



Memorandum

TO: HONORABLE MAYOR AND
CITY COUNCIL

FROM: Planning Commission

SUBJECT: SEE BELOW

DATE: April 5, 2018

COUNCIL DISTRICT: 4

SUBJECT: PDC15-027. PLANNED DEVELOPMENT REZONING FROM THE A(PD) PLANNED DEVELOPMENT ZONING DISTRICT TO THE CP(PD) PLANNED DEVELOPMENT ZONING DISTRICT TO FACILITATE THE DEMOLITION OF APPROXIMATELY 1,850 SQUARE FEET OF EXISTING COMMERCIAL BUILDINGS (RETAIL KIOSK, CARWASH) AND THE NEW DEVELOPMENT OF AN APPROXIMATELY 3,212-SQUARE FOOT CONVENIENCE STORE, A SEPARATE 2,490-SQUARE FOOT RETAIL BUILDING, AND A 1,086-SQUARE FOOT DRIVE-THROUGH CARWASH ON THE SUBJECT 1.04-GROSS ACRE SITE (1705 BERRYESSA ROAD).

RECOMMENDATION

The Planning Commission voted unanimously (7-0) to recommend that the City Council:

- (a) Adopt a resolution adopting the Mitigated Negative Declaration and related Mitigation Monitoring and Reporting Plan (MND/MMRP); and
- (b) Approve an ordinance adopting the proposed Planned Development Rezoning and Development Standards as described in the attached staff report and as recommended by staff, with modifications to the General Development Plan, including the addition of conditions related to the type of equipment used in the carwash, mitigation measures to address other impacts, and the removal of language indicating 24-hour use of the carwash from the cover page of the plan set.

OUTCOME

If the City Council adopts the MND/MMRP resolution and the ordinance approving the proposed Planned Development Rezoning and Development Standards, the applicant will be able to implement the Planned Development Zoning, demolish the existing commercial buildings, develop a convenience store, a separate retail building, and a drive-through carwash adjacent to the existing gas station with an issued development permit that is consistent with the proposed Development

Standards and the underlying General Plan Land Use/Transportation Diagram designation of Industrial Park.

BACKGROUND

On March 21, 2018, the Planning Commission held a Public Hearing to consider the proposed Planned Development Rezoning. The Planning Commission voted unanimously (7-0) to recommend approval of the proposed Rezoning.

The proposed Planned Development rezoning was included on the Consent Calendar, and staff provided a verbal update to the Commissioners regarding additions to the Development Standards for the proposed Planned Development. These changes included conditions requiring the installation of specific noise reduction equipment for the carwash dryers and reconfiguration of the site design to minimize potential noise impacts to nearby residential uses. Staff also noted the added requirements to the Development Standards to address air quality, biological resources, hazards, and hazardous materials. These development standards are derived from the Initial Study/Mitigation Negative Declaration documents, and were added to the General Development Standards to clarify and further strengthen regulation of the carwash use. Staff informed the Commission that text indicating use of the drive-through carwash “24 Hours/Day, 7 Days/Week, 365 Days/Year” had been removed from the plan set. Staff also informed the Commission that a detailed Operations Plan would be reviewed at a future date concurrent with the submittal of a Planned Development Permit and would not be approved as part of the Planned Development Rezoning. There was no other discussion and the item remained on the consent agenda.

ANALYSIS

A complete analysis of the issues regarding this project, including the Envision San José 2040 General Plan conformance is contained in the attached Planning Commission Staff Report.

EVALUATION AND FOLLOW UP

If Council approves the MND/MMRP resolution and Rezoning for this project, the applicant will be required to obtain a Planned Development Permit to demolish the existing commercial buildings and develop a convenience store, a separate retail building, and carwash facility at this gas station. The applicant has not yet filed a Planned Development Permit.

PUBLIC OUTREACH

Staff followed Council Policy 6-30: Public Outreach Policy, in that notices for the public hearings for the project were mailed to the owners and tenants of all properties located within 500 feet of the project site. An electronic version of this memorandum is available online, accessible from the City Council Agenda for the hearing on April 10, 2018. Staff has been available to discuss the proposal with members of the public.

COORDINATION

Preparation of this memorandum has been coordinated with the City Attorney's Office.

CEQA

An Initial Study (IS) and Mitigated Negative Declaration (MND) were prepared by the Director of Planning, Building, and Code Enforcement for the subject Planned Development Rezoning. The documents were circulated for public review between February 23, 2017 to March 15, 2017, and no public comments were received.

The final IS/MND states that the proposed project will not have a significant effect on the environment. The IS/MND concluded that noise from the project will not have a significant impact, as there is already an existing carwash and ambient noise from the traffic, and the noise levels generated from the proposed project were predicted to result in less than a 1dBA increase in the existing noise environment at noise-sensitive receivers and with the mitigation measures are predicted to range from 53 to 54 dBA DNL at the property boundaries. Therefore, the primary environmental issues addressed in the final Initial Study include potential impacts on the physical development of the site on air quality, biological resources, hazards and hazardous materials. The MND includes mitigation measures that would reduce any potentially significant project impacts to a less-than-significant level. In addition to mitigation measures the future Planned Development Permit will include conditions of approval to address potential impacts.

The IS/MND, the Mitigation Monitoring and Reporting Program, and other related environmental documents are available on the Planning website at <http://www.sanjoseca.gov/index.aspx?NID=5394> under File No. PDC15-027.

/s/
ROSALYNN HUGHEY, SECRETARY
Planning Commission

For questions, please contact Planning Official, Steve McHarris, at (408) 535-7819.

Attachment: Planning Commission Staff Report
Revised Draft Development Standards
Revised Cover Page of Plan Set



PLANNING COMMISSION STAFF REPORT

File No.	PDC15-027
Applicant	Muthana Ibrahim, M I Architects Inc.
Location	Northwest corner of Berryessa Road and Lundy Avenue (1705 Berryessa Road)
Existing Zoning	A(PD) Planned Development (File No. PDC95-003)
Proposed Zoning	CP(PD) Planned Development
Council District	4
Historic Resource	None
Annexation Date	December 3, 1971 (Berryessa No. 19)
CEQA	Mitigated Negative Declaration for the 1705 Berryessa Road Gas Station Project.

APPLICATION SUMMARY:

File No. PDC15-027: Planned Development Rezoning from the A(PD) Planned Development Zoning District to the CP(PD) Planned Development Zoning District to facilitate the demolition of approximately 1,850 square feet of existing commercial buildings (retail kiosk, carwash) and the new development of an approximately 3,212-square foot convenience store, a separate 2,490-square foot retail building, and a 1,086-square foot carwash tunnel on the subject 1.04-gross acre site.

RECOMMENDATION:

Planning staff recommends that the Planning Commission to the City Council the following actions:

1. Consider the Mitigated Negative Declaration for the 1705 Berryessa Gas Station Project in accordance with the California Environmental Quality Act (CEQA) and adopt a Resolution to approve the Mitigated Negative Declaration and associated Mitigation Monitoring and Reporting Plan; and
2. Adopt an Ordinance of the City of San José rezoning certain real property located at Northwest corner of Berryessa Road and Lundy Avenue (1705 Berryessa Road) from the A(PD) Planned Development Zoning District to the CP(PD) Planned Development Zoning District on a 1.04-gross acre site to facilitate the demolition of approximately 1,850 and facilitate the new development of an approximately 3,212-square foot convenience store, a separate 2,490-square foot retail building, and a 1,086-square foot carwash tunnel.

PROJECT DATA

GENERAL PLAN CONSISTENCY			
General Plan Designation		Urban Village <input checked="" type="checkbox"/> Consistent <input type="checkbox"/> Inconsistent	
Consistent Policies		LU-5.1, LU-5.4, LU-5.6, and VN-1.7	
SURROUNDING USES			
	General Plan Land Use	Zoning	Existing Use
North	Urban Village	CN Commercial Neighborhood	Lundy Avenue and Gas Station
South	Residential Neighborhood	A(PD) Planned Development (File No. PDC83-070)	Residential Duplex
East	Urban Village	CP Commercial Pedestrian	Commercial & Retail Shopping Center
West	Residential Neighborhood	A(PD) Planned Development (File No. PDC83-070)	Residential Duplex

RELATED APPROVALS	
Date	Action
6/30/95	Planned Development Permit to allow the construction of kiosk, gas station and car wash (PD95-035)
4/4/95	Planned Development Rezoning approved to develop a Gas Station and Car Wash development. (File PDC95-003)

PROJECT DESCRIPTION

On June 24, 2015, a Planned Development Rezoning application was filed to rezone the subject site from the A(PD) Planned Development to the CP(PD) Planned Development Zoning District on a 1.04-gross acre site. The rezoning would facilitate a future Planned Development Permit to allow the demolition of approximately 1,850 square feet of commercial buildings (retail kiosk, carwash) and the new development of an approximately 3,212-square foot convenience store, a separate 2,490-square foot retail building, and a 1,086-square foot carwash.

The General Development Plan approved as part of the existing A(PD) Planned Development Zoning District (File No. PDC95-003) at the subject site established exact locations for all of the existing buildings and provided for only the existing gas station and carwash buildings. Therefore, any additions to the site or changes to building footprints and/or uses require rezoning the property to change the General Development Plan. The proposed rezoning would provide design flexibility and help ensure compatibility with the Land Use, Transportation, and Urban Village policies of the Envision San José 2040 General Plan. The new Planned Development Zoning District would facilitate redevelopment of the existing site and incorporate reduced setbacks for the new buildings to be located closer to the street frontage. The conceptual site plans submitted with this rezoning application maintain the existing fueling pumps but propose the future construction of a new convenience store, a separate retail building along Berryessa Road, and a new carwash tunnel.

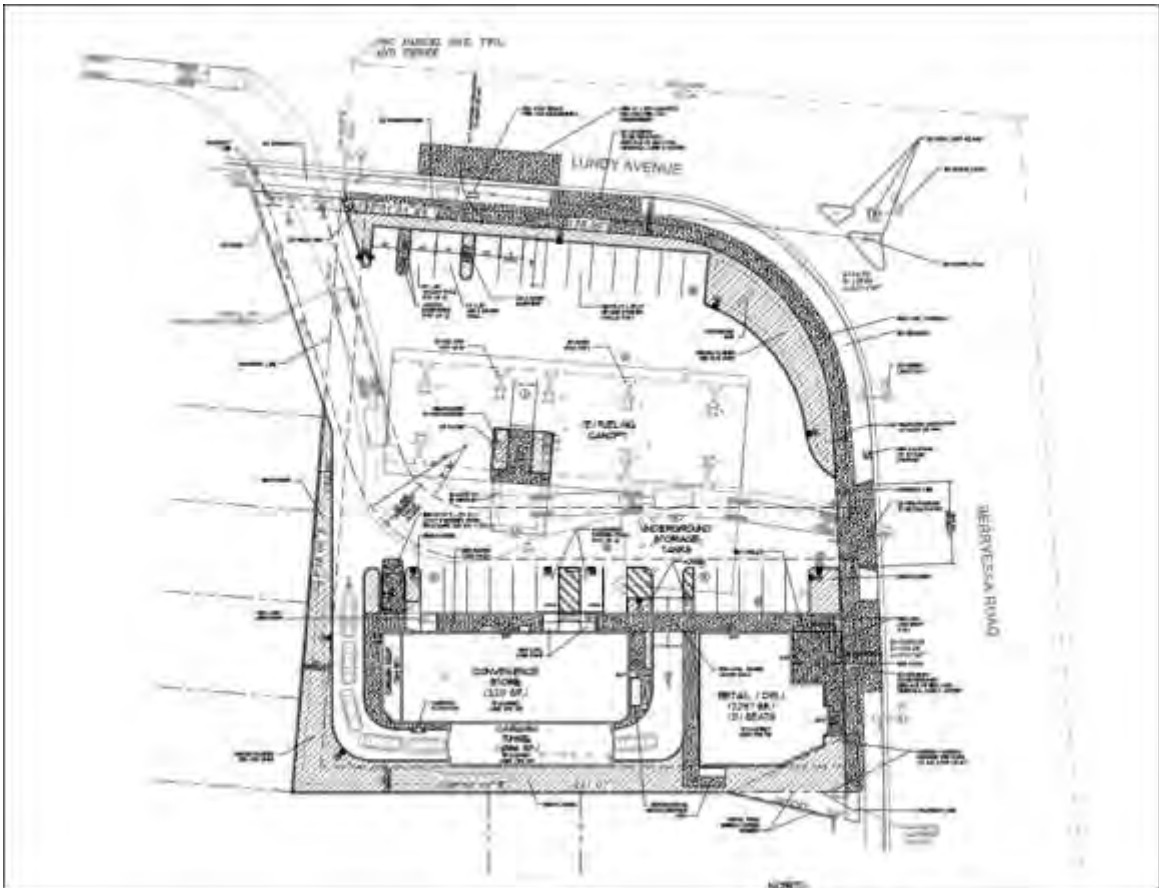


Figure 1: Conceptual Site Plan (larger plan set attached to Staff Report)



Figure 2: Aerial of Subject Site

Site Location and Surrounding uses:

The subject 1.04-gross acre site is located at the northwest corner of Berryessa Road and Lundy Avenue. The site is currently developed with a gas station built in 1999 with eight fueling pumps, a 544-square foot retail kiosk, and a 1,307-square foot carwash tunnel. The applicant intends to keep the existing pumps and fueling tanks while demolishing the existing kiosk and carwash tunnel and replacing those buildings with a convenience store, separate commercial/retail building and a modern carwash facility. The site is adjacent to an existing multi-family development to the south and west. Across Lundy Avenue (four-lane road) and Berryessa Road (six-lane road) are an existing gas station and two commercial strip malls. Additionally, along Lundy adjacent to the north property line is an existing Santa Clara Valley Transportation Authority (VTA) bus stop.

ANALYSIS

This Planned Development Rezoning was analyzed with respect to conformance with the Envision San José 2040 General Plan, Urban Village Policies, Zoning Ordinance, and CEQA.

Envision San José 2040 General Plan Conformance

The subject site is located within the Berryessa BART Urban Village boundary and has an Urban Village land use designation on the General Plan Land Use/Transportation Diagram. Prior to preparation of an Urban Village Plan, this designation supports uses consistent with those of the Neighborhood Community Commercial designation. This designation allows a broad range of commercial uses including retail, personal services, office, hospitals, and community gathering facilities. The proposed rezoning would facilitate the redevelopment of an existing gas station and the construction of a new convenience store, retail building, and carwash, which would provide retail services to the nearby community consistent with the General Plan land use designation.



UV: Urban Village (NCC Uses)

RN: Residential Neighborhood

Figure 3: General Plan Map of Project Site and Surroundings

In addition to conforming to the General Plan Land Use/Transportation Diagram, the project is also in conformance with the following General Plan policies as discussed below:

1. Land Use Policy LU-5.1: In order to create complete communities, promote new commercial uses and revitalize existing commercial areas in locations that provide safe and convenient multi-modal access to a full range of goods and services.

Analysis: The proposed rezoning would facilitate the redevelopment of the existing site to develop a larger convenience store and enable it to provide a broader selection of foods and services. Additionally, the conceptual plan includes a proposed retail building with frontage on Berryessa Road. The proposed development is directly adjacent to a VTA bus stop and within 2,000 feet of the proposed new BART station.

2. Land Use Policy LU-5.4: Require new commercial development to facilitate pedestrian and bicycle access through techniques such as minimizing building separation from public sidewalks; providing safe, accessible, convenient, and pleasant pedestrian connections.

Analysis: The existing kiosk is located in the center of the lot and does not have direct connection from the street frontages. The conceptual site plan and Development Standards show buildings located within ten feet of a street frontage, which minimizes building separation from the public realm and would facilitate safe and convenient pedestrian connections.

3. Land Use Policy LU-5.6: Encourage and facilitate the upgrading, beautifying, and revitalization of existing strip commercial areas and shopping centers. Minimize the visual impact of large parking lots by locating them away from public streets.

Analysis: The proposed project would upgrade and expand an approximately twenty-year-old existing facility, and enable the operators to improve and expand the site. Proposed site improvements to be considered under a future Planned Development Permit would include upgraded outdoor lighting, landscaping and modifications to the existing driveways to make the subject site safer and more aesthetically appealing. The proposed General Development Plan provides sufficient parking per the Municipal Code. As part of the Planned Development Permit additional landscaping will be required along Lundy Avenue to screen the parking areas and reduce visual impacts.

4. Vibrant Neighborhood Policy VN-1.7: Use new development within neighborhoods to enhance the public realm, provide for direct and convenient pedestrian access, and visually connect to the surrounding neighborhood. As opportunities arise, improve existing development to meet these objectives as well.

Analysis: The conceptual commercial/retail building fronts along Berryessa Road with new walkways and landscaping to improve the pedestrian access and street visibility. The new building would help frame the street and additional landscaping would soften existing hardscape.

Consistency with Urban Village Boundary Designation

The subject project is located within the boundaries of the Berryessa BART Urban Village, which is identified as a Horizon 2 Urban Village. Projects developed prior to the adoption of an Urban Village plans should adopt measures to promote pedestrian design principles. The proposed development standards and conceptual design elements are consistent with these principles with a greater emphasis on placing buildings closer to the sidewalk and fronting on major streets. Additionally, this sets a pedestrian-orientated precedent for any future development on Berryessa Avenue. Although the General Plan discourages drive-through uses within Urban Village boundaries, the site has is an existing gas station with a carwash use.

Therefore, the applicant may redevelop and upgrade these existing uses while incorporating additional pedestrian-focused designs. The General Development Plan for this Planned Development Zoning District provides flexibility as it allows any use in the CN Commercial Neighborhood Zoning District while requiring more reduced setbacks along the street frontage and allowing the site improvements and modifications to enhance pedestrian safety and accessibility.

Conformance with City of San José Drive-Through Policy

The conceptual site development plan would conform to the City's Drive-Through policy 6-10. The location of the proposed drive-through use would provide sufficient space for stacking of vehicles and proposed ingress/egress to the site therefore would not have any adverse impacts on the intersection of Berryessa Road and Lundy Avenue. The site would improve street circulation by removing a driveway along Lundy Avenue. The site has an existing carwash that is located in the same location as conceptual location of the new carwash tunnel. A noise report was conducted as part of the evaluation of the project and concluded that based on existing noise levels experienced in the vicinity of the project site, project-generated average day-night noise levels are predicted to be at or below ambient noise levels in the majority of the project study area. Mitigation measure such as sound attenuation walls and modern dryer system mitigate potential impacts to the adjacent residential uses and would therefore conform to the General Plan noise policies. Additional review of the specific site design and operation of the car wash facility will be conducted with the submittal of a Planned Development Permit application.

Zoning Code Conformance

The site is currently in the A(PD) Planned Development Zoning District, which established the exact location of the building footprints and does not allow modifications or increase in commercial development. The proposed PD rezoning would allow flexibility to allow Commercial Neighborhood uses, while including pedestrian oriented setbacks that would require buildings to front along the street and further meet the Urban Village policies for pedestrian site design and access. The General Development Plan would require the following setbacks:

Buildings minimum setback requirements:	
Setback Line	Setback Distance in feet
Building to the front (Lundy Ave)	Zero minimum*
Building to the front (Berryessa Rd)	Zero minimum*
Building to rear & side	10

** At least one of the buildings must have frontage no further than 10 feet from the public right-of-way from either Lundy Avenue or Berryessa Road.*

The Planned Development Zoning District enables the redevelopment of an existing use and improves the site layout and provides additional commercial and retail amenities to the surrounding area, while incorporating additional development standards to further the Urban Village and General Plan policies to include a building present along the public right-of-way.

CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA)

An Initial Study (IS) and Mitigated Negative Declaration (MND) were prepared by the Director of Planning, Building, and Code Enforcement for the subject Planned Development Rezoning. The documents were circulated for public review between February 23, 2017, to February March 15, 2017, and no public comments were received.

The final IS/MND states that the proposed project will not have a significant effect on the environment. The IS/MND concluded that noise from the project will not have a significant impact as there is already an existing carwash and ambient noise from the traffic, that noise levels generated from the proposed project were predicted to result in less than a 1dBA increase in the existing noise environment at noise-sensitive receivers. Therefore, the primary environmental issues addressed in the final Initial Study include potential impacts on the physical development of the site on: noise, air quality, biological resources, and hazards and hazardous materials. The MND includes mitigation measures that would reduce any potentially significant project impacts to a less-than-significant level. In addition to mitigation measures the future Planned Development Permit will include conditions of approval to address potential impacts.

The entire IS/MND, the Mitigation Monitoring and Reporting Program, and other related environmental documents are available on the Planning website at <http://www.sanjoseca.gov/index.aspx?NID=5394>

PUBLIC OUTREACH

Staff informed the public of the proposed project in accordance with Council Policy 6-30. A notice of the public hearing was distributed to the owners and tenants of all properties located within 500 feet of the project site and posted on the City website. The staff report was also posted on the City's website prior to the hearing. Staff has been available to respond to questions from the public.

Project Manager: Tong (John) Tu

Approved by:



Planning Official for Rosalynn Hughey,
Acting Director of Planning, Building and Code Enforcement

Date: 3/9/18

Attachments:

- A. Draft Development Standards
- B. Draft Ordinance
- C. Reduced Plan Sets

Owner:	Applicant:
A U Energy LLC 41805 Albrae Street Fremont, CA 94538	MI Architects Inc. 2221 Olympic Boulevard, Suite 100 Walnut Creek, CA 94595

General Development Standards*
File No. PDC15-027
CP(PD) Planned Development Zoning District

**In any cases where the graphic plans and text may differ, this text takes precedence*

ALLOWED USES

- Permitted, Conditional, and Special Uses of the CN – Commercial Neighborhood Zoning District of Title 20 of the San José Municipal Code, as may be amended in the future. Conditional and Special Uses as identified in the CN – Commercial Neighborhood Zoning District shall be subject to approval of a Planned Development Permit or Amendment by the City of San José Planning Director.

DEVELOPMENT STANDARDS

SETBACKS

Buildings shall meet the following minimum setback requirements:

Setback Line	Setback Distance in feet
Building to the front (Lundy Ave)	Zero minimum*
Building to the front (Berryessa Rd)	Zero minimum*
Building to rear & side	10

** At least one of the buildings must have frontage no further than 10 feet from the public right-of-way from either Lundy Avenue or Berryessa Road.*

HEIGHT

- Height is per code as amended per Title 20 of the San José Municipal Code, Section 20.40.200, as may be amended.

PARKING

- Parking spaces shall be provided per Title 20 of the San José Municipal Code, Section 20.90, as may be amended.
- Bicycle Parking shall be provided per Title 20 of the San José Municipal Code, Section 20.90, as may be amended.

PERFORMANCE STANDARDS

- Performance standards shall be per Section 20.40.600 of the San José Municipal Code, as may be amended.

ENVIRONMENTAL MITIGATION

- Implement the mitigation measures identified in the Initial Study (IS) and Mitigated Negative Declaration (MND) for the 1705 Berryessa Road Development Project (File No. PDC15-027) and Mitigation Monitoring and Reporting Program, as may be amended.

DRAFT

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY OF SAN JOSE REZONING CERTAIN REAL PROPERTY OF APPROXIMATELY 1.04 GROSS ACRES SITUATED ON THE NORTHWEST CORNER OF BERRYESSA ROAD AND LUNDY AVENUE (1705 BERRYESSA ROAD) FROM THE A(PD) PLANNED DEVELOPMENT ZONING DISTRICT TO THE CP(PD) PLANNED DEVELOPMENT ZONING DISTRICT

WHEREAS, all rezoning proceedings required under the provisions of Chapter 20.120 of Title 20 of the San José Municipal Code have been duly had and taken with respect to the real property hereinafter described; and

WHEREAS, a Mitigated Negative Declaration was prepared in conformance with the California Environmental Quality Act of 1970 (CEQA), as amended, for the subject rezoning to CP(PD) Planned Development Zoning District under File No. PDC15-027 (the “MND”); and

WHEREAS, the City Council of the City of San José is the decision-making body for the proposed subject rezoning to CP(PD) Planned Development Zoning District; and

WHEREAS, this Council of the City of San José has considered, approved and adopted said MND and related Mitigation Monitoring and Reporting Program under separate Council resolution prior to taking any approval actions on this project.

NOW, THEREFORE, BE IT ORDAINED BY THE COUNCIL OF THE CITY OF SAN JOSE:

SECTION 1. The recitals above are incorporated herein.

SECTION 2. All that real property hereinafter described in this section, hereinafter referred to as "subject property," is hereby rezoned as CP(PD) Planned Development Zoning District. The base district zoning of the subject property shall be the CP Commercial Pedestrian Zoning District. The Planned Development zoning of the subject property shall be that July 27, 2014 development plan for the subject property entitled, "General Development Plan – File No. PDC15-027."

Said General Development Plan is on file in the office of the Director of Planning and is available for inspection by anyone interested therein, and said General Development Plan is by this reference adopted and incorporated herein the same as if it were fully set forth herein.

The subject property referred to in this section is all that real property situated in the County of Santa Clara, State of California, described in Exhibit "A" attached hereto and incorporated herein by this reference.

SECTION 3. The district map of the City is hereby amended accordingly.

SECTION 4. The land development approval that is the subject of City File No. PDC15-027 is subject to the operation of Part 2.75 of Chapter 15.12 of Title 15 of the San José Municipal Code. The applicant for or recipient of such land use approval hereby acknowledges receipt of notice that the issuance of a building permit to implement such land development approval may be suspended, conditioned or denied where the City Manager has determined that such action is necessary to remain within the aggregate operational capacity of the sanitary sewer system available to the City of San José or to meet the discharge standards of the sanitary sewer system imposed by the California Regional Water Quality Control Board for the San Francisco Bay Region.

PASSED FOR PUBLICATION of title this _____ day of _____, 2018 by the following vote:

AYES:

NOES:

ABSENT:

DISQUALIFIED:

SAM LICCARDO
Mayor

ATTEST:

TONI J. TABER, CMC
City Clerk

SHELL GAS STATION, CARWASH,
"LOOP" CONVENIENCE STORE
& RETAIL BUILDING

1705 BERRYESSA RD.
SAN JOSE, CALIFORNIA 95133

DRAWING INDEX

- 1 TITLE SHEET
- 2 LAND USE PLAN
- 21 STANDARD DEVELOPMENT PLAN
- 31 EXISTING SITE PLAN
- 32 CONCEPTUAL SITE PLAN
- 4 CONCEPTUAL GRADING PLAN
- 5 CONCEPTUAL STORMWATER CONTROL PLAN
- 7A CONCEPTUAL CONVENIENCE STORE / CARWASH BUILDING ELEVATIONS
- 7B CONCEPTUAL RETAIL BUILDING ELEVATIONS
- 9A CONCEPTUAL CONVENIENCE STORE / CARWASH BUILDING FLOOR PLAN
- 9B CONCEPTUAL RETAIL BUILDING FLOOR PLAN
- 10 CONCEPTUAL LANDSCAPE PLAN
- 11 CONCEPTUAL SITE PHOTOMETRIC
- 12 SITE DETAILS

PROJECT DESCRIPTION

PRIOR DEVELOPMENT PERMITS:
• PD05-003
• PD05-035

PROPOSED USES:
• SHELL GAS STATION, "LOOP" CONVENIENCE STORE, DRIVE-THRU CARWASH & RETAIL / DELI BUILDING

PROJECT DEMOLITION:
• REMOVE EXISTING SNACK SHOP BUILDING
• REMOVE EXISTING CARWASH BUILDING
• REMOVE TWO (2) DRIVEWAYS, ONE ALONG LINDAY AVE. FRONTAGE & ONE ALONG BERRYESSA RD FRONTAGE

NEW CONSTRUCTION:
• CONSTRUCT A 3212 S.F. CONVENIENCE STORE W/ ATTACHED 1086 SELF SERVE CARWASH TUNNEL & 264 S.F. CARWASH EQUIPMENT ROOM
• CONSTRUCT A 2241 S.F. RETAIL / DELI BUILDING

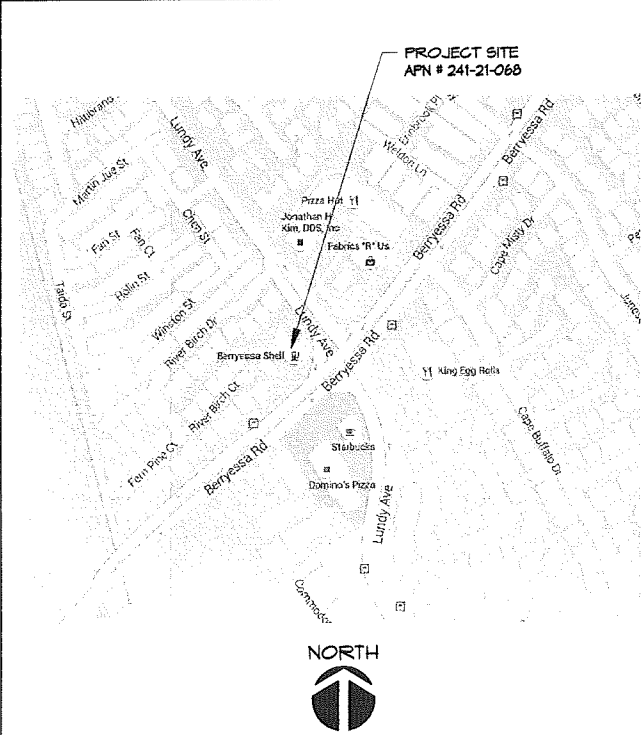
NEW SITE IMPROVEMENTS:
• NEW 32 FOOT WIDE DRIVEWAY ALONG THE BERRYESSA RD FRONTAGE
• NEW 4'-8" DEDICATION TO RIGHT OF WAY ALONG BERRYESSA RD
• NEW 4'-11" DEDICATION TO RIGHT OF WAY ALONG LINDAY AVE.
• NEW ONSITE PARKING STALLS
• NEW ACCESSIBLE PATH OF TRAVEL TO THE RIGHT-OF-WAY
• NEW MASONRY TRASH ENCLOSURE
• NEW SITE LIGHTING
• NEW LANDSCAPING
• NEW SELF SERVICE AIR/WATER AND VACUUM UNITS

ADDITIONAL OPERATIONAL FACILITY INFORMATION:
• HOURS OF OPERATION FOR THE GAS STATION CONVENIENCE STORE & CARWASH SHALL BE: 24 HRS. / DAY, 7 DAYS / WEEK, 365 DAYS / YEAR

PROJECT DIRECTORY

ARCHITECT M I ARCHITECTS, INC. 2221 OLYMPIC BLVD. SUITE 100 WALNUT CREEK, CA 94595 TEL: (925) 287-1174 FAX: (925) 943-1581 CELL: (925) 878-4875 MR. MUTHANA IBRAHIM, ARCHITECT	LANDSCAPE ARCHITECT GIARDELLA ASSOCIATES 640 MENLO AVE. SUITE 10 MENLO PARK, CA 94025 TEL: (650)326-6100 FAX: (650)323-6106 CELL: - MR. RICHARD GIARDELLA
CIVIL ENGINEER STUKAH CONSULTING ENGINEERS, INC. 11844 COLIMA ROAD SUITE 229C GOLDENVIEW, CA 95610 TEL: (416) 835-5741 FAX: (416) 988-6316 MR. FAREED T. SIDDIGU, P.E.	DEVELOPER A U ENERGY, LLC 41805 ALBERKE ST., 2ND FLR. FREMONT, CA 94538 TEL: (650) 868-1454 FAX: - MR. NICK GOYAL

LOCATION MAP



M I Architects, Inc.
ARCHITECTURE
PLANNING
MANAGEMENT
DESIGN
2221 OLYMPIC BLVD.,
SUITE 100
WALNUT CREEK, CA
94595
925-287-1174 Tel
925-943-1581 Fax
925-878-4875 Cell
muthana@miarchitect.com
www.miarchitect.com

(PD05-027)

DRRAWINGS AND SPECIFICATIONS AND THE CONCEPTS EMBODIED WITHIN THEM CONSTITUTE THE ORIGINAL UNPUBLISHED WORK OF M I ARCHITECTS, INC. ANY REPRODUCTION OR DISSEMINATION OF THIS DOCUMENT WITHOUT THE WRITTEN CONSENT OF M I ARCHITECTS, INC. IS PROHIBITED.

SHELL GAS STATION, CARWASH,
"LOOP" CONVENIENCE STORE & RETAIL BLDG
1705 BERRYESSA RD.
SAN JOSE, CA 95133

ISSUED FOR CONSTRUCTION
ISSUED FOR PLAN CHECK
07-27-14 ISSUED FOR PLANNING

NO.	DATE	DESCRIPTION
1		
2		
3		
4		
5		

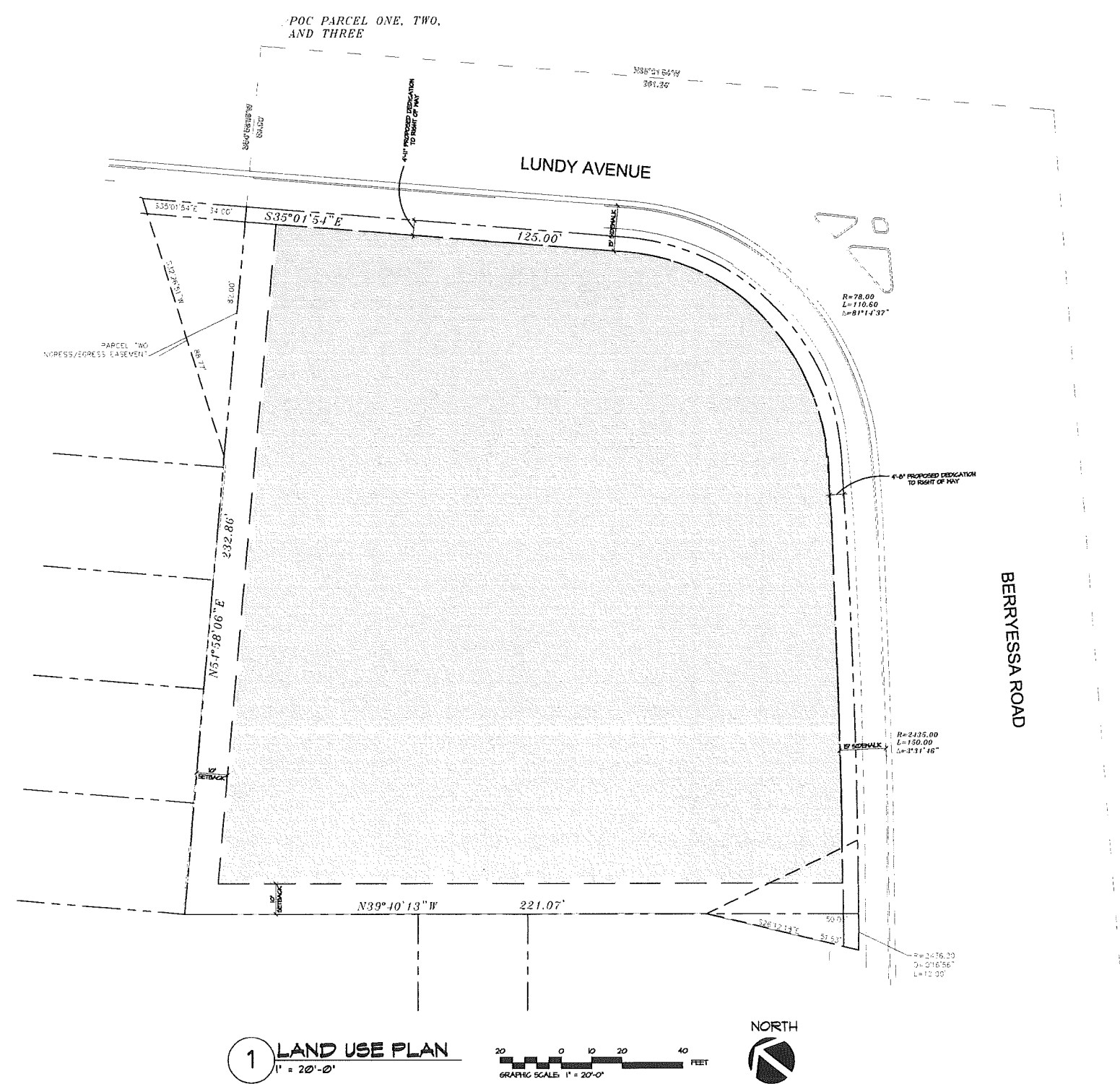
TITLE SHEET

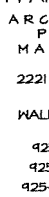
PROJECT #: 14-5064
DRAWN: BB CHECKED: MI
SCALE: AS NOTED DATE: 5-11-15

1

SHEET OF

LEGEND	
	CN COMMERCIAL NEIGHBORHOOD USE (AMENDED)





M | Architects, Inc.

ARCHITECTURE
PLANNING
MANAGEMENT
DESIGN

2221 OLYMPIC BLVD.,
SUITE 100
WALNUT CREEK, CA
94595
925-287-1174 Tel
925-943-1501 Fax
925-878-4875 Cell

muthana@marchitect.com
www.marchitect.com

**SHELL GAS STATION, CARWASH,
"LOOP" CONVENIENCE STORE & RETAIL BLDG
1705 BERRYESSA RD.
SAN JOSE, CA 95133**

(PDC15-027)

DRAWINGS AND SPECIFICATIONS AND THE CONCEPTS DESCRIBED WITHIN THESE CONSTITUTE THE ORIGINAL UNPUBLISHED WORK. M1 ARCHITECTS, INC. IS THE AUTHOR OF THESE DRAWINGS AND SPECIFICATIONS. ANY REPRODUCTION OR USE OF THESE DRAWINGS AND SPECIFICATIONS WITHOUT THE WRITTEN CONSENT OF M1 ARCHITECTS, INC. IS PROHIBITED. THE USE, DUPLICATION OR DISCLOSURE OF THE DOCUMENTS WITHOUT EXPRESSED WRITTEN CONSENT FROM M1 ARCHITECTS, INC. IS PROHIBITED.

NO.	DATE	DESCRIPTION
△		
△		
△		
△		

LAND USE PLAN

PROJECT #: 14-5064

DRAWN: BB

CHECKED: MI

SCALE: AS NOTED

DATE: 5-11-15

2

SHEET OF

DEVELOPMENT STANDARDS

General Development Standards*
File No. PDC15-027
(Berryessa Road)

*In any cases where the graphic plans and text may differ, this text takes precedence

ALLOWED USES

- Permitted, Conditional, and Special uses of the CN – Commercial Neighborhood Zoning District of Title 20 of the San José Municipal Code, as may be amended in the future. Conditional and Special uses as identified in the CN – Commercial Neighborhood Zoning District shall be subject to approval of a Planned Development Permit or Amendment by the City of San José Planning Director.

DEVELOPMENT STANDARDS

SETBACKS

At a minimum, buildings and surface parking lots shall meet the following setback requirements:

Setback Line	Setback Distance in feet
Building to the front (Lundy Ave)	Zero feet*
Building to the front (Berryessa Rd)	Zero feet*
Building to rear & side	10 feet

* Buildings along at least one street frontage (Lundy Avenue or Berryessa Road) is required to be setback no more than 10 feet from the public right-of-way.

HEIGHT

- Height is per code as amended per Title 20 of the San José Municipal Code, Section 20.40.200, as may be amended.

PARKING

- Parking spaces shall be provided per Title 20 of the San José Municipal Code, Section 20.90, as may be amended.
- Bicycle Parking shall be provided per Title 20 of the San José Municipal Code, Section 20.90, as may be amended.

PERFORMANCE STANDARDS

- Performance standards shall be per Section 20.40.600 of the San José Municipal Code, as may be amended.



Architects

M I Architects, Inc.

ARCHITECTURE
PLANNING
MANAGEMENT
DESIGN

2221 OLYMPIC BLVD.,

SUITE 100

WALNUT CREEK, CA

94595

925-287-1174 Tel

925-443-1581 Fax

925-878-9875 Cell

mthanadmiarchitect.com

www.miarchitect.com

SHELL GAS STATION, CARWASH,
"LOOP" CONVENIENCE STORE & RETAIL BLDG
1705 BERRYESSA RD.
SAN JOSE, CA 95133

(PDC15-027)

DRAWINGS AND SPECIFICATIONS AND THE CONVENTS DESCRIBED WITHIN CONSTITUTE THE ORIGINAL UNPUBLISHED WORK OF M I ARCHITECTS, INC. ANY REPRODUCTION OR TRANSMISSION OF THIS DOCUMENT WITHOUT WRITTEN CONSENT FROM M I ARCHITECTS, INC. IS PROHIBITED.

- ISSUED FOR CONSTRUCTION
- ISSUED FOR PLAN CHECK
07-27-14 ISSUED FOR PLANNING

NO.	DATE	DESCRIPTION
Δ		
Δ		
Δ		
Δ		
Δ		

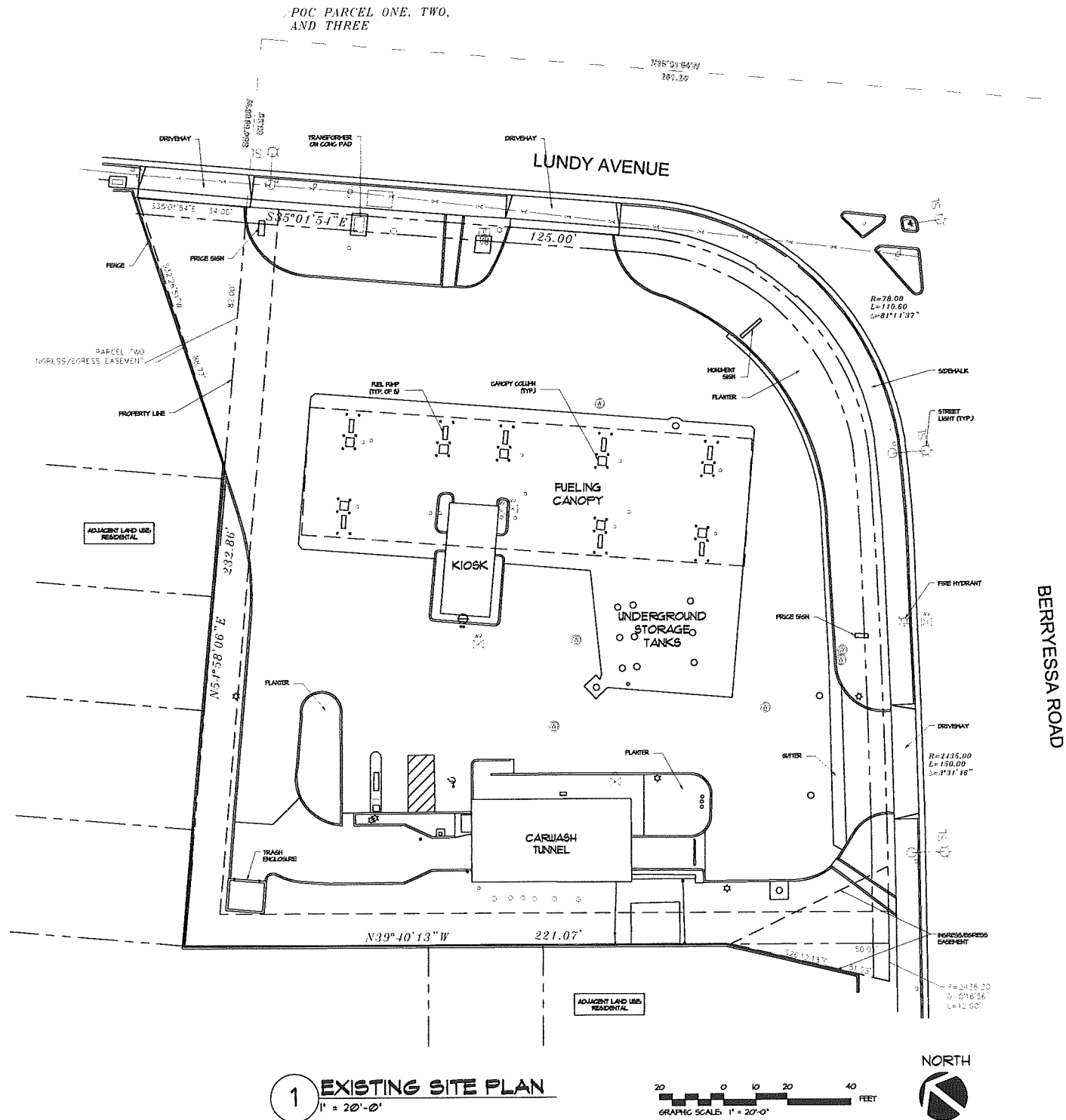
DEVELOPMENT
STANDARDS

PROJECT #: 14-5064
DRAWN: BB CHECKED: MI
SCALE: AS NOTED DATE: 11-11-15

2.1

SHEET OF

S:\1-Projects\14-5064 1705 Berryessa San Jose\Drawings\Planning\14-5064-SUB-1.dwg modified by mianar2 at Mar 08, 2018 - 9:05am



SITE DEMOLITION INFO

BUILDINGS TO BE DEMOLISHED:
CARWASH TUNNEL: 1307 SF.
KIOSK: 544 SF.



Architects

M I Architects, Inc.

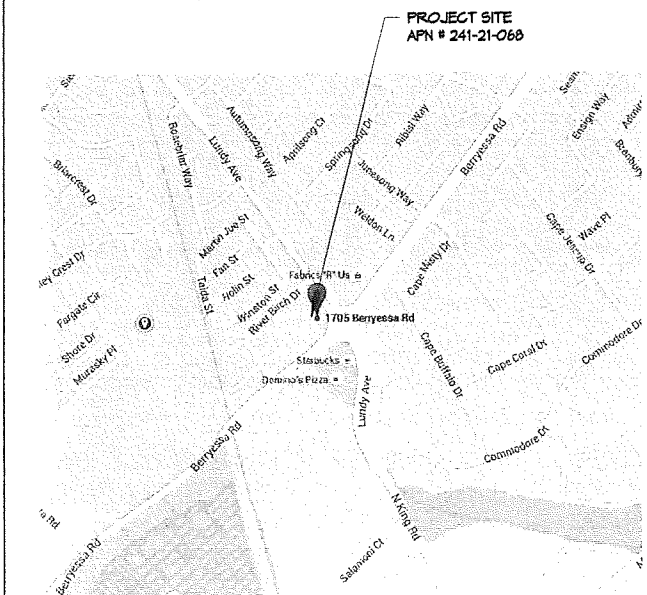
ARCHITECTURE
PLANNING
MANAGEMENT
DESIGN
2221 OLYMPIC BLVD.,
SUITE 100
WALNUT CREEK, CA
94545
925-287-1174 Tel
925-443-1581 Fax
925-878-4875 Cell
mthanam@miarchitect.com
www.miarchitect.com

SHELL GAS STATION, CARWASH,
"LOOP" CONVENIENCE STORE & RETAIL BLDG.
1705 BERRYESSA RD.
SAN JOSE, CA 95133
(PDC15-027)

THESE DRAWINGS AND SPECIFICATIONS AND THE CONCEPTS EXPRESSED THEREIN CONSTITUTE THE ORIGINAL AND UNPUBLISHED WORK OF M I ARCHITECTS, INC. ANY REPRODUCTION OR TRANSMISSION OF THESE DRAWINGS AND SPECIFICATIONS WITHOUT THE WRITTEN CONSENT OF M I ARCHITECTS, INC. IS PROHIBITED.

LOCATION MAP

SCALE: 1" = 500'



ISSUED FOR CONSTRUCTION
ISSUED FOR PLAN CHECK
07-27-14 ISSUED FOR PLANNING

NO.	DATE	DESCRIPTION
1		
2		
3		
4		

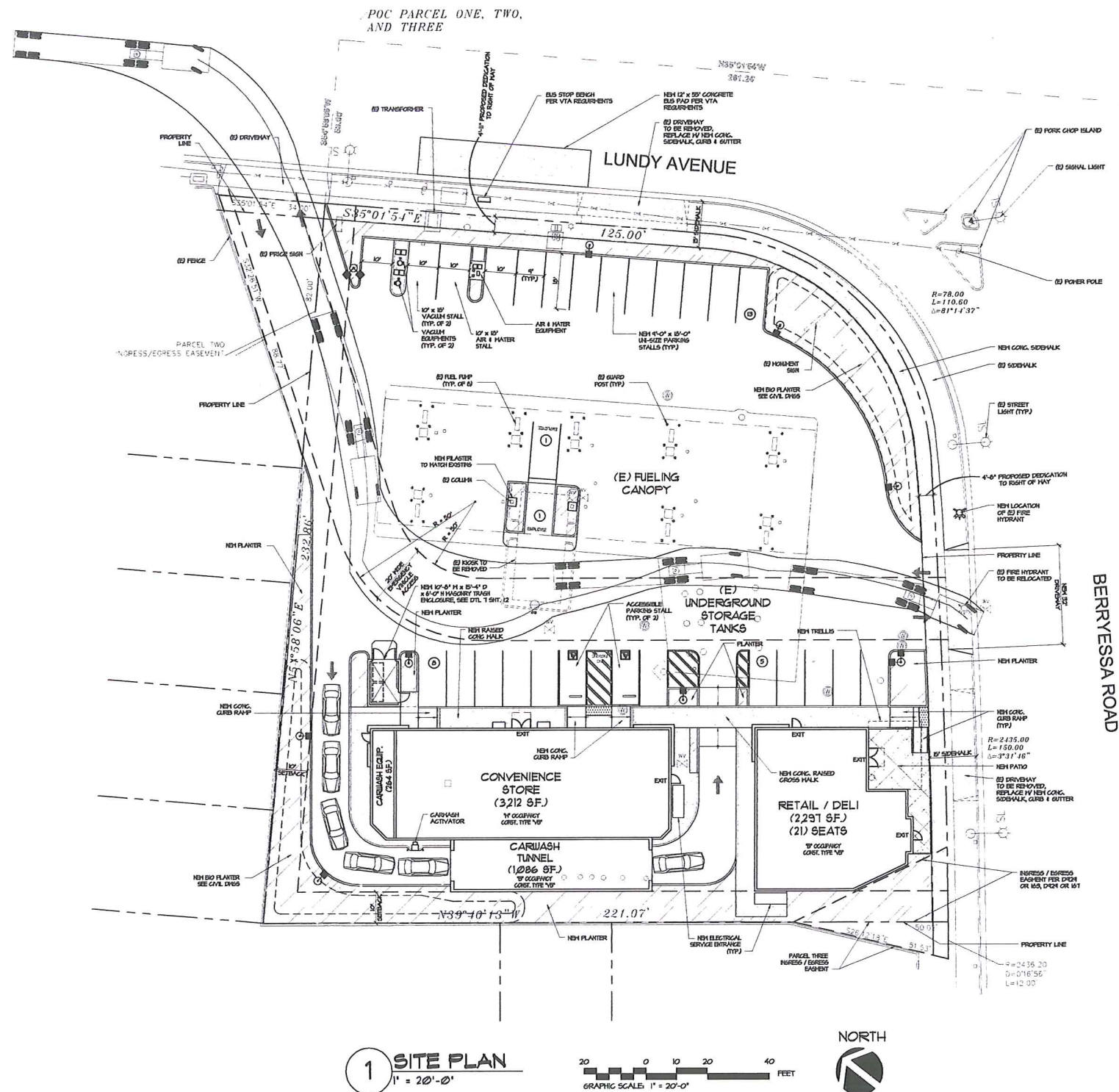
EXISTING SITE PLAN

PROJECT #: 14-5064
DRAWN: BB
CHECKED: MI
SCALE: AS NOTED DATE: 5-11-15

3.1

SHEET

OF



SITE INFORMATION

APN# 241-21-068

JURISDICTION: CITY OF SAN JOSE, CA
CURRENT ZONING: (PD) PLANNED DEVELOPMENT
PRIOR PERMIT: PDC45-003
PROPOSED ZONING: CN - PD PLANNED DEVELOPMENT

EXISTING USE: GAS STATION, SNACK SHOP & CARWASH

PROPOSED USE: GAS STATION, CONVENIENCE STORE, RETAIL BUILDING & CARWASH

LOT AREA: 45,640 S.F.

SETBACKS, BUILDING AND/OR PARKING & CIRCULATION:

FRONT-PROPOSED DEDICATION TO RIGHT OF WAY (BERRYESSA RD): 4'-0"
INTERIOR SIDE: 10 FEET
ZONING: 10 FEET
SIDE-PROPOSED DEDICATION TO RIGHT OF WAY (LUNDY AVE): 4'-11"

PARKING REQUIREMENTS:
RETAIL / DELI:

1 SPACE PER 200 S.F.
(2,291 S.F. + 3,212 S.F.) x 0.05 = 4,682
4,682 / 200 = 23 SPACES

GAS STATION:
1 SPACE PER AIR/WATER = 1 SPACE
1 SPACE FOR INFORMATION STOP = 1 SPACE
(OVERLAP W/ CONVENIENCE STORE)

CARWASH:
VEHICLE STACKING FOR (5) CARS

PARKING REQUIRED: 24 SPACES

PARKING PROVIDED: 33 SPACES

UNI-SIZE PARKING STALLS (8.5'x17'): 24 SPACES
VAN ACCESSIBLE PARKING STALL (17'x10'): 2 SPACES
50% FUEL CANOPY POSITIONS: 7 SPACES

SITE INFO TABLE:

BUILDING	EXISTING	PROPOSED (NEW)	PROPOSED (DEMOLISHED)
CONVENIENCE STORE/KIOSK	544 S.F.	3,212 S.F.	544 S.F.
CARWASH TUNNEL	1,307 S.F.	1,086 S.F.	1,307
CARWASH EQUIP.	N/A	264 S.F.	
RETAIL	N/A	2,291 S.F.	

SITE COVERAGE:

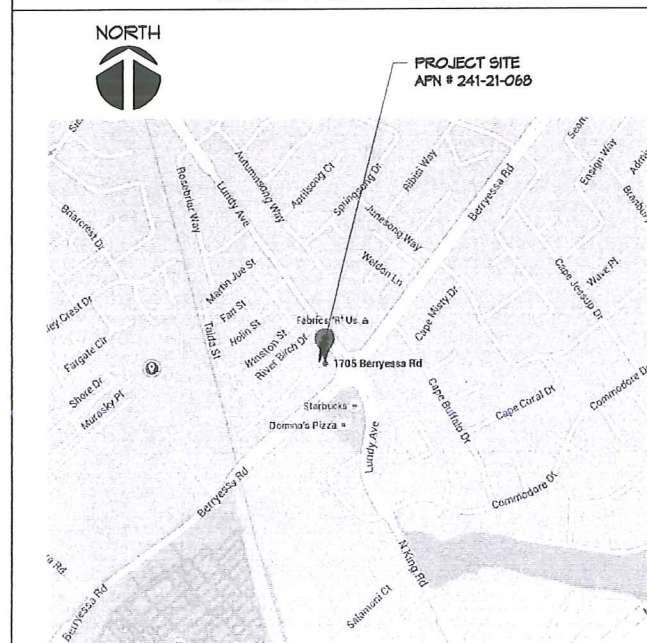
TOTAL PROPOSED BUILDINGS: 6,854 S.F.
PERCENTAGE OF PROPOSED COVERAGE: 15%
OPEN SPACE: 85%

SITE PLAN LEGEND

- LANDSCAPING
- NEW CONCRETE PAVING
- BIO PLANTER, SEE CIVIL DWGS.
- EXISTING TO REMAIN
- EXISTING CURB TO REMAIN
- NEW CONCRETE CURB

LOCATION MAP

SCALE: 1" = 500'



ISSUED FOR CONSTRUCTION
ISSUED FOR PLAN CHECK
07-21-14 ISSUED FOR PLANNING

NO.	DATE	DESCRIPTION
1		
2		
3		
4		
5		

CONCEPTUAL SITE PLAN

PROJECT # 14-5064
DRAWN: BB CHECKED: MI
SCALE: AS NOTED DATE: 5-11-15

3.2

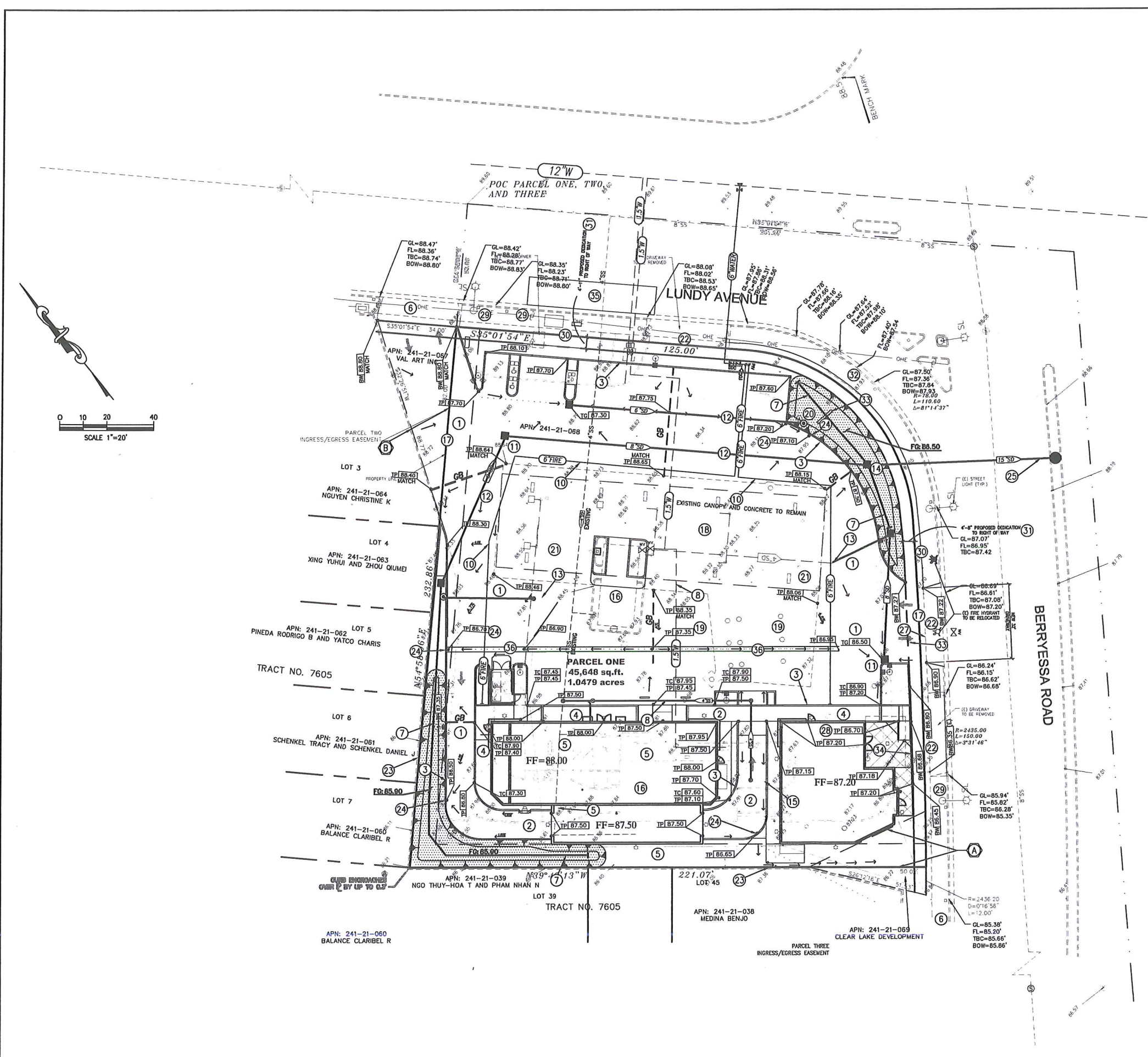
SHEET 09



M I Architects, Inc.
ARCHITECTURE
PLANNING
MANAGEMENT
DESIGN
2221 OLYMPIC BLVD.,
SUITE 100
WALNUT CREEK, CA
94595
925-287-1174 Tel
925-443-1581 Fax
925-878-4875 Cell
mthana@miarchitect.com
www.miarchitect.com

SHELL GAS STATION, CARWASH,
"LOOP" CONVENIENCE STORE & RETAIL BLDG.
1705 BERRYESSA RD.
SAN JOSE, CA 95133
(PDC15-027)

DRAWINGS AND SPECIFICATIONS ARE THE PROPERTY OF M I ARCHITECTS, INC. AND ARE NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, WITHOUT THE WRITTEN PERMISSION OF M I ARCHITECTS, INC. THE USE, REPRODUCTION OR DISSEMINATION OF ANY PART OF THESE DOCUMENTS WITHOUT THE WRITTEN PERMISSION OF M I ARCHITECTS, INC. IS PROHIBITED.

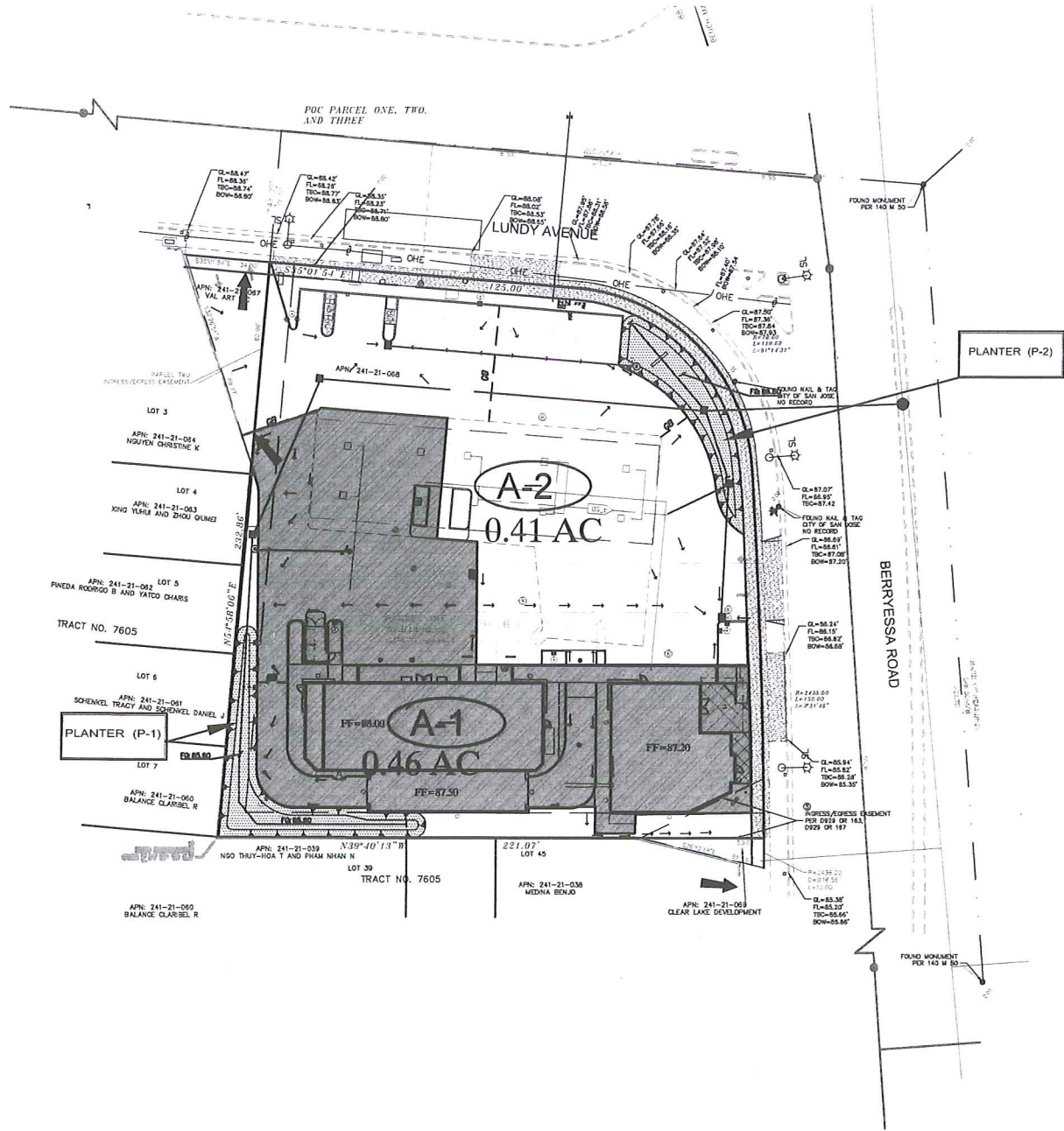


PROPOSED IMPROVEMENT NOTES:

- 1 PLACE 3.5" AC OVER 6" AB COMPACTED TO A MINIMUM 95% COMPACTION.
- 2 PLACE 6" PCC OVER 4" AB COMPACTED TO A MINIMUM 95% COMPACTION.
- 3 CONSTRUCT 6" VERTICAL CURB.
- 4 SIDEWALK, PLACE 4" (MIN) PCC OVER 4" (MIN) AB COMPACTED TO A MINIMUM 95% COMPACTION.
- 5 REMOVE EXISTING SEWER SERVICE AND CLEANOUT.
- 6 EXISTING DRIVEWAY TO REMAIN
- 7 PLACE 6" PERFORATED STORMDRAIN INSIDE BIO-PLANTER.
- 8 PROPOSED SEWER SERVICE
- 9 PROPOSED WATER SERVICE
- 10 PROPOSED FIRE SERVICE
- 11 PROPOSED STORM DRAIN INLET OR JUNCTION BOX.
- 12 PROPOSED STORM DRAIN PIPE MIN S=0.005.
- 13 PLACE NEW SD CLEANOUT AND CONNECT EXISTING STORM DRAIN FROM CANOPY TO NEW STORM DRAIN.
- 14 PLACE NEW CATCH BASIN TOP OF GRATE 2" ABOVE FLOW LINE OF BIO SWALE.
- 15 PLACE 2.0' CURB & GUTTER.
- 16 SAWCUT AND REMOVE EXISTING BUILDING AND CONCRETE SIDEWALK
- 17 SAWCUT AND LIMIT OF WORK LINE.
- 18 PROTECT ALL EXISTING FUEL DISPENSERS AND CONCRETE. REMOVE CONCRETE IF REQUIRED FOR GRADING TO MAINTAIN PROPER DRAINAGE.
- 19 EXISTING TANK TO REMAIN MATCH EXISTING GRADES AROUND TANK SLAB.
- 20 PLACE NEW BUBBLER CATCH BASIN TOP OF GRATE 2" ABOVE FLOW LINE OF BIO SWALE
- 21 SAWCUT AND REMOVE EXISTING CONCRETE SLAB AT THE EDGE OF CANOPY AS REQUIRED FOR CONSTRUCTION OF NEW IMPROVEMENTS.
- 22 SAWCUT AND REMOVE EXISTING DRIVEWAY OR CURB, GUTTER & SIDEWALK AND REPLACE WITH NEW DRIVEWAY PER CITY STANDARD DETAIL.
- 23 EXISTING WALL AND FENCE TO REMAIN AND PROTECTED.
- 24 PLACE 2.0' CURB OPENING EVERY 10 FEET.
- 25 PLACE 15" SD PIPE AND NEW MANHOLE, CONNECT TO EXISTING 36" SD.
- 26 PLACE RETAINING WALL HIGHT MAXIMUM HEIGHT 2.5 FEET.
- 27 PLACE 8" LIP BEVELED WITH MAXIMUM GRADIENT 1:2 BETWEEN WALK AND PAVEMENT TO PREVENT WATER GOING INTO PUBLIC SIDEWALK.
- 28 REMOVE EXISTING CONCRETE GUTTER.
- 29 EXISTING UTILITIES TO REMAIN AND PROTECTED
- 30 SAWCUT AND REMOVE EXISTING DRIVEWAY, SIDEWALK AND REPLACE WITH NEW 10-FOOT WIDE SIDEWALK PER CITY STANDARD DETAIL.
- 31 DEDICATE ADDITIONAL 4.0' RIGHT OF WAY NECESSARY TO PROVIDE 10-FOOT SIDEWALK ALONG LUNDY AVENUE & BERRYESSA ROAD.
- 32 UPGRADE HANDICAP RAMP TO CURRENT CITY & ADA STANDARDS.
- 33 REMOVE EXISTING SIGN OR RELOCATE PER ARCHITECT PLANS.
- 34 PLACE HAND RAIL BEHIND SIDEWALK.
- 35 CONSTRUCT A 12' BY 55' BUS PAD AND INSTALL A BUS STOP BENCH ACCORDING TO VIA SPECIFICATIONS.
- 36 PLACE 2.0' VEE GUTTER MIN S=0.005.

- EASEMENT
- A INGRESS/EGRESS EASEMENT PER D929 OR 163, D929 OR 167
 - B PARCEL TWO INGRESS/EGRESS EASEMENT

A.U. ENERGY		NICK GOYAL 41805 ALBRAE STREET, 2ND FLOOR FREMONT, CA 94538		PDC 15-027	
STUKAM CONSULTING ENGINEERS, INC.		11344 COLOMA ROAD SUITE 235C GOLD RIVER, CALIFORNIA 95670 (916) 858-8241 (916) 988-6316 FAX			
CONCEPTUAL GRADING PLAN 1705 BERRYESSA RD		GAS STATION, CONVENIENCE STORE RETAIL BUILDING & CARWASH APN: 241-21-068		CITY OF SAN JOSE SANTA CLARA COUNTY CALIFORNIA JOB NO: 2015-009 Designated FTS Checked FTS Dated: 11-10-14	
SHEET 4 OF 5 SHEETS		JOB NUMBER: 2015-009			

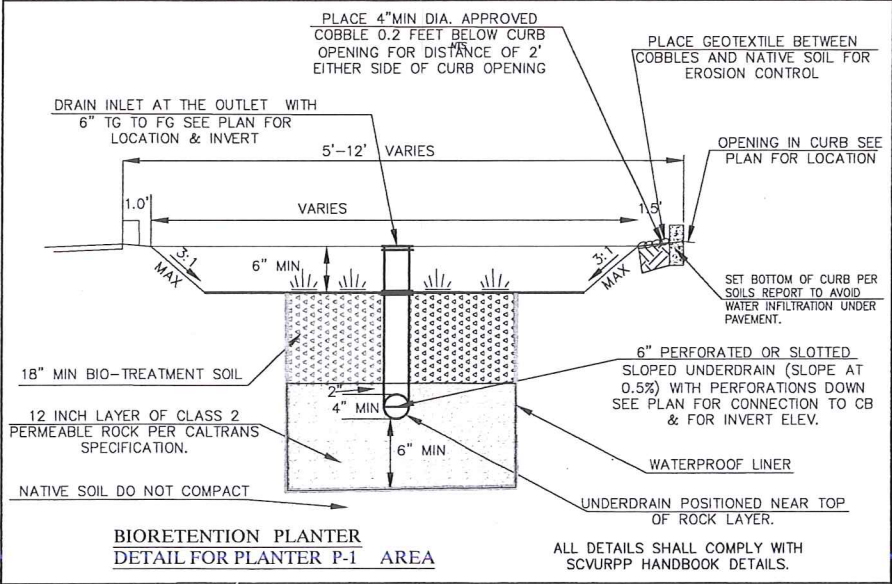
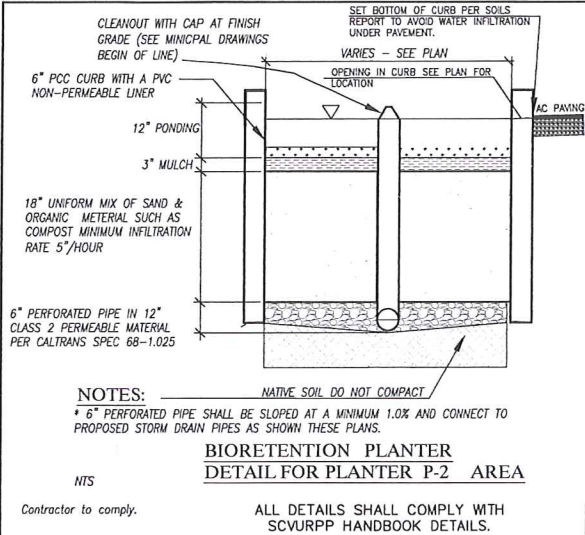


Treatment Control Measure Summary

Area	TCM#	Treatment Type	Drainage Area (SF)	Impervious Area (SF)	Pervious Area (SF)	Bioretention Area Required (SF)	Bioretention Area Provided (SF)	Bioretention Lined or Unlined	Overflow Rise Height (in)
1	A-1	Bio-Treatment	20,197	20,051	146	802	824	Unlined	6
2	A-2	Bio-Treatment	17,816	17,816	0	713	1,257	Unlined	6
Totals:			37,863	37,717	146	1,509	2,081		

*SIZING FOR BIORETENTION AREA REQUIRED CALCULATED USING THE 4% METHOD (IMPERVIOUS AREA X 0.04)
**PER CHAPTER 2.3 OF THE C3 STORMWATER HANDBOOK ROADWAY PROJECTS THAT ADD NEW SIDEWALK ALONG AN EXISTING ROADWAY ARE EXEMPT FROM PROVISION C.3.C OF THE MUNICIPAL STORMWATER PERMIT
***DMA XX IS NOT BEING TREATED BUT WILL BE TREATED BY EQUIVALENT TREATMENT AREA EQ-1. AREA EQ-1 IS EQUAL TO OR GREATER THAN THE REQUIRED TREATMENT AREA OF DMA XX. EQ-1 IS NOT REQUIRED TO BE TREATED AS IT IS (INSERT REASON HERE)

PERVIOUS AND IMPERVIOUS SURFACES COMPARISON TABLE			
a. PROJECT PHASE NUMBER (N/A, 1, 2, 3, ETC.):	1	Total Site (Acres):	1.00
b. TOTAL SITE EXISTING IMPERVIOUS SURFACES (SQUARE FEET):	32,481	Total Site (Acres): Disturbed:	1.00
e. IMPERVIOUS SURFACES			
Existing Condition of Site Area Disturbed (square feet)		PROPOSED CONDITION OF SITE ARE DISTURBED (SQUARE FEET)	
		REPLACED	NEW
Roof Area(s)/Building	7,431	7,431	5,007
Parking			
Sidewalks, Patios, Driveways, etc.	25,050	24,857	
Canopy			
Streets/private			
TOTAL IMPERVIOUS SURFACES:	e.1: 32,481	e.2: 32,481	e.3: 5,007
f. PERVIOUS AREAS			
Landscaped Areas	11,284	6,277	
Pervious Paving			
Other Pervious Surface (green roof, etc.)			
TOTAL PERVIOUS SURFACES:	f.1: 11,284	f.2: 6,277	f.3: 0
g. TOTAL PROPOSED REPLACED + NEW IMPERVIOUS SURFACES (e.2 + e.3)			
			37,488
h. TOTAL PROPOSED REPLACED + NEW PERVIOUS SURFACES (f.2 + f.3)			
			6,277
i. PERCENT OF REPLACEMENT OF IMPERVIOUS AREA IN REDEVELOPMENT PROJECTS (e.2 - C x 100):			
			99%



SIZING OF CURB OPENING FOR 2 YEAR

$Q = AV = 1.486/N \times R^{2/3} \times S^{1/2} \times A$
A = CROSS SECTIONAL AREA (CFS)
V = FLOW VELOCITY (F/S)
R = HYDRAULIC RADIUS
S = SLOPE PER FT
n = MANNING COEFFICIENT
A = 0.28 AC
Q = 0.0448 CFS
S = 0.01
R = $a/p = 1.5 / 4 = 0.375ft$
n = 0.13
 $Q = AV = 1.486/N \times R^{2/3} \times S^{1/2} \times A$
 $Q = 1.486/0.015 \times (0.375)^{2/3} \times (0.1)^{1/2} \times 0.28$
Q = 4.56 CFS

STORMWATER QUALITY FLOW CALCULATIONS FOR HYDRAULIC DESIGN

Q = CIA
C = 0.80 FOR COMMERCIAL/INDUSTRIAL PROPERTY
I = 0.20 INCHES PER HOUR
A = AREA, ACRES

AREA -1 0.46* AC
Q: $0.80 \times 0.20 \times 0.46^* = 0.0736$ CFS

AREA -2 0.41* AC
Q: $0.80 \times 0.20 \times 0.41^* = 0.0656$ CFS

MINIMUM TCMS AND BMPS FOR LAND USE OF CONCERN:

- GAS STATION OR EQUIPMENT FUELING FACILITIES
- ALL NEW FUELING STATIONS OR EXPANSION OF SUCH USES SHOULD INCLUDE THE FOLLOWING BMPS:
- INSTALL AND MAINTAIN A TREATMENT CONTROL MEASURE
 - PAVE THE FUELING AREA FLOORS WITH AN IMPERMEABLE SURFACE (I.E., PORTLAND CEMENT CONCRETE OR EQUIVALENT SMOOTH IMPERVIOUS SURFACE).
 - COVER THE FUELING AREAS WITH A CANOPY OR COVER THAT EXTENDS A MINIMUM OF TEN FEET IN EACH DIRECTION FROM EACH PUMP. ALTERNATIVELY, COVER THE FUELING AREAS WITH A CANOPY OR COVER THAT HAS MINIMUM DIMENSIONS EQUAL TO OR GREATER THAN THE AREA WITH THE GRADE BREAK OR FUEL DISPENSING AREA. (THE FUEL DISPENSING AREA IS DEFINED AS THE AREA EXTENDING A MINIMUM OF 6.5 FEET FROM THE CORNER OF EACH FUEL DISPENSER OR THE LENGTH AT WHICH THE HOSE AND NOZZLE ASSEMBLY MAY BE OPERATED PLUS A MINIMUM OF ONE FOOT, WHICHEVER IS GREATER. IN NO CASE SHOULD THE CANOPY OR COVER DRAIN ONTO THE FUELING AREA.)
 - GRADE THE FUEL AREA TO PREVENT WATER DRAINING TOWARD THE FUELING AREA.
 - GRADE THE FUEL AREA WITH THE MINIMUM SLOPE NECESSARY TO PREVENT PONDING.
 - SEPARATE THE FUELING AREA FROM THE REST OF THE SITE BY A GRADE BREAK THAT PREVENTS RUN-ON OF STORM WATER TO THE MAXIMUM EXTENT PRACTICABLE.
 - DRY SWEEP THE FUELING AREA ROUTINELY.
 - STENCIL ALL ON-SITE STORM DRAINS IN CONFORMANCE WITH THE CITY'S REQUIREMENTS.
 - PREPARE A SPILL CLEANUP PLAN IN CONFORMANCE WITH THE CITY OF SAN JOSE FIRE CODE.

LEGEND

- DRAINAGE AREA BOUNDARY
- A-1 DRAINAGE AREA
- DIRECTION OF FLOW
- ← OVERLAND RELEASE
- RD ROOF DOWNSPOUT

REGISTERED PROFESSIONAL ENGINEER
No. 56122
Expires 12-31-16
CIVIL
STATE OF CALIFORNIA

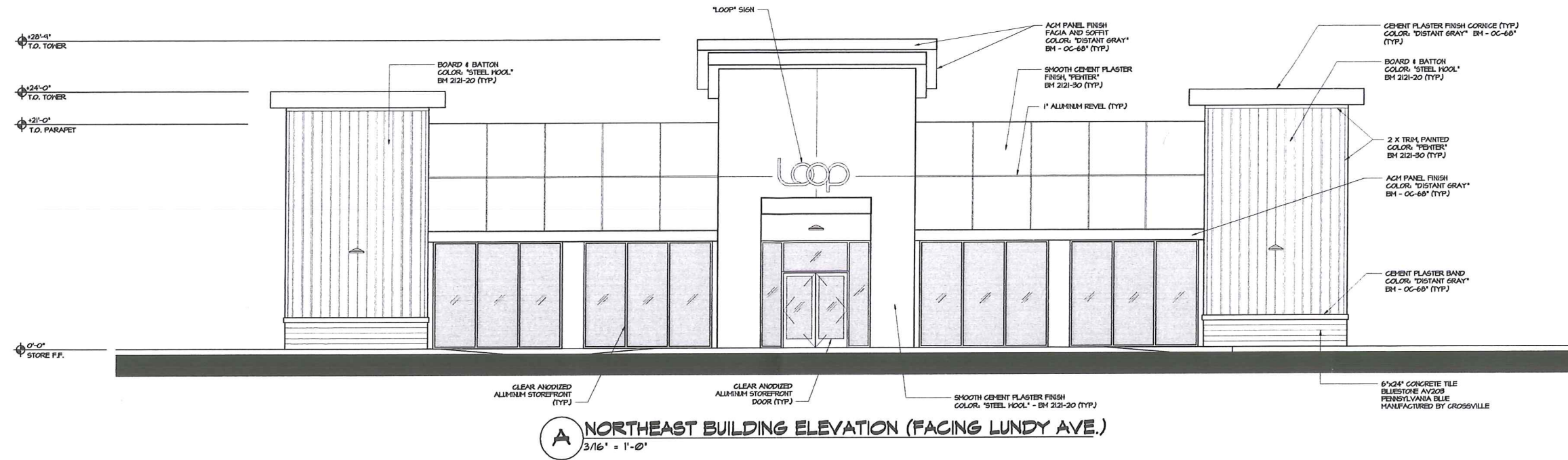
A.U. ENERGY
NICK GOYAL
41805 ALBRAE STREET, 2ND FLOOR
FREMONT, CA 94538
PDC 15-027

STUKAM CONSULTING ENGINEERS, INC.
11344 COLOMA ROAD, SUITE 235C
GOLD RIVER, CALIFORNIA 95670
(916) 858-8241
(916) 988-6316 FAX

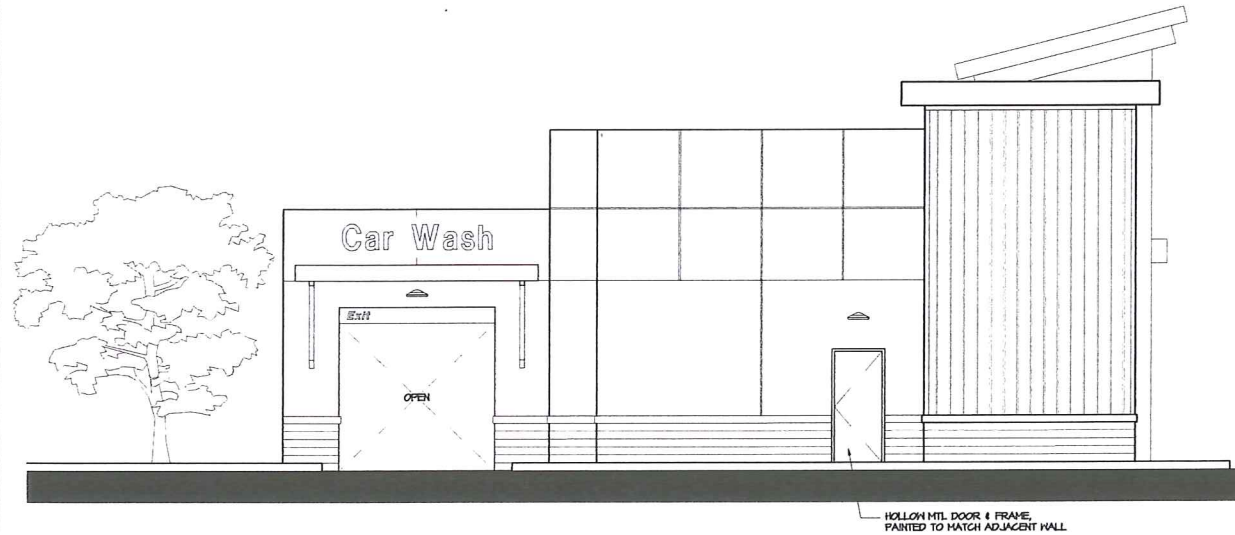
CONCEPTUAL STORMWATER CONTROL PLAN
1705 BERRYESSA RD
GAS STATION CONVENIENCE STORE,
RETAIL BUILDING & CARWASH
APN: 241-21-068
CITY OF SAN JOSE SANTA CLARA COUNTY
JOB NO: 2015-009 [Designed: ETS] [Checked: ETS] [Date: 06-15-15]

REVISIONS
BY CSJ DATE
SHEET
5
Of 5 SHEETS
JOB NUMBER:
2015-009

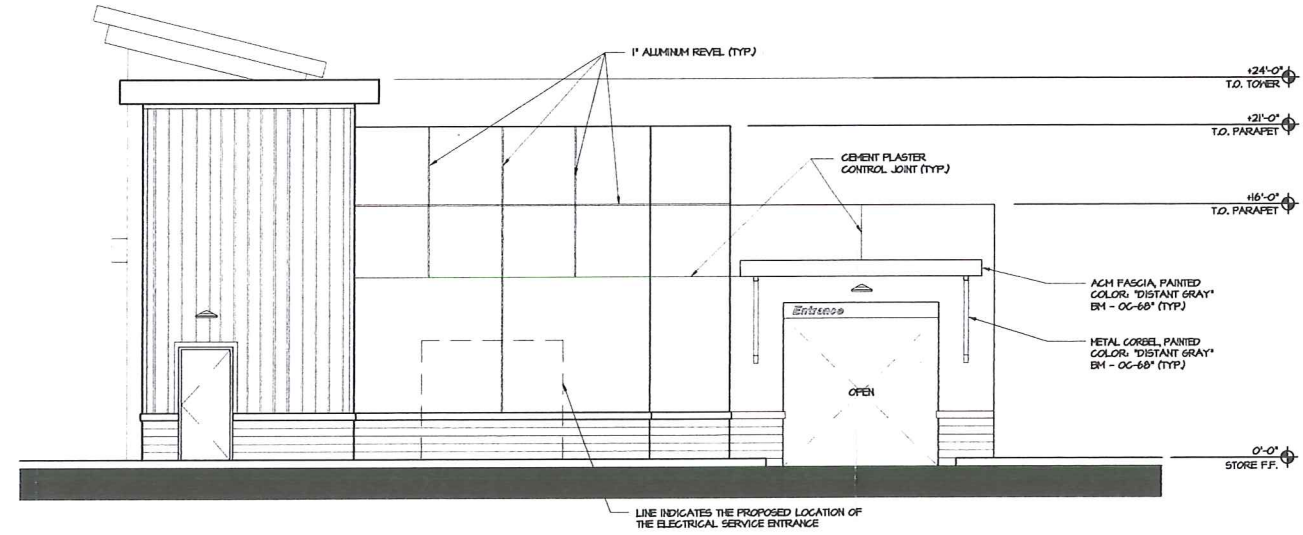
S:\1-Projects\14-5064 1705 Berryessa San Jose\Drawings\Planning\14-5064-02.1 & 02.2-7A & 7B.dwg modified by mthana at Mar 09, 2018 - 9:10am



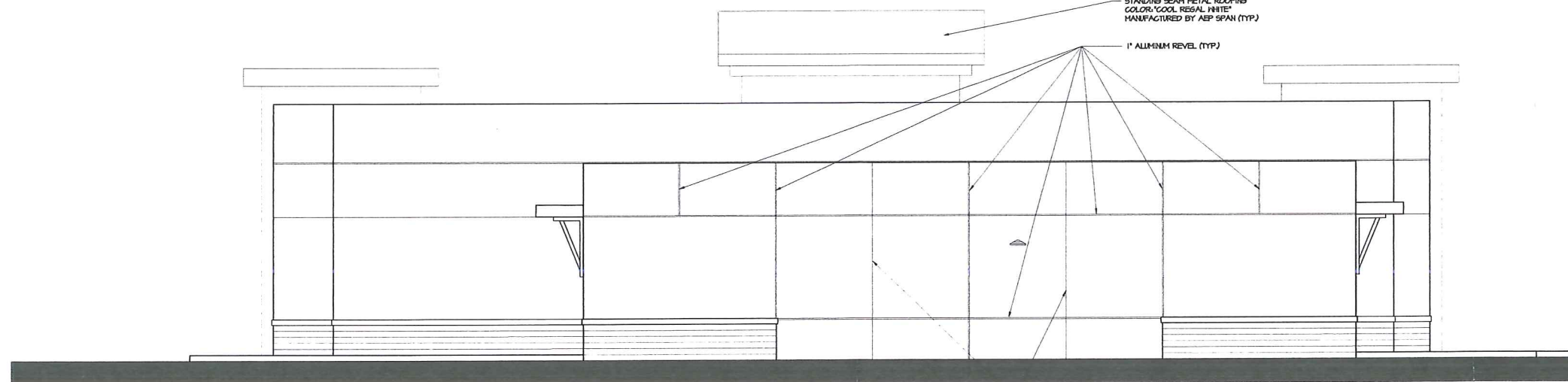
A NORTHEAST BUILDING ELEVATION (FACING LUNDY AVE.)
3/16" = 1'-0"



B SOUTHEAST BUILDING ELEVATION
3/16" = 1'-0"



C NORTHWEST BUILDING ELEVATION
3/16" = 1'-0"



D SOUTHWEST BUILDING ELEVATION
3/16" = 1'-0"

5 0 5 10
GRAPHIC SCALE: 3/16" = 1'-0"



M I Architects, Inc.
ARCHITECTURE
PLANNING
MANAGEMENT
DESIGN
2221 OLYMPIC BLVD.,
SUITE 100
WALNUT CREEK, CA
94595
925-287-1174 Tel
925-443-1581 Fax
925-878-4875 Cell
mthana@miarchitect.com
www.miarchitect.com

SHELL GAS STATION, CARWASH,
"LOOP" CONVENIENCE STORE & RETAIL BLDG
1705 BERRYESSA RD.
SAN JOSE, CA 95133
(PDC15-027)

THESE DRAWINGS AND SPECIFICATIONS AND THE CONCEPTS EMBODIED THEREIN CONSTITUTE THE ORIGINAL UNPUBLISHED WORK OF M I ARCHITECTS, INC. ANY REPRODUCTION OR TRANSMISSION OF THIS DOCUMENT WITHOUT THE WRITTEN CONSENT OF M I ARCHITECTS, INC. IS PROHIBITED.

ISSUED FOR CONSTRUCTION
ISSUED FOR PLAN CHECK
07-27-14 ISSUED FOR PLANNING

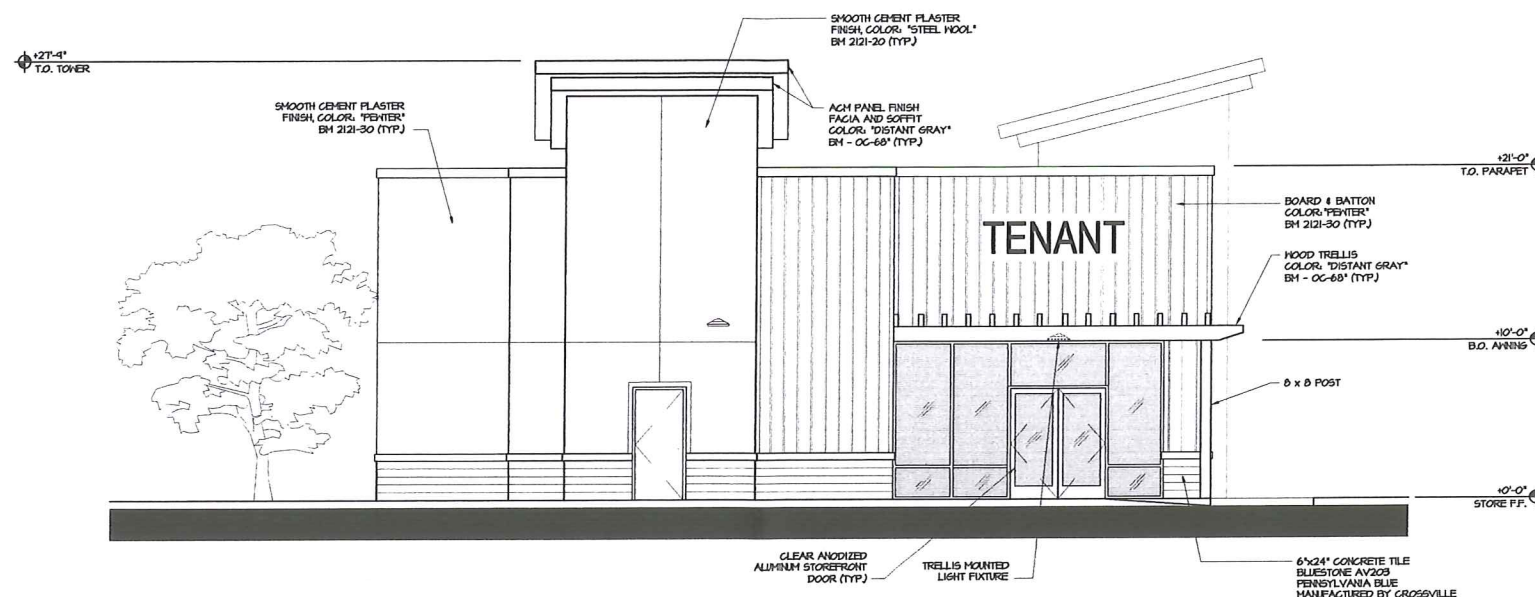
NO.	DATE	DESCRIPTION
1		
2		
3		
4		

CONCEPTUAL CONVENIENCE STORE/
CARWASH BUILDING ELEVATIONS

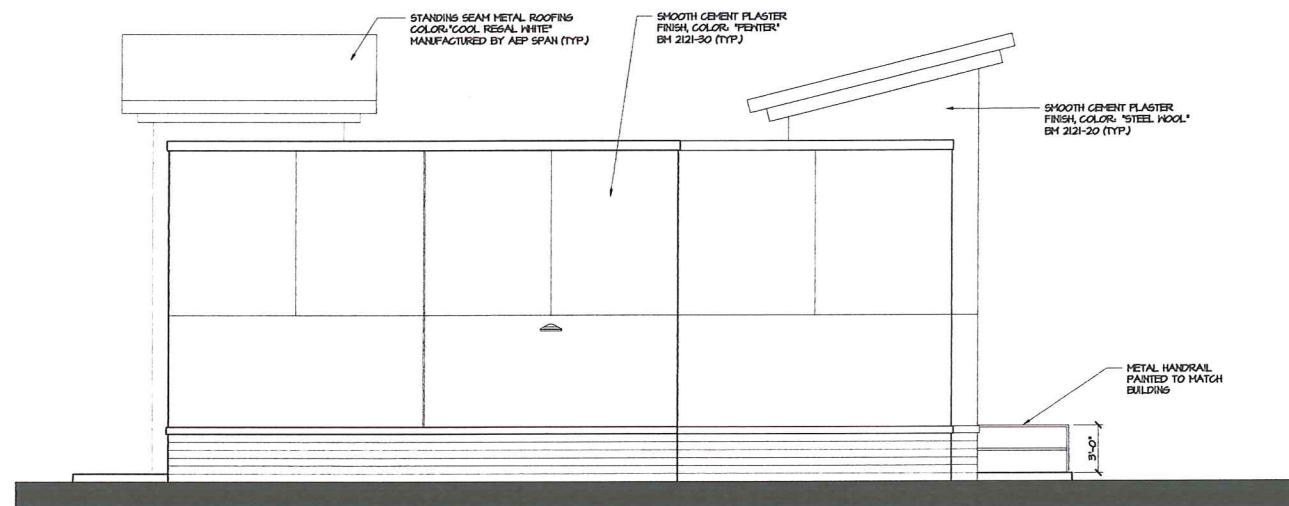
PROJECT #: 14-5064
DRAWN: BB CHECKED: MI
SCALE: AS NOTED DATE: 5-11-15

7A

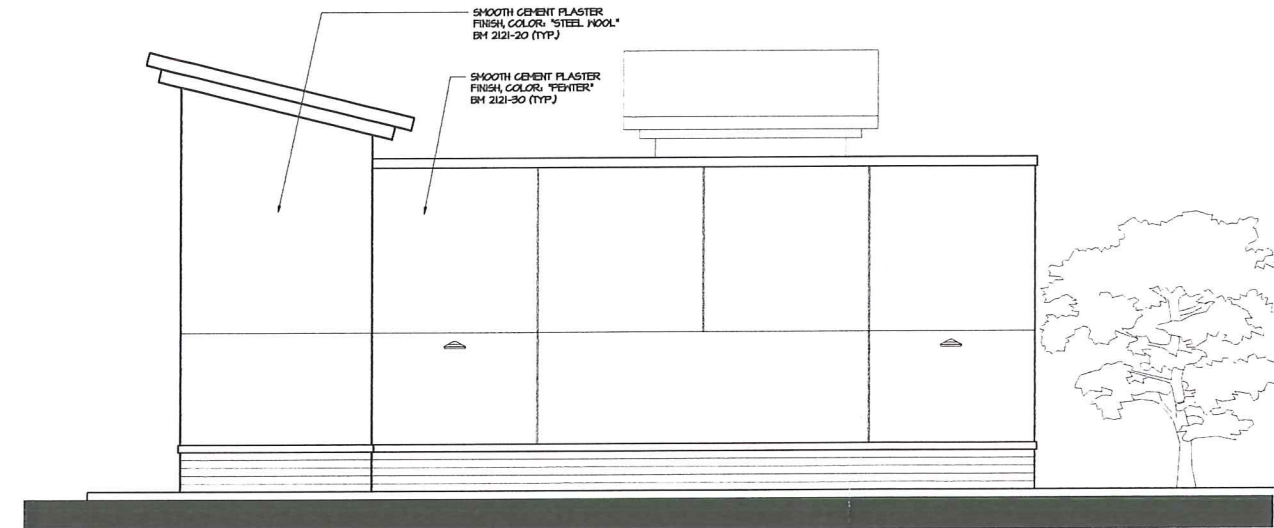
SHEET OF



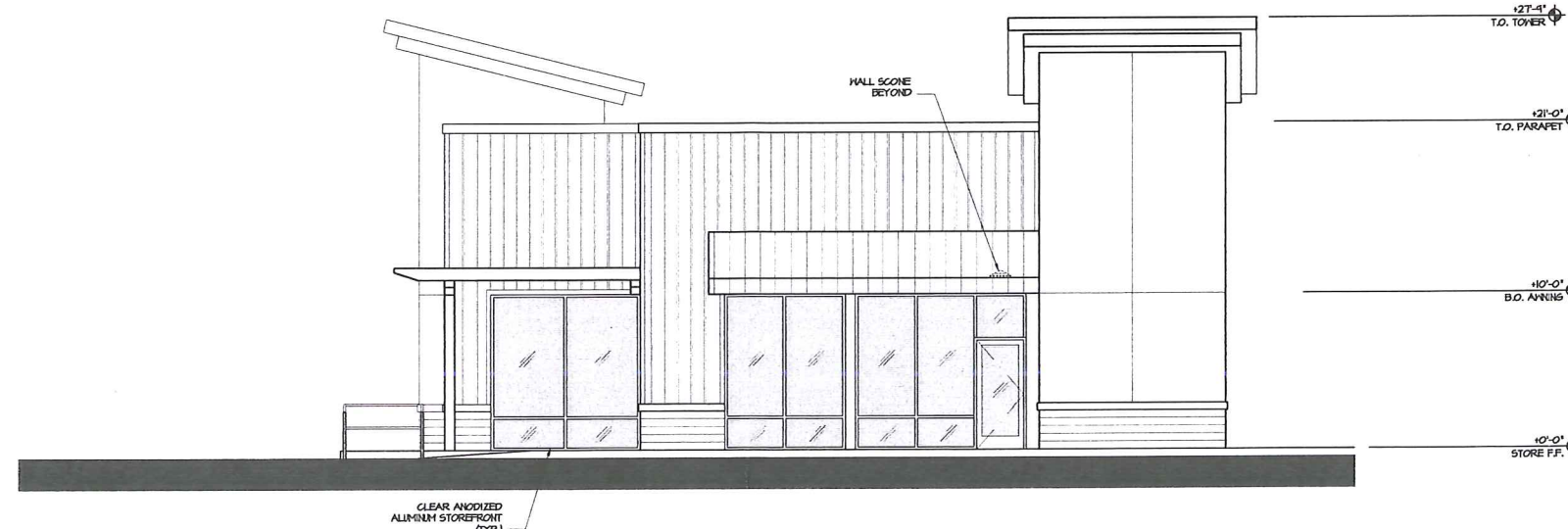
A SOUTHEAST RETAIL BUILDING ELEVATION (FACING BERRYESSA RD)
3/16" = 1'-0"



B SOUTHWEST RETAIL BUILDING ELEVATION
3/16" = 1'-0"



C NORTHWEST RETAIL BUILDING ELEVATION
3/16" = 1'-0"



D NORTHEAST RETAIL BUILDING ELEVATION (FACING LUNDY AVE.)
3/16" = 1'-0"

5 0 5 10 FEET
GRAPHIC SCALE: 3/16" = 1'-0"

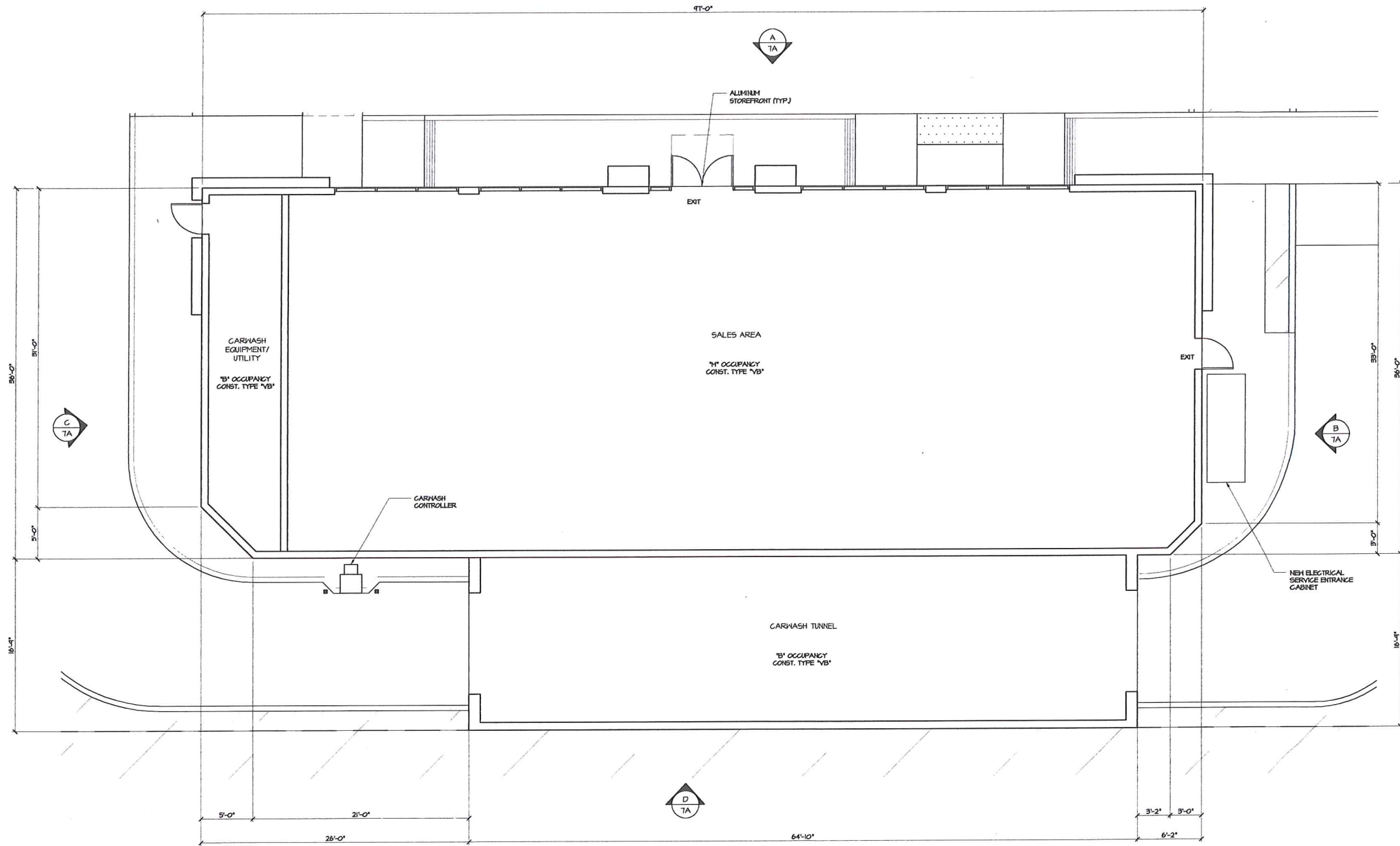
-	ISSUED FOR CONSTRUCTION
-	ISSUED FOR PLAN CHECK
07-27-14	ISSUED FOR PLANNING

NO.	DATE	DESCRIPTION
1		
2		
3		
4		
5		

CONCEPTUAL RETAIL
BUILDING ELEVATIONS

PROJECT #: 14-5064
DRAWN: BB CHECKED: MI
SCALE: AS NOTED DATE: 5-11-15

S:\1-Projects\14-5064 1705 Berryessa San Jose\Drawings\Planning\14-5064-A1.1 & A1.2.dwg modified by mibauer2 at Mar 09, 2018 - 9:12am



1 CONVENIENCE STORE / CARWASH FLOOR PLAN
3/16" = 1'-0"

GRAPHIC SCALE: 3/16" = 1'-0"



M I Architects, Inc.
ARCHITECTURE
PLANNING
MANAGEMENT
DESIGN
2221 OLYMPIC BLVD.,
SUITE 100
WALNUT CREEK, CA
94595
925-287-1174 Tel
925-443-1581 Fax
925-878-4875 Cell
mthanam@miarchitect.com
www.miarchitect.com

SHELL GAS STATION, CARWASH,
"LOOP" CONVENIENCE STORE & RETAIL BLDG
1705 BERRYESSA RD.
SAN JOSE, CA 95138
(PDC15-027)

THESE DRAWINGS AND THE CONCEPTS EMBODIED WITHIN THEM CONSTITUTE THE ORIGINAL UNPUBLISHED WORK OF M I ARCHITECTS, INC. ANY REUSE, REPRODUCTION, OR DISSEMINATION OF THESE DRAWINGS WITHOUT THE WRITTEN CONSENT OF M I ARCHITECTS, INC. IS PROHIBITED.

ISSUED FOR CONSTRUCTION
ISSUED FOR PLAN CHECK
07-21-14 ISSUED FOR PLANNING

NO.	DATE	DESCRIPTION
1		
2		
3		
4		
5		

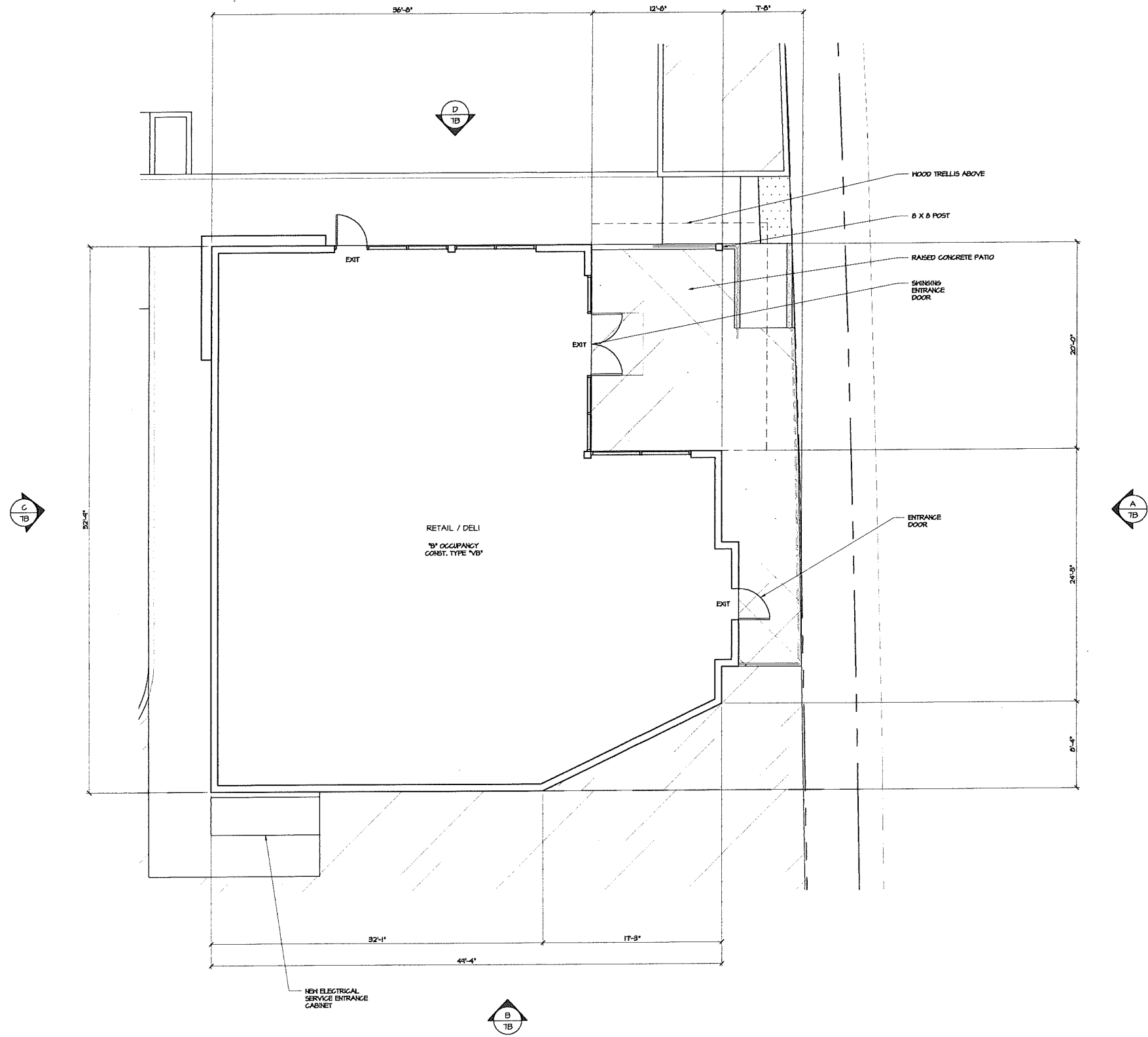
CONCEPTUAL CONVENIENCE STORE/
CARWASH FLOOR PLAN

PROJECT #: 14-5064
DRAWN: BB CHECKED: MI
SCALE: AS NOTED DATE: 05-11-15

9A

SHEET OF

S:\-Projects\14-5084 1705 Berryessa San Jose\Drawings\Planning\14-5084-A1.1x A1.2.0x & 02.dwg modified by mlsuser2 at Mar 09, 2018 - 9:12am



1 RETAIL/DELI FLOOR PLAN
3/16" = 1'-0"

GRAPHIC SCALE: 3/16" = 1'-0"



Architects

M I Architects, Inc.
ARCHITECTURE
PLANNING
MANAGEMENT
DESIGN
2221 OLYMPIC BLVD.,
SUITE 100
WALNUT CREEK, CA
94595
925-287-1174 Tel
925-443-1581 Fax
925-878-9875 Cell
mihandmarchitect.com
www.marchitect.com

SHELL GAS STATION, CARWASH,
"LOOP" CONVENIENCE STORE & RETAIL BLDG
1705 BERRYESSA RD.
SAN JOSE, CA 95133

(PDC15-027)

THESE DRAWINGS AND THE CONCEPTS EMBODIED THEREIN ARE THE PROPERTY OF M I ARCHITECTS, INC. AND ARE NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, WITHOUT THE WRITTEN CONSENT OF M I ARCHITECTS, INC. THE USE, REPRODUCTION OR DISSEMINATION OF THESE DRAWINGS WITHOUT THE WRITTEN CONSENT OF M I ARCHITECTS, INC. IS PROHIBITED.

ISSUED FOR CONSTRUCTION
ISSUED FOR PLAN CHECK
01-21-14 ISSUED FOR PLANNING

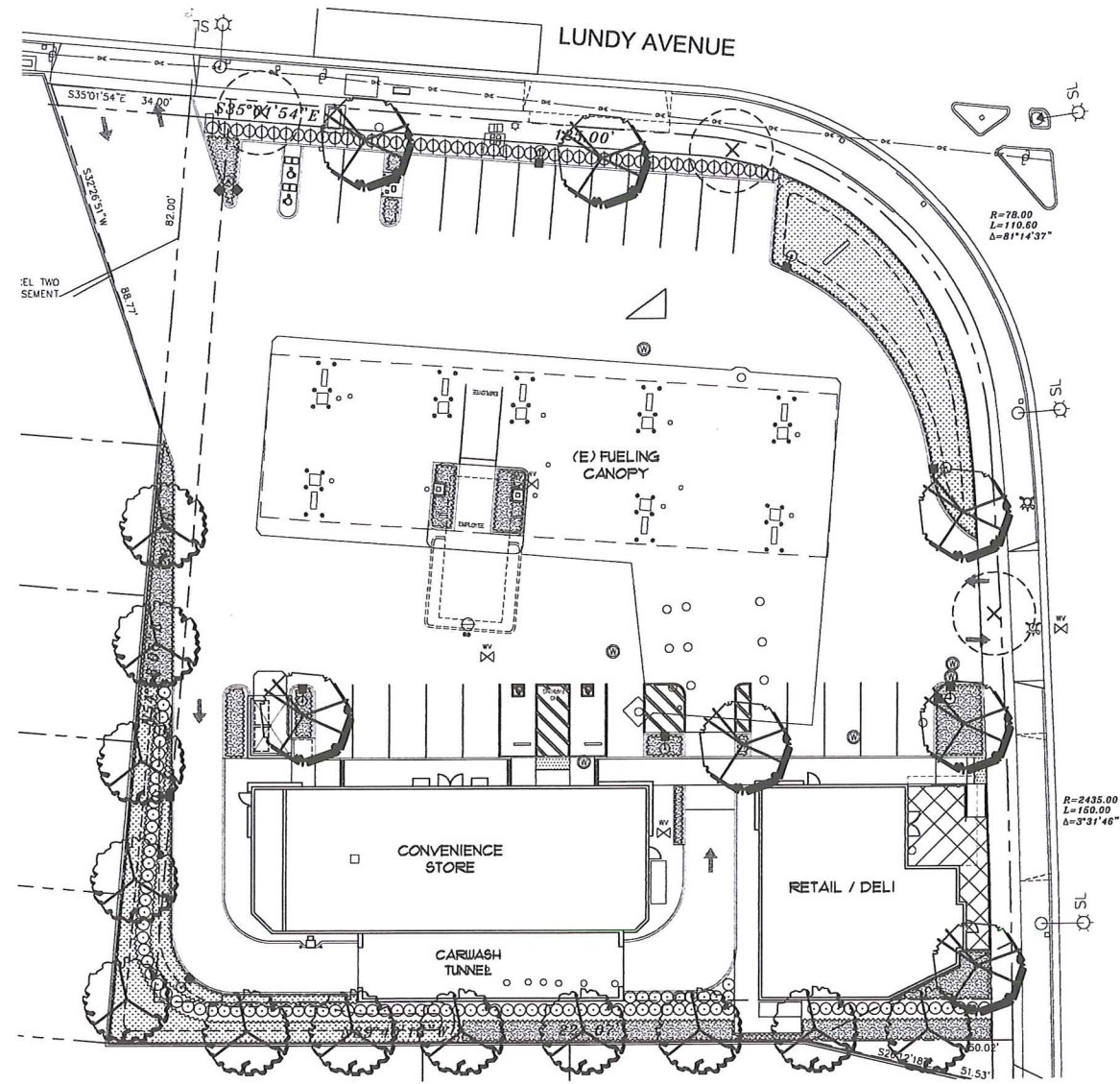
NO.	DATE	DESCRIPTION
1		
2		
3		
4		

CONCEPTUAL RETAIL / DELI
BUILDING FLOOR PLAN

PROJECT #: 14-5084
DRAWN: BB CHECKED: MI
SCALE: AS NOTED DATE: 03-11-15

9B

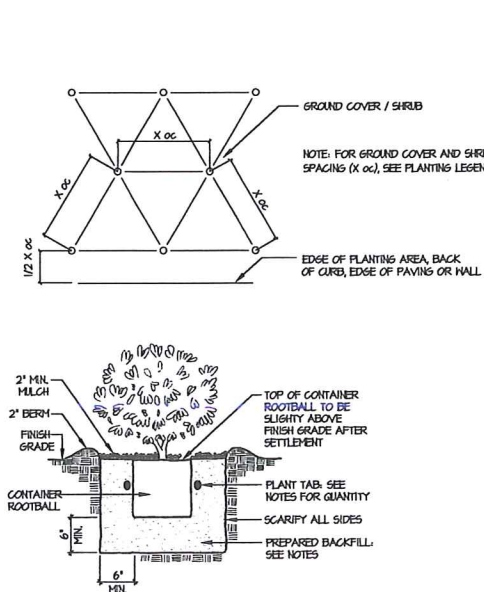
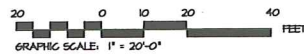
SHEET OF



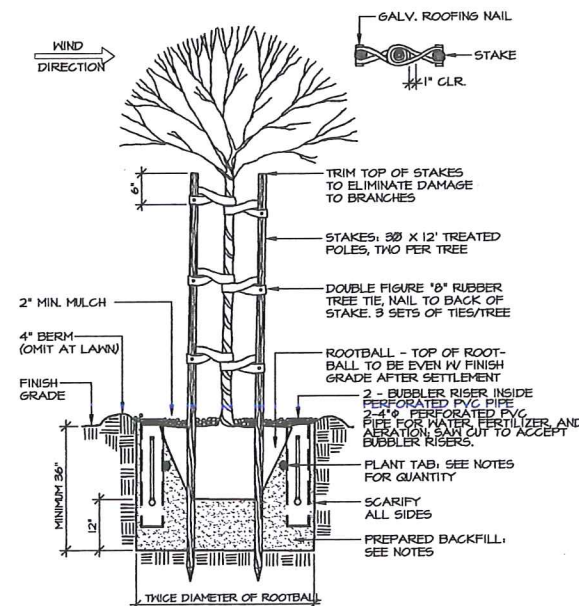
Maximum Applied Water Allowance			
Plant	Water Use (Eto)	ETP	ETPA
Low	28.086	2,458	69,041
Medium	28.086	0	0
High	28.086	0	0
Total			69,041

Proposed Landscape Water Use			
Plant Type	H2O Use	ETP	Gallons
Low	0.35	5,684	100.0%
Medium	0.60	0	0.0%
High	0.90	0	0.0%
Totals		5,684	100%

1 LANDSCAPE PLAN
1" = 20'-0"



PLANING INSTALLATION AND LAYOUT



TREE PLANTING AND STAKING

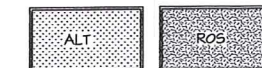


Ciardella
associates
Landscape Architecture
Urban Design
640 Menlo Ave, Suite 10
Menlo Park, CA 94025
Tel 650 326 6100
F 650 323 6706
ca@ciardella-assoc.com

PLANING LEGEND

Symbol	BOTANICAL NAME	COMMON NAME	H2O
	<i>Gleditsia triacanthos</i>	'Shademaster'	Honey Locust L 24" Box
	<i>Platanus acerifolia</i>	'Yarwood'	Yarwood Sycamore L 24" Box
	<i>Myrica californica</i>	Pacific Wax Myrtle	L 5 Gallon
	<i>Bouteloua gracilis</i>	Blonde Ambition	BA Grama Grass L 5 Gallon
	<i>Juncus pallidus</i>	Glant Rush	L 1 Gallon @ 36" oc
	<i>Juncus patens</i>	California Gray Rush	L 1 Gallon @ 36" oc
	<i>Helleborichon sempervirens</i>	Blue Oat Grass	L 1 Gallon @ 36" oc
	<i>ROS Rosmarinus officinalis</i>	Hunting Carpet	Rosemary L 1 Gallon @ 30" oc

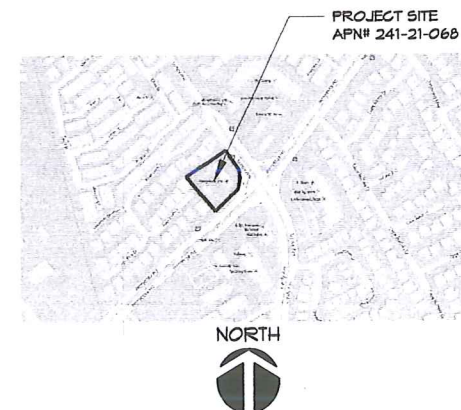
ALT* = Alternate the three plants in a triangular pattern



PLANTING NOTES

- All trees are to be staked as shown in the staking diagram per city requirement.
- Plant locations are to be adjusted as necessary to screen utilities but not block windows or impede access.
- All ground cover and shrub areas shall be top-dressed with a 3" layer of bark mulch.
- All ground cover planting will be placed no farther than 6" from edge of pavement, edge of header or back of curb. Spacing shall ensure full coverage in one year.
- There shall be no storing of material or equipment, permitting of any burning or operating or parking of equipment under branches of any existing plants to remain. If existing plants to remain are damaged during construction, the plants shall be replaced with the same species an size as those damaged.
- All plant material shall be nursery grown stock. All plant materials shall be tagged at the nursery at least 1 month prior to planting for the Landscape Architects review.
- Review layout of all landscape elements with the Landscape Architect prior to installation. Field modifications may be necessary. Final layout to be reviewed by the Landscape Architect.
- Written dimensions supersede scaled dimension. Measurements are from the wall face, back of curb, edge of walk, building wall, property line or center line as graphically indicated.
- All layout corners are at 90 degrees right angles unless otherwise indicated. All curves shown are segments of circles with noted radii or diameter if noted. Circles can be scaled and be connected by freeform curves.
- HERBICIDE APPLICATION: Herbicide shall not be used until all plant material has been planted a minimum of 20-days. All planting areas shall be kept weed-free by non-herbicide methods during this time period. Herbicide shall not be applied to any areas which are or have been seeded. Contractor must be licensed by the State and County for fertilizer application, and must have current registration on file with the County.
- Landscape shall be maintained in a manner to prevent landscaping from growing above 3' in height in the areas indicated in the plans as being located within a safety visibility triangle area.
- CERTIFICATION: Prior to occupancy, the Landscape Architect shall certify in writing in a manner acceptable to the Building Inspection Division, that the landscaping has been installed in accordance with all aspects of the approved landscape plans.
- A minimum of 8" of non-mechanically compacted soil shall be available for water absorption and root growth in planted areas.
- Incorporate compost or natural fertilizer into the soil to a minimum depth of 8" at a minimum rate of 6 cubic yards per 1000 square feet.
- All plantings shall be automatically irrigated utilizing state of the art system, components and installation techniques.
- All planting and irrigation shall comply with the city of Emeryville Water Efficient Landscape Requirements for larger landscapes.
- All details shall comply with city standard details.
- No existing trees are of ordinance size.

VICINITY MAP



M I Architects, Inc.
ARCHITECTURE
PLANNING
MANAGEMENT
DESIGN
2221 OLYMPIC BLVD.
SUITE 100
WALNUT CREEK, CA
94595
425-287-1174 Tel
425-443-1581 Fax
425-878-4875 Cell
mthana@miarchitect.com
www.miarchitect.com

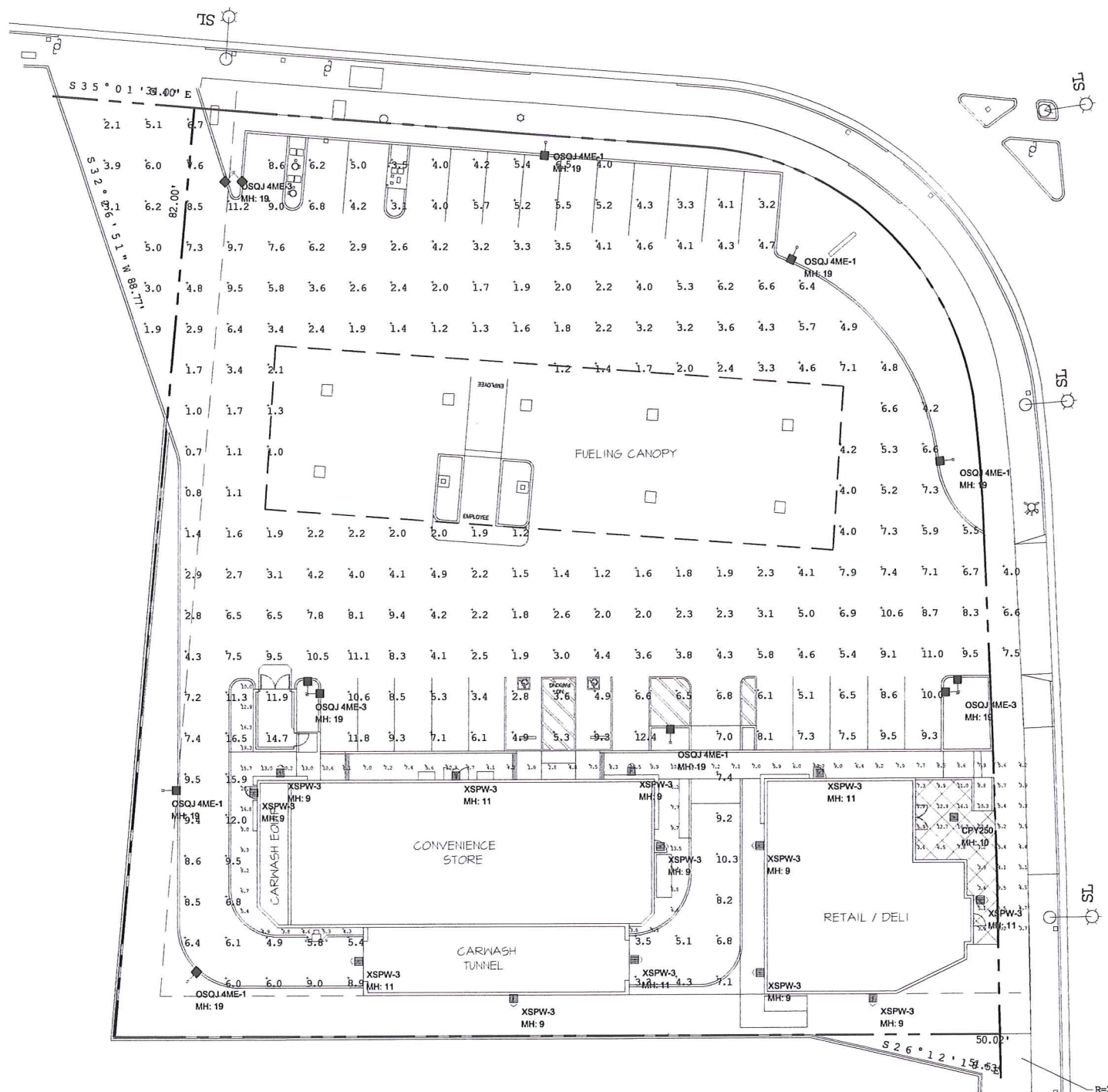
SHELL GAS STATION, CARWASH,
"LOOP" CONVENIENCE STORE & RETAIL BLDG
1705 BERRYESSA RD.
SAN JOSE, CA 95138
(PDC15-027)

ISSUED FOR CONSTRUCTION	
ISSUED FOR PLAN CHECK	
07-21-14 ISSUED FOR PLANNING	
NO. DATE DESCRIPTION	
1	
2	
3	
4	
5	
CONCEPTUAL LANDSCAPE PLAN	
PROJECT # 14-5064	
DRAWN: RC CHECKED: RC	
SCALE: AS NOTED DATE: 05-14-15	

Luminaire Schedule					LLF	Total Watts	Description
Symbol	Qty	Label	Arrangement	Lumens/Lamp			
1	1	CPY250	SINGLE	4400	0.910	43	CPY250 A-DW-D-CUL-MH
6	6	OSQJ 4ME-1	SINGLE	17117	0.920	1008	OSQJ 4ME-1 J-STK-UL-XX w/OSQJ DA-XX
3	3	OSQJ 4ME-3	2 @ 90 DEGREES	17117	0.920	1008	OSQJ 4ME-3 J-STK-UL-XX w/OSQJ DA-XX
13	13	XSPW-3	SINGLE	4109	0.890	545	XSPW-3 A-DW-D-CUL-MH

Pole Schedule:
(1) PS4517C1XX (17' X 4" X 125" STEEL SQUARE POLE)
(3) PS4517C3XX (17' X 4" X 125" STEEL SQUARE POLE)
Proposed poles meet 140 MPH eastward winds.

Footcandle calculations using predicted lumen values after 50K hours of operation					
Label	Avg	Max	Min	Avg/Min	Max/Min
PAVEMENT	5.22	16.5	0.7	7.45	23.57
SEAWALK	8.00	20.6	1.8	4.44	11.44



1 CONCEPTUAL SITE PHOTOMETRIC
1" = 16'-0"



R=2436.20
D = 0' 16' 56"
L=12.00'

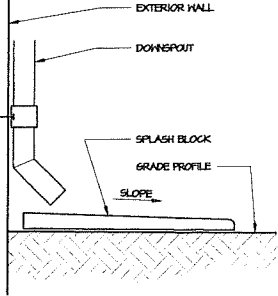
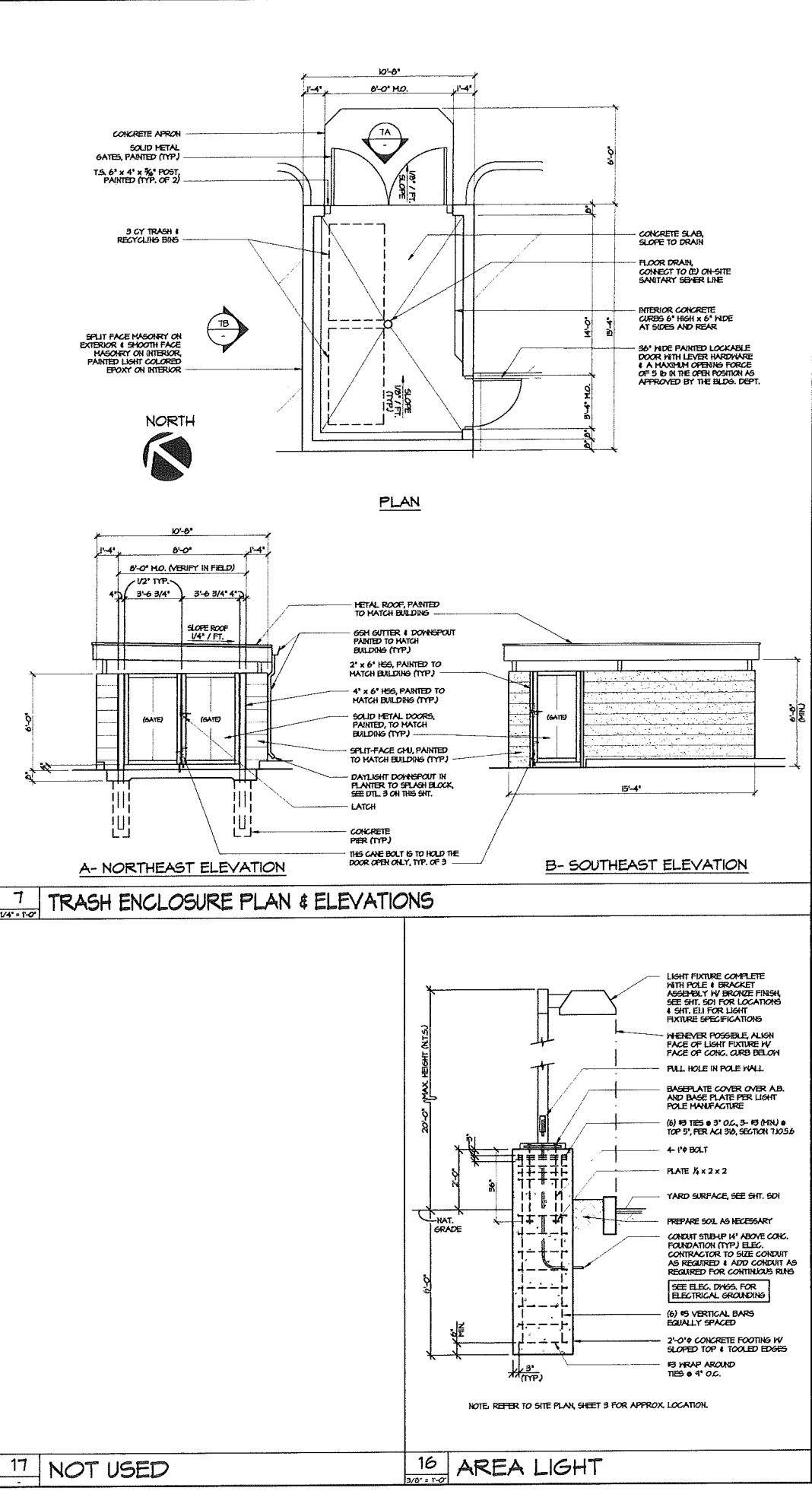
(PDC15-027)
CONCEPTUAL SITE PHOTOMETRIC



11

Project Name: 1705 BERRYESSA RD, SAN JOSE, CA
Date: 6/22/2015
Scale: 1"=16'
Footcandle calculations at grade
Filename: V:\Common\Eng\BETA\PETITM150616S\JAC\BR1 AGI
Layout by: CHRIS BOSANEC
SR No: 5399
1200 92nd Street - Shoreline, WA 98147
www.cree.com - (800) 235-6803
Illumination results shown on this lighting design are based on project parameters provided to Cree, Inc. used in conjunction with luminaire test procedures conducted under laboratory conditions. Actual project conditions differing from these design parameters may affect field results. The customer is responsible for verifying dimensional accuracy along with compliance with any applicable electrical, lighting, or energy code.

S:_Projects\14-5064-1703 Business San Jose\Drawings\14-5064-502_12.dwg modified by mltan2 at: Mar 09, 2018 - 01:30m

			
5	NOT USED	4	NOT USED
10	NOT USED	9	NOT USED
15	NOT USED	14	NOT USED
20	NOT USED	19	NOT USED
		3 1/2" = 1'-0"	DOWNSPOUT TO SPLASH BLOCK
		8	NOT USED
		13	NOT USED
		18	NOT USED
		17 3/8" = 1'-0"	NOT USED
		16 3/8" = 1'-0"	AREA LIGHT
			



M I Architects, Inc.
ARCHITECTURE
PLANNING
MANAGEMENT
DESIGN
2221 OLYMPIC BLVD.,
SUITE 100
WALNUT CREEK, CA
94595
925-287-1174 Tel
925-443-1581 Fax
925-878-4875 Cell
mthanad@miarchitect.com
www.miarchitect.com

SHELL GAS STATION, CARNASH,
"LOOP" CONVENIENCE STORE & RETAIL BLDG.
1705 BERRYESSA RD.
SAN JOSE, CA 95133
(PDC15-027)

THESE DRAWINGS AND SPECIFICATIONS ARE THE PROPERTY OF M I ARCHITECTS, INC. AND ARE NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF M I ARCHITECTS, INC. THE USE, REPRODUCTION OR DISSEMINATION OF THESE DRAWINGS WITHOUT THE WRITTEN PERMISSION OF M I ARCHITECTS, INC. IS PROHIBITED.

ISSUED FOR CONSTRUCTION
ISSUED FOR PLAN CHECK
07-21-14 ISSUED FOR PLANNING

NO.	DATE	DESCRIPTION
1		
2		
3		
4		

SITE DETAILS

PROJECT #: 14-5064
DRAWN: BB CHECKED: MI
SCALE: AS NOTED DATE: 5-11-15

The following
items were
received after
packets were
distributed.

General Development Standards*
File No. PDC15-027
CP(PD) Planned Development Zoning District

**In any cases where the graphic plans and text may differ, this text takes precedence*

ALLOWED USES

- Permitted, Conditional, and Special Uses of the CN – Commercial Neighborhood Zoning District of Title 20 of the San José Municipal Code, as may be amended in the future. Conditional and Special Uses as identified in the CN – Commercial Neighborhood Zoning District shall be subject to approval of a Planned Development Permit or Amendment by the City of San José Planning Director.

DEVELOPMENT STANDARDS

SETBACKS

Buildings shall meet the following minimum setback requirements:

Setback Line	Setback Distance in feet
Building to the front (Lundy Ave)	Zero minimum*
Building to the front (Berryessa Rd)	Zero minimum*
Building to rear & side	10

** At least one of the buildings must have frontage no further than 10 feet from the public right-of-way from either Lundy Avenue or Berryessa Road.*

HEIGHT

- Height is per code as amended per Title 20 of the San José Municipal Code, Section 20.40.200, as may be amended.

PARKING

- Parking spaces shall be provided per Title 20 of the San José Municipal Code, Section 20.90, as may be amended.
- Bicycle Parking shall be provided per Title 20 of the San José Municipal Code, Section 20.90, as may be amended.

PERFORMANCE STANDARDS

- Performance standards shall be per Section 20.40.600 of the San José Municipal Code, as may be amended.

CARWASH CONDITIONS

- Drive-through uses adjacent to residential uses or zoned parcels, shall include a sound attenuation wall that conforms to the above noise performance standard and General Plan Policies for noise.

- Development of a car wash facility on-site shall be consistent with the site configuration and sound attenuation walls that are depicted in Figure 3.1-4 of the IS/MND entitled "1705 Berryessa Road Gas Station Project, File Number: PDC15-027," dated February 2017, to the satisfaction of the Director of Planning.
- Development of a car wash facility on-site shall include the use of a Ryko 3-Fan SlimLine dryer system with incorporated Ryko Quiet-Kit silencer consistent with the Noise Report entitled "1705 Berryessa Avenue Convenience Store, Gas Station and Car Wash Environmental Noise Assessment Extant Report No. 160701.01" dated September 9, 2016, prepared for: A.U. Energy LLC 41805 Albrae Street Fremont, CA 94538, prepared by: Michael Carr, INCE, CTS Principal Consultant.
Alternatively, to the satisfaction of the Director of Planning, an equivalent product may be used that achieves at least the same level of noise minimization as the noise equipment and measures described in the above-mentioned noise report.

ENVIRONMENTAL MITIGATION

IMPLEMENT THE MITIGATION MEASURES IDENTIFIED IN THE INITIAL STUDY (IS) AND MITIGATED NEGATIVE DECLARATION (MND) FOR THE 1705 BERRYESSA ROAD DEVELOPMENT PROJECT (FILE NO. PDC15-027) AND MITIGATION MONITORING AND REPORTING PROGRAM, AS MAY BE AMENDED.

THE FOLLOWING SPECIFIC MITIGATION MEASURES, OR EQUIVALENT MITIGATION MEASURES TO THE SATISFACTION OF THE DIRECTOR OF PLANNING, SHALL BE INCLUDED IN DEVELOPMENT AND USES ON-SITE TO REDUCE POTENTIALLY SIGNIFICANT EFFECTS TO A LESS THAN SIGNIFICANT LEVEL:

1. AIR QUALITY. Impact AIR-1: The use of various diesel-powered vehicles and equipment during construction could expose adjacent sensitive receptors to substantial toxic air contaminants.
 - MM AIR-1.1: Diesel-fueled construction equipment used onsite for more than two days shall meet EPA Tier 2 emission standards. The project applicant shall submit, for review and approval, a construction operations plan that includes a list of equipment to be used more than two days during construction, specifications of the equipment to be used during construction and a letter signed by a qualified air quality specialist, verifying that the equipment included in the plan meets the standards set forth in this mitigation measure to the Director of the Department of Planning, Building, and Code Enforcement prior to issuance of a grading or demolition permit. The plan shall demonstrate compliance with the Tier 2 emissions standards.

2. BIOLOGICAL RESOURCES. Impact BIO-1: Demolition and construction activities, including the removal of trees from the project site, could impact nesting migratory birds.

- MM BIO-1.1: If feasible, construction shall be scheduled between September 1 and January 31 (inclusive) to avoid the nesting season. If this is not feasible, pre-construction surveys for nesting raptors and other migratory breeding birds (including yellow warblers) shall be conducted by a qualified ornithologist to identify active nests that may be disturbed during project implementation on-site and within 250 feet of the site. Between February 1 and April 30 (inclusive) preconstruction surveys shall be conducted no more than 14 days prior to initiation of construction activities (including any ground-disturbing activities) or tree removal. Between May 1 and August 31 (inclusive), pre-construction surveys shall be conducted no more than 30 days prior to initiation of these activities. The surveying ornithologist shall inspect all trees in and immediately adjacent (within 250 feet) to the construction area for nests. If an active nest is found in or close enough to the construction area to be disturbed by these activities, the ornithologist shall, in consultation with the California Department of Fish and Wildlife, designate a construction-free buffer zone (typically 250 feet for raptors and 100 feet for other birds) around the nest, which shall be maintained until after the breeding season has ended and/or a qualified ornithologist has determined that the young birds have fledged. The project applicant shall submit a report indicating the results of the survey and any designated buffer zones to the satisfaction of the Director of Planning, Building, and Code Enforcement prior to issuance of any grading permits.

3. HAZARDS AND HAZARDOUS MATERIALS. Impact HAZ-1: Hazardous materials contamination on the site, if discovered in soil or groundwater, could pose a risk to construction workers and others on or around the project site.

- MM HAZ-1.1: The following mitigation measures will be implemented prior to the start of ground-disturbing activities to reduce the potential for construction workers or others to encounter hazardous materials contamination. Prior to the issuance of a demolition or grading permit, a site management plan (SMP) shall be prepared and submitted to the Santa Clara County Department of Environmental Health Hazardous Materials Compliance Division to address the impact of disturbance of any residual soil or groundwater contamination. Appropriate soil testing, characterization, storage, transportation, and disposal procedures shall be specified in the SMP. Contaminated soils shall be disposed of at a licensed facility in accordance with appropriate local, state, and federal regulations. The SMP shall also establish management practices for handling contaminated soil or other materials if encountered during demolition and construction activities. The SMP shall be reviewed and approved by the County of Santa Clara Department of Environmental Health and submitted to the City of San José Department of Planning, Building, and Code Enforcement prior to issuance of grading or building permits.

General Development Standards* (Revised March 21, 2017)

File No. PDC15-027

CP(PD) Planned Development Zoning District

**In any cases where the graphic plans and text may differ, this text takes precedence*

ALLOWED USES

- Permitted, Conditional, and Special Uses of the CN – Commercial Neighborhood Zoning District of Title 20 of the San José Municipal Code, as may be amended in the future. Conditional and Special Uses as identified in the CN – Commercial Neighborhood Zoning District shall be subject to approval of a Planned Development Permit or Amendment by the City of San José Planning Director.

DEVELOPMENT STANDARDS

SETBACKS

Buildings shall meet the following minimum setback requirements:

Setback Line	Setback Distance in feet
Building to the front (Lundy Ave)	Zero minimum*
Building to the front (Berryessa Rd)	Zero minimum*
Building to rear & side	10

** At least one of the buildings must have frontage no further than 10 feet from the public right-of-way from either Lundy Avenue or Berryessa Road.*

HEIGHT

- Height is per code as amended per Title 20 of the San José Municipal Code, Section 20.40.200, as may be amended.

PARKING

- Parking spaces shall be provided per Title 20 of the San José Municipal Code, Section 20.90, as may be amended.
- Bicycle Parking shall be provided per Title 20 of the San José Municipal Code, Section 20.90, as may be amended.

PERFORMANCE STANDARDS

- Performance standards shall be per Section 20.40.600 of the San José Municipal Code, as may be amended.

CARWASH CONDITIONS

- Drive-through uses adjacent to residential uses or zoned parcels, shall include a sound attenuation wall that conforms to the above noise performance standard and General Plan Policies for noise.

- Development of a car wash facility on-site shall be consistent with the site configuration and sound attenuation walls that are depicted in Figure 3.1-4 of the IS/MND entitled "1705 Berryessa Road Gas Station Project, File Number: PDC15-027," dated February 2017, to the satisfaction of the Director of Planning.
- Development of a car wash facility on-site shall include the use of a Ryko 3-Fan SlimLine dryer system with incorporated Ryko Quiet-Kit silencer consistent with the Noise Report entitled "1705 Berryessa Avenue Convenience Store, Gas Station and Car Wash Environmental Noise Assessment Extant Report No. 160701.01" dated September 9, 2016, prepared for: A.U. Energy LLC 41805 Albrae Street Fremont, CA 94538, prepared by: Michael Carr, INCE, CTS Principal Consultant.
Alternatively, to the satisfaction of the Director of Planning, an equivalent product may be used that achieves at least the same level of noise minimization as the noise equipment and measures described in the above-mentioned noise report.

ENVIRONMENTAL MITIGATION

IMPLEMENT THE MITIGATION MEASURES IDENTIFIED IN THE INITIAL STUDY (IS) AND MITIGATED NEGATIVE DECLARATION (MND) FOR THE 1705 BERRYESSA ROAD DEVELOPMENT PROJECT (FILE NO. PDC15-027) AND MITIGATION MONITORING AND REPORTING PROGRAM, AS MAY BE AMENDED.

THE FOLLOWING SPECIFIC MITIGATION MEASURES, OR EQUIVALENT MITIGATION MEASURES TO THE SATISFACTION OF THE DIRECTOR OF PLANNING, SHALL BE INCLUDED IN DEVELOPMENT AND USES ON-SITE TO REDUCE POTENTIALLY SIGNIFICANT EFFECTS TO A LESS THAN SIGNIFICANT LEVEL:

1. AIR QUALITY. Impact AIR-1: The use of various diesel-powered vehicles and equipment during construction could expose adjacent sensitive receptors to substantial toxic air contaminants.
 - MM AIR-1.1: Diesel-fueled construction equipment used onsite for more than two days shall meet EPA Tier 2 emission standards. The project applicant shall submit, for review and approval, a construction operations plan that includes a list of equipment to be used more than two days during construction, specifications of the equipment to be used during construction and a letter signed by a qualified air quality specialist, verifying that the equipment included in the plan meets the standards set forth in this mitigation measure to the Director of the Department of Planning, Building, and Code Enforcement prior to issuance of a grading or demolition permit. The plan shall demonstrate compliance with the Tier 2 emissions standards.

2. BIOLOGICAL RESOURCES. Impact BIO-1: Demolition and construction activities, including the removal of trees from the project site, could impact nesting migratory birds.

- MM BIO-1.1: If feasible, construction shall be scheduled between September 1 and January 31 (inclusive) to avoid the nesting season. If this is not feasible, pre-construction surveys for nesting raptors and other migratory breeding birds (including yellow warblers) shall be conducted by a qualified ornithologist to identify active nests that may be disturbed during project implementation on-site and within 250 feet of the site. Between February 1 and April 30 (inclusive) preconstruction surveys shall be conducted no more than 14 days prior to initiation of construction activities (including any ground-disturbing activities) or tree removal. Between May 1 and August 31 (inclusive), pre-construction surveys shall be conducted no more than 30 days prior to initiation of these activities. The surveying ornithologist shall inspect all trees in and immediately adjacent (within 250 feet) to the construction area for nests. If an active nest is found in or close enough to the construction area to be disturbed by these activities, the ornithologist shall, in consultation with the California Department of Fish and Wildlife, designate a construction-free buffer zone (typically 250 feet for raptors and 100 feet for other birds) around the nest, which shall be maintained until after the breeding season has ended and/or a qualified ornithologist has determined that the young birds have fledged. The project applicant shall submit a report indicating the results of the survey and any designated buffer zones to the satisfaction of the Director of Planning, Building, and Code Enforcement prior to issuance of any grading permits.

3. HAZARDS AND HAZARDOUS MATERIALS. Impact HAZ-1: Hazardous materials contamination on the site, if discovered in soil or groundwater, could pose a risk to construction workers and others on or around the project site.

- MM HAZ-1.1: The following mitigation measures will be implemented prior to the start of ground-disturbing activities to reduce the potential for construction workers or others to encounter hazardous materials contamination. Prior to the issuance of a demolition or grading permit, a site management plan (SMP) shall be prepared and submitted to the Santa Clara County Department of Environmental Health Hazardous Materials Compliance Division to address the impact of disturbance of any residual soil or groundwater contamination. Appropriate soil testing, characterization, storage, transportation, and disposal procedures shall be specified in the SMP. Contaminated soils shall be disposed of at a licensed facility in accordance with appropriate local, state, and federal regulations. The SMP shall also establish management practices for handling contaminated soil or other materials if encountered during demolition and construction activities. The SMP shall be reviewed and approved by the County of Santa Clara Department of Environmental Health and submitted to the City of San José Department of Planning, Building, and Code Enforcement prior to issuance of grading or building permits.

S:\1-Projects\14-5064-1705 Berryessa San Jose\Draw's\Planning\14-5064-C51_1.dwg modified by mtauser2 at Mar 22, 2018 - 10:52am

SHELL GAS STATION, CARWASH, "LOOP" CONVENIENCE STORE & RETAIL BUILDING

1705 BERRYESSA RD.
SAN JOSE, CALIFORNIA 95133

DRAWING INDEX

- | | |
|-----|--|
| 1 | TITLE SHEET |
| 2 | LAND USE PLAN |
| 2.1 | STANDARD DEVELOPMENT PLAN |
| 3.1 | EXISTING SITE PLAN |
| 3.2 | CONCEPTUAL SITE PLAN |
| 4 | CONCEPTUAL GRADING PLAN |
| 5 | CONCEPTUAL STORMWATER CONTROL PLAN |
| 7A | CONCEPTUAL CONVENIENCE STORE / CARWASH BUILDING ELEVATIONS |
| 7B | CONCEPTUAL RETAIL BUILDING ELEVATIONS |
| 9A | CONCEPTUAL CONVENIENCE STORE / CARWASH BUILDING FLOOR PLAN |
| 9B | CONCEPTUAL RETAIL BUILDING FLOOR PLAN |
| 10 | CONCEPTUAL LANDSCAPE PLAN |
| 11 | CONCEPTUAL SITE PHOTO-METRIC |
| 12 | SITE DETAILS |

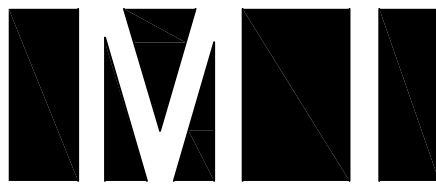
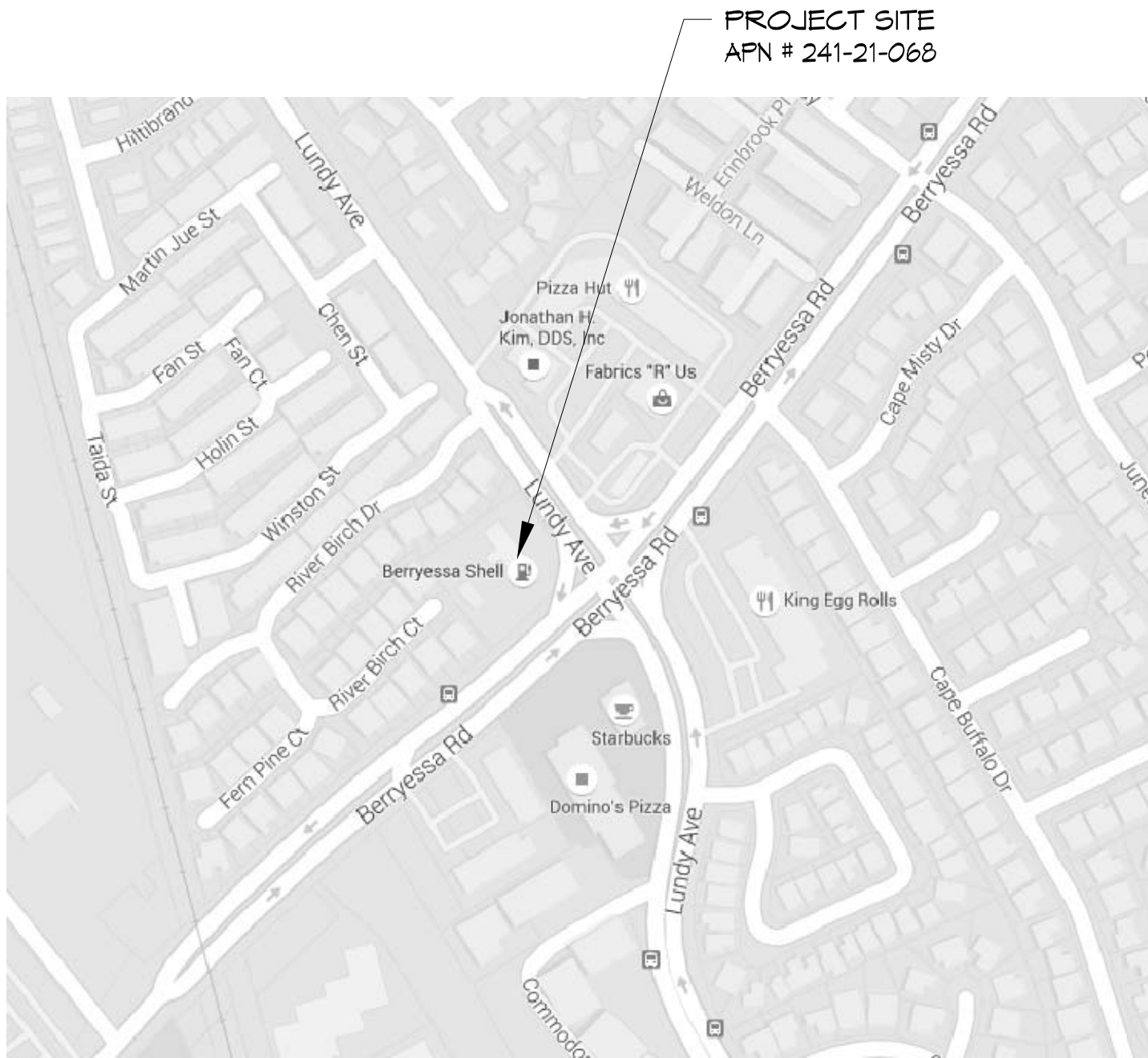
PROJECT DESCRIPTION

- PRIOR DEVELOPMENT PERMITS:**
- PDC45-003
 - PD45-035
- PROPOSED USES:**
- SHELL GAS STATION, "LOOP" CONVENIENCE STORE, DRIVE-THRU CARWASH & RETAIL / DELI BUILDING
- PROJECT DEMOLITION:**
- REMOVE EXISTING SNACK SHOP BUILDING
 - REMOVE EXISTING CARWASH BUILDING
 - REMOVE TWO (2) DRIVEWAYS, ONE ALONG LUNDAY AVE. FRONTAGE & ONE ALONG BERRYESSA RD FRONTAGE
- NEW CONSTRUCTION:**
- CONSTRUCT A 3,212 S.F. CONVENIENCE STORE W/ ATTACHED 1,086 SELF SERVE CARWASH TUNNEL & 264 S.F. CARWASH EQUIPMENT ROOM
 - CONSTRUCT A 2,241 S.F. RETAIL / DELI BUILDING
- NEW SITE IMPROVEMENTS:**
- NEW 32 FOOT WIDE DRIVEWAY ALONG THE BERRYESSA RD FRONTAGE
 - NEW 4'-8" DEDICATION TO RIGHT OF WAY ALONG BERRYESSA RD
 - NEW 4'-11" DEDICATION TO RIGHT OF WAY ALONG LUNDAY AVE.
 - NEW ONSITE PARKING STALLS
 - NEW ACCESSIBLE PATH OF TRAVEL TO THE RIGHT-OF-WAY
 - NEW MASONRY TRASH ENCLOSURE
 - NEW SITE LIGHTING
 - NEW LANDSCAPING
 - NEW SELF-SERVICE AIR/WATER AND VACUUM UNITS
- ADDITIONAL OPERATIONAL FACILITY INFORMATION:**
- HOURS OF OPERATION FOR THE GAS STATION, CONVENIENCE STORE & CARWASH SHALL BE: 24 HRS. / DAY, 7 DAYS / WEEK, 365 DAYS / YEAR

PROJECT DIRECTORY

- | | |
|--|---|
| ARCHITECT
M I ARCHITECTS, INC.
2221 OLYMPIC BLVD, SUITE 100
WALNUT CREEK, CA 94595
TEL: (925) 281-1174
FAX: (925) 943-1581
CELL: (925) 878-9875
MR. MUTHANA IBRAHIM, ARCHITECT | LANDSCAPE ARCHITECT
GIARDELLA ASSOCIATES
640 MENLO AVE, SUITE 10
MENLO PARK, CA 94025
TEL: (650)326-6100
FAX: (650)323-6706
CELL: -
MR. RICHARD GIARDELLA |
| CIVIL ENGINEER
STUKAM CONSULTING ENGINEERS, INC.
11844 COLIMA ROAD, SUITE 235C
GOLDENVIEW, CA 95610
TEL: (916) 835-5741
FAX: (916) 988-6316
MR. FAREED T. SIDDIQUI, P.E. | DEVELOPER
A U ENERGY, LLC
41805 ALBRAE ST., 2ND FLR.
FREMONT, CA 94538
TEL: (650) 868-7454
FAX: -
MR. NICK GOYAL |

LOCATION MAP



Architects

M I Architects, Inc.
ARCHITECTURE
PLANNING
MANAGEMENT
DESIGN
2221 OLYMPIC BLVD.,
SUITE 100
WALNUT CREEK, CA
94595
925-281-1174 Tel
925-943-1581 Fax
925-878-9875 Cell
muthana@miarchitect.com
www.miarchitect.com

SHELL GAS STATION, CARWASH,
"LOOP" CONVENIENCE STORE & RETAIL BLDG
1705 BERRYESSA RD.
SAN JOSE, CA 95133

(PDC15-027)

- | | |
|----------|-------------------------|
| - | ISSUED FOR CONSTRUCTION |
| - | ISSUED FOR PLAN CHECK |
| 07-27-14 | ISSUED FOR PLANNING |

NO.	DATE	DESCRIPTION
△		
△		
△		
△		

TITLE SHEET

PROJECT #: 14-5064
DRAWN: BB CHECKED: MI
SCALE: AS NOTED DATE: 5-11-15

1
SHEET OF

DRAWINGS AND SPECIFICATIONS AND THE CONCEPTS EMPLOYED HEREIN, WHEN CONSIDERED IN CONNECTION WITH THE PROJECT, SHALL BE USED ONLY FOR THE PROJECT AND SHALL NOT BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF M I ARCHITECTS, INC. THE USE, REPRODUCTION OR DISCLOSURE OF THE DOCUMENTS WITHOUT WRITTEN CONSENT FROM M I ARCHITECTS, INC. IS PROHIBITED.