the Diridon Train Station" or even "I wish there was more warehouse space and parking spaces around here".

I DO say very frequently "I wish my kids' best friends weren't moving away to a less expensive city in Oregon" and "I wish my child's preschool teacher didn't have to live in a one-bedroom apartment with her husband and two children" and "I wish there was an affordable apartment nearby for my widowed father to live in where we could walk to visit him every day."

We desperately need this housing in our neighborhood. It is not too tall. It is not too dense. It is a great design in a great location and perfectly appropriate for the area. My home is literally adjacent to a 55' tall building just a few feet from my rear property line, so I know what I'm talking about when I tell you that I have no problem with tall buildings that are well-designed and well-located. This project is BOTH of those things.

Our neighborhood is transforming into a dense walkable community with more people, homes, businesses, and bustle. It's wonderful! I cannot wait for these mid-rise new buildings to be built - and I hope it encourages other property owners to do the same (I'm looking at you U-Haul on the Alameda!). Please vote Yes and follow the recommendation of the staff report.

Respectfully,
Kelly Snider
Pershing Avenue

From: Meaghan Halligan [mailto:meaghan.halligan@gmail.com]

Sent: Thursday, February 22, 2018 3:06 PM

To: Planning Commission 1 <PlanningCom1@sanjoseca.gov>; Planning Commission 2 <PlanningCom2@sanjoseca.gov>; Planning Commission 3 <PlanningCom3@sanjoseca.gov>; Planning Commission 4 <PlanningCom4@sanjoseca.gov>; Planning Commission 5 <PlanningCom5@sanjoseca.gov>; Planning Commission 6 <PlanningCom6@sanjoseca.gov>; Planning Commission 7 <PlanningCom7@sanjoseca.gov>; Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>

Subject: Senior Housing on Race Street

As a resident and home owner in the Shasta Hanchette Park Neighborhood I am writing to voice my support for the senior housing project on Race Street. I think it is important that the city I live in can provide more urgently needed housing for our residents. I hope that you will support this project in the planning committee.

Thank you,

Meaghan Halligan

900 Pershing Ave, San Jose

From: Carol Stephenson [mailto:cstephenson@me.com]

Sent: Tuesday, February 27, 2018 7:08 AM

To: Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>; Hughey, Rosalynn <Rosalynn.Hughey@sanjoseca.gov>; Planning Commission 2 <PlanningCom2@sanjoseca.gov>; Planning Commission 3 <PlanningCom3@sanjoseca.gov>; Planning Commission 1 <PlanningCom1@sanjoseca.gov>; Planning Commission 4 <PlanningCom4@sanjoseca.gov>; Planning Commission 5 <PlanningCom5@sanjoseca.gov>; Planning Commission 7 <PlanningCom7@sanjoseca.gov>; Planning Commission 6 <PlanningCom6@sanjoseca.gov>; info@CatalyzeSV.org

Subject: Affordable Housing in Midtown

Dear Planning Commission Members,

I am a resident of Willow Glen and a member of Catalyze SV. I support affordable housing in Midtown, an area I regularly shop and visit. I am excited about the prospect of higher density in an around the area I live for a more sustainable, liable and affordable neighborhood and city.

Carol Stephenson

95125

From: Michael Casas [mailto:michael.casas@sjsu.edu]

Sent: Tuesday, February 27, 2018 7:07 AM

To: Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>; +Rosalynn.Hughey@sanjoseca.gov; ++Planningcom2@sanjoseca.gov; +Planningcom3@sanjoseca.gov; +Planningcom1@sanjoseca.gov; +Planningcom4@sanjoseca.gov; +Planningcom5@sanjoseca.gov; +Planningcom7@sanjoseca.gov; +Planningcom6@sanjoseca.gov; +info@catalyzesv.org

Subject: Let's Make Core Companies Proposal Even Better

Dear San Jose Planning Commission,

I am encouraged by the proposal from Core Companies to build 206 affordable homes on Race Street in Midtown San Jose.

I believe this project would greatly benefit our City and our Valley, especially if it included active commercial or retail space on its ground floor.

I'm very glad this proposal will support homes for hundreds of working families and seniors on fixed incomes. We desperately need housing for these groups!

And I'm glad the developer has made improvements to the project in response to community groups such as Catalyze SV.

However, I'm concerned that the developer isn't proposing retail on the crucial commercial corridor along Race Street. This is a large project that will bring hundreds of new residents to Midtown San Jose. They need more places to walk and shop to create the vibrant neighborhoods we are building in San Jose. A project this ambitious on a street this commercially focused should have new retail on site to benefit the new residents and the existing neighbors.

This is a very promising project; I look forward to advocating for it in the months ahead, especially if the developer improves it further.

Correspondence

Thank you for considering my perspective.

Sincerely,

Michael Casas

From: Sshoor [mailto:sshoor@aol.com]

Sent: Tuesday, February 27, 2018 8:14 AM

To: Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>; Hughey, Rosalynn <Rosalynn.Hughey@sanjoseca.gov>; Planning Commission 2 <PlanningCom2@sanjoseca.gov>; Planning Commission 3 <PlanningCom3@sanjoseca.gov>; Planning Commission 1 <PlanningCom1@sanjoseca.gov>; Planning Commission 4 <PlanningCom4@sanjoseca.gov>; Planning Commission 5 <PlanningCom5@sanjoseca.gov>; Planning Commission 7 <PlanningCom7@sanjoseca.gov>; Planning Commission 6 <PlanningCom6@sanjoseca.gov>; info@CatalyzeSV.org
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This is a very promising project; I look forward to advocating for it in the months ahead, especially if the developer improves it further.

Thank you for considering my perspective.

Sincerely,

Stan Shoor and Laurie Duckham-Shoor

Sent from my iPhone

Correspondence

From: kirk vartan [mailto:kirk@kvartan.com]

Sent: Tuesday, February 27, 2018 11:56 AM

To: Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>; Hughey, Rosalynn <Rosalynn.Hughey@sanjoseca.gov>; Planning Commission 2 <PlanningCom2@sanjoseca.gov>; Planning Commission 3 <PlanningCom3@sanjoseca.gov>; Planning Commission 1 <PlanningCom1@sanjoseca.gov>; Planning Commission 4 <PlanningCom4@sanjoseca.gov>; Planning Commission 5 <PlanningCom5@sanjoseca.gov>; Planning Commission 7 <PlanningCom7@sanjoseca.gov>; Planning Commission 6 <PlanningCom6@sanjoseca.gov>; info@CatalyzeSV.org

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And I'm glad the developer has made improvements to the project in response to community groups such as Catalyze SV.

However, I'm concerned that the developer isn't proposing retail on the crucial commercial corridor along Race Street. This is a large project that will bring hundreds of new residents to Midtown San Jose. **They need more places to walk and shop to create the vibrant neighborhoods we are building in San Jose.** A project this ambitious on a street this commercially focused should have new retail on site to benefit the new residents and the existing neighbors. Placemaking should be an integral part of the process and its result.

This is a very promising project; I look forward to advocating for it in the months ahead, especially if the developer improves it further.

Thank you for considering my perspective.

Sincerely,

Kirk Vartan
Catalyze SV Board Member and Co-Founder
Vice President, Cory Neighborhood Association
President, Winchester NAC
Co-chair, Stevens Creek Advisory Group
Founder and General Manager, A Slice of New York (now a worker cooperative)
Lead, Forest-Pruneridge Nextdoor Neighborhood
San Jose District 6 resident

Correspondence

From: Marguerite Lee [mailto:marguerite@asony.com]

Sent: Tuesday, February 27, 2018 11:49 AM

To: Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>; Hughey, Rosalynn <Rosalynn.Hughey@sanjoseca.gov>; Planning Commission 2 <PlanningCom2@sanjoseca.gov>; Planning Commission 3 <PlanningCom3@sanjoseca.gov>; Planning Commission 1 <PlanningCom1@sanjoseca.gov>; Planning Commission 4 <PlanningCom4@sanjoseca.gov>; Planning Commission 5 <PlanningCom5@sanjoseca.gov>; Planning Commission 7 <PlanningCom7@sanjoseca.gov>; Planning Commission 6 <PlanningCom6@sanjoseca.gov>; info@CatalyzeSV.org

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This is a very promising project; **I look forward to advocating for it in the months ahead, especially if the developer improves it further.**

Thank you for considering my perspective.

Sincerely,

Marguerite Lee

Catalyze SV Member

Guadalupe River Park Conservancy, Board Secretary

Small Business Owner

Bay Area Native

D6 Resident

Correspondence

From: Anthony Perry [mailto:perryanthonyj@gmail.com]

Sent: Tuesday, February 27, 2018 1:12 PM

To: Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>; Hughey, Rosalynn <Rosalynn.Hughey@sanjoseca.gov>; Planning Commission 2 <PlanningCom2@sanjoseca.gov>; Planning Commission 3 <PlanningCom3@sanjoseca.gov>; Planning Commission 1 <PlanningCom1@sanjoseca.gov>; Planning Commission 4 <PlanningCom4@sanjoseca.gov>; Planning Commission 5 <PlanningCom5@sanjoseca.gov>; Planning Commission 7 <PlanningCom7@sanjoseca.gov>; Planning Commission 6 <PlanningCom6@sanjoseca.gov>; info@catalyzesv.org

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This is a very promising project; I look forward to advocating for it in the months ahead, especially if the developer improves it further.

Thank you for considering my perspective.

Sincerely,

Anthony (A.J.) Perry

PerryAnthonyJ@gmail.com

From: Mendrin, Shaunn
Sent: Tuesday, February 27, 2018 4:53 PM
To: Planning Commission 7
Cc: McHarris, Steve; Thomas, Ned; Espinoza, Melissa
Subject: RE: PDC17-019

Hello Commissioner Yesney,
Below are the responses to your questions from yesterday.

-Shaunn

From: Planning Commission 7
Sent: Monday, February 26, 2018 3:19 PM
To: Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>
Cc: McHarris, Steve <Steve.McHarris@sanjoseca.gov>; Thomas, Ned <ned.thomas@sanjoseca.gov>
Subject: PDC17-019

Shaunn - I had a few minor questions. It says under the discussion for LU-9.13 that the project will "further buffer" the single family neighborhood from the nearby commercial on San Carlos. I don't have a readable aerial or land use map, but I don't understand how that will occur, based on the General Plan diagram in the staff report.

There are several single family homes along Park Avenue that abut the project site. The proposed development will buffer these homes from commercial activity south of the site.

I'm also a little confused about who is going to develop this property. I'm familiar with Core, but references in the staff report to "an affordable housing provider" and later to the Santa Clara County Housing Authority are very vague. Is Core proposing to build the project for somebody else to operate? Is the affordable nature of the use assured, or just possible?

That issue has caused a little confusion initially with the project submittal. Core Companies owns the site and is in contract to sell it to the Santa Clara County Housing Authority. If the sale goes through, Santa Clara County will building family and senior housing on the project site. Since we did not have the Planned Development Permit at this time, we have assumed that it could go either way and this is what we had to do in the environmental review for the project.

The last paragraph on page 5 is confusing. I think there is one or more words missing from the second sentence. The last sentence is (I believe) just explaining why the proposed base zoning is different from the existing base zoning, but it doesn't actually say that.

This is what is should say:

Analysis: The site is currently in the A(PD) Planned Development Zoning District, which allowed ground-floor commercial space and 80 attached residential units in one building. The conforming Zoning District identified in the Zoning Code is Residential Multi-Family (R-M) which allows for an increased density, however, it does not meet the allowed density identified in the General Plan without the use of a Planned Development Zoning District. Although the previous

rezoning's included A(PD), staff is recommending that this Planned Development zoning use R-M, which is consistent with the General Plan Land Use/Transportation Diagram designation.

The staff report discusses the use of a PD zoning instead of the R-M district that conforms to the General Plan designation, and identifies the inconsistencies between the standard zoning and the proposed project. I didn't find an explanation, though, of why reduced setbacks are a good idea at this particular location other than it meets the intent of the GP designation. Are they consistent with existing development patterns in the area? The paragraph on page 9 just says what the height is, and states that the design will be evaluated later (presumably with the PD Permit). Since the zoning sets the height, could there be some discussion of its compatibility and/or relationship to context?

Page 6 list the requirements of R-M and the proposed Development Standards of the R-M(PD). The side setbacks are actually around 14 feet, which is greater than the 5 feet allowed in the R-M district. The front and rear setbacks will be reduced slightly less than what is required. Regarding the height, we wanted to keep the PD Zoning simple with a basic building envelope based on the tallest building, which would range from 75-80 feet in height. If there is concern, we can update the development standards to lock in the height of building A. I can confirm with the applicant on an envelope that will work for building A.

Thank you for your help with this.

Michelle Yesney
Planning Commissioner

From: Mendrin, Shaunn
Sent: Tuesday, February 27, 2018 4:53 PM
To: Planning Commission 7; McHarris, Steve
Cc: Do, Sylvia; Espinoza, Melissa
Subject: RE: Core Companies Proposal

Hello Commissioner Yesney,
The County Housing Authority is in contract with Core to purchase the site after the Rezone is completed and they will develop it with affordable housing.

-Shaunn

From: Planning Commission 7
Sent: Tuesday, February 27, 2018 3:26 PM
To: Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>; McHarris, Steve <Steve.McHarris@sanjoseca.gov>
Subject: Fw: Core Companies Proposal

This commentor is apparently also under the impression that these will be affordable homes. Is that, in fact, the case?

Michelle Yesney
Planning Commissioner

From: Anthony Perry <perryanthonyj@gmail.com>
Sent: Tuesday, February 27, 2018 1:12 PM
To: Mendrin, Shaunn; Hughey, Rosalynn; Planning Commission 2; Planning Commission 3; Planning Commission 1; Planning Commission 4; Planning Commission 5; Planning Commission 7; Planning Commission 6; info@catalyzesv.org
Subject: Core Companies Proposal

Dear San Jose Planning Commission,
I am encouraged by the proposal from Core Companies to build 206 affordable homes on Race Street in Midtown San Jose.

I believe this project would greatly benefit our City and our Valley, especially if it included active commercial or retail space on its ground floor.

I'm very glad this proposal will support homes for hundreds of working families and seniors on fixed incomes. We desperately need housing for these groups!

And I'm glad the developer has made improvements to the project in response to community groups such as Catalyze SV.

However, I'm concerned that the developer isn't proposing retail on the crucial commercial corridor along Race Street. This is a large project that will bring hundreds of new residents to Midtown San Jose. They need more places to walk and shop to create the vibrant neighborhoods we are building in San Jose. A project this ambitious on a street this commercially focused should have new retail on site to benefit the new residents and the existing neighbors.

PC Agenda: 2/28/18

Item: 5.a.

Correspondence

This is a very promising project; I look forward to advocating for it in the months ahead, especially if the developer improves it further.

Thank you for considering my perspective.

Sincerely,

Anthony (A.J.) Perry

PerryAnthonyJ@gmail.com

PC Agenda: 2/28/18

Item: 5.a.

Correspondence

From: byron@schimpp.com <byron@schimpp.com>
Sent: Wednesday, February 28, 2018 12:17 PM
To: Mendrin, Shaunn
Cc: byron
Subject: I am opposed to rezoning at Race and Grand Avenue for PDC17-019

Hello,

I am opposed to the proposed rezoning at 237-253 Race Street and 216-280 Grand Avenue (File No. PDC17-019).

First, it is not in keeping with the established character of the neighborhood.

Second, the increased height will not contribute to an orderly and attractive growth of the city. Putting a very tall building across the street from mostly single story homes (Hanchette Park), homes that are not going anywhere, is a very bad idea. Development is necessary, but the city should ensure that developers are building an attractive city that people want to live in, and not a city that's just a collection of hulking buildings that are little more than self-storage units to live in. To that end, the tallest buildings should be centered around the train lines, and the building heights should taper down to the roof heights of the single family homes.

Third, in keeping with the goal of creating a vibrant city, the first floor of the development should be set back farther from the street. Our sidewalks are already too narrow. The higher stories can cantilever over the sidewalk if desired. A likely tenant on the first floor will be a restaurant or coffee shop, maybe even a bakery. A wider sidewalk would allow for outdoor seating without crowding pedestrians into the street; the city becomes a more vibrant, desirable and safer place.

Thank you,

Byron A Schimpp
236 Tillman Ave
San Jose, CA

PC Agenda: 2/28/18

Item: 5.a.

Correspondence

From: Planning Commission 4

Sent: Tuesday, February 27, 2018 7:53 PM

To: Kelly Snider; Planning Commission 1; Planning Commission 2; Planning Commission 3; Planning Commission 5; Planning Commission 6; Planning Commission 7; Mendrin, Shaunn

Subject: Re: Item 5.a. "Race Street Investments" - I support! Please Approve!

Well said!

From: kellyyosha@gmail.com <kellyyosha@gmail.com> on behalf of Kelly Snider <kelly@sniderware.com>

Sent: Thursday, February 22, 2018 2:24:05 PM

To: Planning Commission 1; Planning Commission 2; Planning Commission 3; Planning Commission 4; Planning Commission 5; Planning Commission 6; Planning Commission 7; Mendrin, Shaunn

Subject: Item 5.a. "Race Street Investments" - I support! Please Approve!

Dear Planning Commissioners,

I have lived in the Shasta-Hanchett neighborhood since 2001 and owned a home here since 2002. I bike, walk, and drive along Race Street ALL THE TIME going to Safeway, Walgreen's, and more recently to Hapa's Brewing Company. So do my husband and three children.

I have never once in my life said "I wish there was a bunch more vacant retail along Race St." or "Buildings should only be 3 stories tall on this busy urban street that's 4 short blocks from the Diridon Train Station" or even "I wish there was more warehouse space and parking spaces around here".

I DO say very frequently "I wish my kids' best friends weren't moving away to a less expensive city in Oregon" and "I wish my child's preschool teacher didn't have to live in a one-bedroom apartment with her husband and two children" and "I wish there was an affordable apartment nearby for my widowed father to live in where we could walk to visit him every day."

We desperately need this housing in our neighborhood. It is not too tall. It is not too dense. It is a great design in a great location and perfectly appropriate for the area. My home is literally adjacent to a 55' tall building just a few feet from my rear property line, so I know what I'm talking about when I tell you that I have no problem with tall buildings that are well-designed and well-located. This project is BOTH of those things.

Our neighborhood is transforming into a dense walkable community with more people, homes, businesses, and bustle. It's wonderful! I cannot wait for these mid-rise new buildings to be built - and I hope it encourages other property owners to do the same (I'm looking at you U-Haul on the Alameda!). Please vote Yes and follow the recommendation of the staff report.

Respectfully,
Kelly Snider
Pershing Avenue

From: Planning Commission 4

Sent: Tuesday, February 27, 2018 7:52 PM

To: Emma Rawnsley; Planning Commission 1; Planning Commission 2; Planning Commission 3; Planning Commission 5; Planning Commission 6; Planning Commission 7; Mendrin, Shaunn

Subject: Re: Item 5.a. "Race Street Investments" - I support! Please Approve!

Hi Emma,

Thanks for your note. And, I'm glad to hear that you bike in the area as well.

Shiloh

From: Emma Rawnsley <emma.rawnsley@gmail.com>

Sent: Thursday, February 22, 2018 4:15:46 PM

To: Planning Commission 1; Planning Commission 2; Planning Commission 3; Planning Commission 4; Planning Commission 5; Planning Commission 6; Planning Commission 7; Mendrin, Shaunn

Subject: Item 5.a. "Race Street Investments" - I support! Please Approve!

Dear Planning Commissioners,

I have lived in several rental properties within the Rosegarden/Shasta-Hanchett neighborhood since 2008 and have owned a home on Hoover Avenue since 2012. I frequently travel along Race Street to access Safeway, Walgreens, my optometrist & mechanic, Hapas Brewery and several other destinations along or to the south of San Carlos. I used to frequently visit the fish market/kitchen on this site when it was operating. And Eduardo's next door to the site is our family's favorite taqueria! I drive and/or bike along Park Avenue daily as part of my commute to downtown from my children's school.

The Race Street property is the perfect location to add more desperately needed housing in our neighborhood. I understand that some neighbors oppose the height and/or density of the project. But to house the same number of dwelling units as this project would require a much greater footprint if the density or height is reduced. I for one support well designed, higher density development (such as the proposed project) in appropriate parts of our neighborhood (such as the project site), so that the historic fabric of our wonderful neighborhood is not destroyed by adhoc, lower density developments that do not significantly increase our housing stock or provide affordable housing options.

I understand that some neighbors are concerned about parking and/or traffic from the development, but people need to live somewhere, and this site is well located on transit routes, with great walkability/bikeability to allow residents to use alternative transportation options for many trips. If we don't allow dense housing in our well situated neighborhoods close to transit and downtown, then those people will have to live further afield, increasing the vehicle miles travelled, further clogging our freeways, and further decreasing our air quality.

Traffic on Park Avenue is not currently congested during the AM or School PM Peak periods when I travel it. If I drive, it typically takes 5-10 minutes for me to get from Trace Elementary to downtown in the morning, or vice-versa in the afternoon. I am fully supportive of a small increase in travel time on these local roads, if it reduces the overall vehicle miles traveled and associated air quality/GHG/noise/etc impacts that would occur if this development is not approved and those people could not live in our neighborhood and are forced to live further from their workplaces. Just because we already live in this great neighborhood and have short commutes and enjoy local amenities, doesn't mean we get to deny others the same privilege.

While I understand that my house is further from the project than many, who may argue that I wouldn't support this if it was closer to my house, I would point out that I do live close to Stockton Avenue, which is slated for several similar mid-rise housing developments, which I also fully support and hope will be as dense, tall, and well designed as this proposal.

Please vote Yes and follow the recommendation of the staff report to approve this project.

Kind regards,

Emma Rawnsley
Hoover Avenue

PC Agenda: 2/28/18

Item: 5.a.

Correspondence

From: Alex Shoor <alexshoor@gmail.com>

Sent: Tuesday, February 27, 2018 4:27 PM

To: Mendrin, Shaunn; Hughey, Rosalynn; Planning Commission 2; Planning Commission 3; Planning Commission 1; Planning Commission 4; Planning Commission 5; Planning Commission 7; Planning Commission 6; Catalyze SV

Subject: Active Ground Floor on Race Street for Core Companies Proposal

Dear San Jose Planning Commission,

I am encouraged by the proposal from Core Companies to build 206 affordable homes on Race Street in Midtown San Jose.

I believe this project would greatly benefit our City and our Valley, especially if it includes active space on its ground floor.

I'm very glad this proposal will support homes for hundreds of working families and seniors on fixed incomes. We desperately need housing for these groups!

And I'm glad the developer has made improvements to the project in response to community groups such as Catalyze SV.

However, I'm concerned that the developer isn't proposing retail on the crucial commercial corridor along Race Street. This is a large project that will bring hundreds of new residents to Midtown San Jose. They need more places to walk and shop to create the vibrant neighborhoods we are building in San Jose. A project this ambitious on a street this commercially focused should have new retail on site to benefit the new residents and the existing neighbors.

At the very least, the ground floor of the buildings on Race Street should be very active to promote community interactions, safety, visual appeal and walkability.

This is a very promising project; I look forward to advocating for it in the months ahead, especially if the developer improves it further.

Thank you for considering my perspective.

Sincerely,

From: Erik Schoennauer [mailto:es@stanfordalumni.org]

Sent: Monday, February 26, 2018 9:09 AM

To: Planning Commission 2 <PlanningCom2@sanjoseca.gov>; Planning Commission 1 <PlanningCom1@sanjoseca.gov>; Planning Commission 5 <PlanningCom5@sanjoseca.gov>; Planning Commission 6 <PlanningCom6@sanjoseca.gov>; Planning Commission 3 <PlanningCom3@sanjoseca.gov>; Planning Commission 4 <PlanningCom4@sanjoseca.gov>; Planning Commission 7 <PlanningCom7@sanjoseca.gov>; Mendrin, Shaunn <shaunn.mendrin@sanjoseca.gov>

Subject: FEB 28th Agenda: Item 5.a. - Race Street Affordable Housing

Dear Planning Commissioners:

The weekly news headlines have made it clear that the housing shortage has reached crisis levels. The lack of availability and the high cost of housing are having a devastating effect on many local residents. The housing crisis is now limiting our overall economic growth in our City and the Region.

I represent The Core Companies. And, we are working in partnership with the Santa Clara County Housing Authority, with the objective of creating a 100% affordable housing development with 116 multi-family units and 90 senior units.

Please find attached letters of support from a number of community organizations and individuals in the neighborhood.

As you review our project, we hope that you will consider the following key factors:

1. The general goal of this project is to provide housing units for people at 20-60% of the Area Median Income, although the final affordability levels cannot be established until a later date. Depending upon household size, this could provide housing opportunity for households making approximately \$20,000 to \$60,000. This will make housing accessible to the senior on a fixed income, the grocery clerk, the restaurant worker, the school custodian, or the small business owner.
2. The location of the project is ideal for housing, especially affordable housing. There is easy access to bus and rapid bus lines along West San Carlos Street and The Alameda. And, all the rail transit of Diridon Station is within a reasonable distance. There are many retail, restaurant, and service jobs nearby on West San Carlos and The Alameda Urban Villages. The job center of Downtown is also reasonably close.
3. The height of the project is appropriate for the location. The site has a General Plan designation of *Urban Residential*. This designation allows structures up to 12 stories. The project proposes a 6-story and a 5-story building. Furthermore, the site is immediately adjacent to the West San Carlos Urban Village plan area, which will allow building heights up to 85 feet right next door. The proposed project zoning will allow a maximum height of just 80 feet.
4. The project will provide adequate parking contained within the buildings and out of view. Consistent with City policy, the project is working to provide the right amount of

parking and not an unnecessary and expensive oversupply of parking. The City Staff and Hexagon Transportation consultants have determined that the proposed parking is sufficient for the project.

5. The project has proposed solutions to community concerns about traffic flow. Some community members expressed concerns about traffic congestion along Race Street when the project proposed driveway access off of Race. In response, we redesigned the project so that the only vehicle access is off of Grand Avenue. This change also allows for a quality open space courtyard between the two buildings and a more positive pedestrian sidewalk area along the Race Street frontage.

For all of these reasons above, we hope that you will find the project worthy of your support. Together we can make a dent in the housing needs of our city.

Thank you for your consideration.

ERIK

--

Erik E. Schoennauer

THE SCHOENNAUER COMPANY, LLC

90 Hawthorne Way

San Jose, CA 95110

(408) 947-7774 cell AND office

(408) 947-1234 fax (call voice line first)



TRANSMITTED VIA EMAIL

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Leslye Corsiglia
Executive Director

February 23, 2017

Members of the Planning Commission
City of San Jose
200 E. Santa Clara Street
San Jose, CA

Re: February 28, 2018 Planning Commission Agenda Item 5a. Race and Grand Residential Rezoning Project (PDC17-019)

Dear Chairperson Pham, Vice Chairperson Allen, and Commissioners Ballard, Bit-Badal, Vora, and Yesney:

I am writing on behalf of SV@Home in regard to the Race and Grand Residential Rezoning Project. We support the proposed rezoning at the Race Street site, which would allow for up to 206 affordable housing units for families and seniors. Given the scarcity of land available for affordable housing development, every opportunity to advance the City's efforts to create 25,000 homes, with 40 percent being affordable to lower income households, must be leveraged. This project is one important opportunity, and we strongly support the proposal to develop a 100 percent affordable housing project on this site.

On behalf of our members, we encourage you to recommend that the City Council adopt the Race and Grand Residential Mitigated Negative Declaration and consider an ordinance to rezone the site of proposed project to the (R-M)(PD) Planned Development Zoning District, as recommended by staff.

SV@Home is encouraged by the proposal from the applicant, which includes several important features that exemplify the type of housing that the City of San Jose needs more of: 100 percent affordable housing for families and seniors, valuable amenities to serve future residents, a residential density of approximately 90 units per acre, and other design elements that can maximize the opportunity for affordable housing on the site.

As you know, San Jose's residents are facing extreme pressure due to the City's lack of affordable housing. Fifty-three percent of the City's renters are burdened by rising rents (paying over 30 percent of their income for housing), and the average monthly rent for a two-bedroom apartment is \$2,600. This means that renters must earn an annual income of over \$104,000 to afford the average two-bedroom apartment rent (City of San Jose Housing Department, San Jose Housing Market Update: Q3 2017). The creation of 100 percent affordable developments is one key strategy to address this problem, and we urge you to recommend that the City Council leverage this strategy by taking action to allowing the proposed project to move forward.

Thank you for your consideration.

Sincerely,

Pilar Lorenzana
Deputy Director



The silicon valley organization

January 19, 2018

PC Agenda: 2/28/18

Item: 5.a.

Correspondence

Mayor Sam Liccardo and City Council
City of San Jose
200 East Santa Clara Street
San Jose, CA 95113

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The Silicon Valley Organization

Re: Race Street Senior Apartments & Race Street Family Apartments

Honorable Mayor Liccardo and City Council:

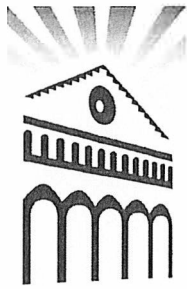
On behalf of The Silicon Valley Organization (The SVO), I am writing to support the affordable housing project put forth by The CORE Companies and the Santa Clara County Housing Authority. This project will add an additional 91 affordable senior homes to San Jose and will meet key density goals that will support San Jose's advancement as it continues to grow into a major, urban city. By way of background, we are the Silicon Valley's premier business advocacy organization representing 1,400+ companies that employ nearly 300,000 workers and we represent our membership as the region's largest Chamber of Commerce.

It should come as no surprise that Silicon Valley is experiencing a housing affordability crisis. As companies and businesses continue to expand in this region, they require an ever-growing pool of talent that will allow businesses to compete in the regional and global economy. The key to addressing this housing affordability crisis is to support additional investments and promote a regulatory framework that will significantly increase the housing supply throughout the region. This approach would bring market rate housing units down to prices that are affordable to our residents and will allow SVO member companies to continue recruiting talent, without exorbitant housing prices being a major impediment to retaining a quality workforce.

The Race Street Senior Apartments & Race Street Family Apartments project will take us one step closer to adding much-needed affordable housing units and work towards increasing the housing supply in San Jose. The SVO urges the Mayor and Council to support this project and allow it to move forward in the development review process. Should you have any further questions, please contact Eddie Truong, Director of Public Policy & Advocacy, at 408-291-5267.

Sincerely,

Matthew R. Mahood
President & CEO



WESTMINSTER PRESBYTERIAN CHURCH

Reverend Dr. Bryan James Franzen, Pastor

PC Agenda: 2/28/18
Item: 5.a.
Correspondence

Planning Commission
City of San Jose
200 East Santa Clara Street
San Jose, CA 95814-4900

February 22, 2018

RE: February 28, 2018 Planning Commission Meeting, Agenda Item 5a
Support for Planned Development Rezoning 237-253 Race St & 216-280 Grand Ave

Dear Chair Pham, Vice Chair Allen, and Honorable Planning Commissioners,

The congregation of Westminster Church would like to express our support for the proposed 206 affordable apartments of Race St and Grand Ave Residential and the Planned Development Rezoning to facilitate its development.

Westminster Presbyterian Church has been worshiping in San Jose for over 125 years and a central part of The Alameda District for over 90 years. Our presence has extended far beyond the church walls by speaking to issues of concern and building the strength of the neighborhood and city. As a leader in District 6 and Alameda Business District, our congregation is active in the Alameda Business Association and the Shasta/Hanchett Park Neighborhood Association and a key supporter of the Rose, White and Blue Parade.

Our vision for a vibrant community in San Jose is being threatened by the lack of affordable housing for working people who do not earn enough to live here. We are already seeing the effects of this housing shortage by losing lower income members and community volunteers to places like Tracy and Los Baños, leaving both a void in the jobs they held and the loss of key people that make our community strong.

Our dream for San Jose and especially The Alameda and San Carlos areas is that they are a vital and thriving community. Unfortunately, we worry that this will not happen without concerted effort to build new affordable housing. We need a diversity of housing for the diverse incomes of our neighbors. If the current trend continues, without appropriate housing for the working poor, the people who make our community healthy and vital will continue to leave in ever-increasing numbers, making this a city without a middle class.

The business community will remind us that we need affordable housing to remain competitive in the innovation economy. As a church, we need to remind you that we need economic diversity so our neighborhoods remain healthy. We also have a moral obligation to care for our neighbor and thus need to build more housing to care for and support all parts of our community.

Sincerely,

Rev. Dr. Bryan J. Franzen

WE ARE A COMMUNITY OF WELCOMING AND NURTURING FAITH

1100 SHASTA AVENUE, SAN JOSE, CA 95126 ☎ (408) 294-7447

INFO@WESTPRES-SJ.ORG

WWW.WESTPRES-SJ.ORG

From: Kelly Snider <kellysniderconsulting@gmail.com>
Sent: Friday, February 23, 2018 9:54 AM
To: Vince Cantore; (Erik) Schoennauer
Subject: Fwd: Race Street Fish

Item: 5.a.
Correspondence

Begin forwarded message:

From: <joe@guerrasolutions.com>
Date: February 23, 2018 at 8:11:33 AM PST
To: planningcom1@sanjoseca.gov, planningcom2@sanjoseca.gov, planningcom3@sanjoseca.gov,
planningcom4@sanjoseca.gov, planningcom5@sanjoseca.gov, planningcom6@sanjoseca.gov,
planningcom7@sanjoseca.gov, shaunn.mendrin@sanjoseca.gov
Subject: Race Street Fish

I live on Magnolia Avenue in the Rose Garden and I support the proposed housing project at the old Race Street Fish and Poultry site. I would appreciate your support.